



LAND USE COMMISSION

Wednesday, June 22, 2022

7:00 P.M.

Lorraine H. Morton Civic Center, 2100 Ridge Avenue, James C. Lytle City Council Chambers

AGENDA

Those wishing to make public comments at the Land Use Commission meeting may submit written comments in advance or sign up to provide public comment in-person during the meeting by calling/texting 847-448-4311 or completing the Land Use Commission meeting online comment form available by clicking [here](#), or visiting the Land Use Commission webpage, <https://www.cityofevanston.org/government/land-use-commission>, clicking on How You Can Participate, then clicking on Public Comment Form. Community members may watch the Plan Commission meeting online at www.cityofevanston.org/channel16 or on Cable Channel 16.

I. CALL TO ORDER/DECLARATION OF A QUORUM

II. APPROVAL OF MEETING MINUTES: June 8, 2022

III. OLD BUSINESS (continued from June 8, 2022)

A. Major Adjustment to a Planned Development | 1571 Maple Avenue | 22PLND-0032

1571 Maple Avenue, LLC, applicant, submits for a major adjustment to the planned development approved by ordinance 19-O-15, and amended by ordinances 61-O-16 and 147-O-18 in the D3 Downtown Core Development District. The applicant is requesting to modify the number of required leased parking spaces from 55 to 0, terminating the existing parking lease between the applicant and the City of Evanston. The Land Use Commission makes a recommendation to the City Council, the determining body for this case, in accordance with Section 6-3-9-8 of the Evanston Zoning Code and Ordinance 92-O-21.

IV. NEW BUSINESS

A. Map Amendment | 22PLND-0038

City initiated Map Amendment to the Zoning Ordinance, Title 6 of the City Code, to rezone properties known as: 2600 Gross Point Rd., PIN 05-33-318-032-0000; 2608-2620 Gross Point Rd./2620 Crawford Ave., PINs 05-33-318-033-0000, 05-33-318-034-0000; 2628-2636 Gross Point Rd., PINs 05-33-311-054-0000, 05-33-311-040-0000, 05-33-311-053-0000; 2600 Crawford Ave., PIN 05-33-319-001-0000

The properties are proposed to be removed from the existing oCSC Central Street Corridor Overlay District, and will remain within the existing underlying B1a Business District. The Land Use Commission makes a recommendation to the City Council, the determining body for this case in accordance with Section 6-3-5-8 of the Evanston Zoning Ordinance and Ordinance 92-O-21.

B. Planned Development | 3434 Central Street | 22PLND-0012

Charles Marlas, applicant, applies for a Special Use for a Planned Development and a Special Use to demolish the existing church and other site improvements and to construct a new 2-story, 22,416 square foot building for a Daycare Center-Child, Kensington School. The following site development allowances are needed: 1) Off-street parking located within the front yard where parking is not permitted, 2) Off-street parking located within the south interior side yard where parking is not permitted, 3) Detached accessory use, refuse enclosure, located within the south interior side yard where a detached accessory use is not permitted, 4) Reduce the required transition landscape strip along the south property from 10' to 6', 5) Eliminate the required 10-foot wide transition landscape strip along the west property line, and 6) Reduce the two-way driveway aisle width from 24' to 16', in the R2 Single-Family Residential District. The applicant may seek and the Land Use Commission may consider additional Site Development Allowances as may be necessary or desirable for the proposed development. The Land Use Commission makes a recommendation to the City Council, the determining body for this case in accordance with Section 6-3-5-8 of the Evanston Zoning Ordinance and Ordinance 92-O-21.

V. COMMUNICATION

VI. PUBLIC COMMENT

VII. ADJOURNMENT

The next meeting of the Evanston Land Use Commission will be held on **Wednesday, July 13, 2022, at 7:00 pm, in the James C. Lytle Council Chambers in the Lorraine H. Morton Civic Center.**

Order & Agenda Items are subject to change. Information about the Land Use Commission is available at: <https://www.cityofevanston.org/government/land-use-commission>. Questions can be directed to Meagan Jones at mmjones@cityofevanston.org or 847-448-4311. The City of Evanston is committed to making all public meetings accessible to persons with disabilities. Any citizen needing mobility or communications access assistance should contact 847-448-4311 or 847-866-5095 (TTY) at least 48 hours in advance of the scheduled meeting so that accommodations can be made.

La ciudad de Evanston está obligada a hacer accesibles todas las reuniones públicas a las personas minusválidas o las quines no hablan inglés. Si usted necesita ayuda, favor de ponerse en contacto con la Oficina de Administración del Centro a 847/866-2916 (voz) o 847/448-8052 (TDD).



MEETING MINUTES
LAND USE COMMISSION

Wednesday, June 8, 2022
7:00 PM

Lorraine H. Morton Civic Center, 2100 Ridge Avenue, James C. Lytle City Council Chambers

Members Present: Myrna Arevalo, Brian Johnson, Kiril Mirintchev, Max Puchtel, Matt Rodgers, Kristine Westerberg

Members Absent: Violetta Cullen, George Halik, John Hewko, Jeanne Lindwall

Staff Present: Alexandra Ruggie, Meagan Jones

Presiding Member: Matt Rodgers

Call to Order

Chair Rodgers opened the meeting at 7:00pm. A roll call was then done and a quorum was determined to be present.

Approval of May 25, 2022 Meeting Minutes

Commissioner Westerberg suggested an edit to the draft minutes. Commissioner Westerberg then made a motion to approve the Land Use Commission meeting minutes from May 25, 2022 as amended. Seconded by Commissioner Arevalo. A voice vote was taken and the motion passed, 5-0, with one abstention.

Old Business

A. Major Adjustment to a Planned Development | 1571 Maple Avenue | 22PLND-0032

1571 Maple Avenue, LLC, applicant, submits for a major adjustment to the planned development approved by ordinance 19-O-15, and amended by ordinances 61-O-16 and 147-O-18 in the D3 Downtown Core Development District. The applicant is requesting to modify the number of required leased parking spaces from 55 to 0, terminating the existing parking lease between the applicant and the City of Evanston. The Land Use Commission makes a recommendation to the City Council, the determining body for this case, in accordance with Section 6-3-9-8 of the Evanston Zoning Code and Ordinance 92-O-21. Staff requests that this item be continued to the June 8, 2022 Commission meeting.

Ms. Jones explained that due to the need for additional information from the applicant, and cross referencing it, staff is requesting that this review be continued to the June 22nd meeting.

Commissioner Westerberg made a motion to continue this item to the June 22nd meeting. Seconded by Commissioner Arevalo. A roll call vote was taken and the motion was approved, 6-0.

Discussion

A. Land Use Commissioner Training

Ms. Ruggie provided a presentation regarding Open Meetings Act (OMA), Freedom of Information Act (FOIA) and Robert's Rules of Order and how they apply to the Commission. She then provided a presentation relating to Standards for Approval, why they are important, how they apply and emphasis on standards needing to be met or met with conditions applied in order for an item to be approved (or recommended for approval). General recommendations when reviewing the standards during hearings were also presented.

A discussion followed regarding deliberations, meeting standards and applying conditions, and examples being provided to illustrate the items mentioned in the training.

Communications

No communications.

Public Comment

No public comment.

Adjournment

Commissioner Westerberg motioned to adjourn, Commissioner Puchtel seconded, and the motion carried, 6-0.

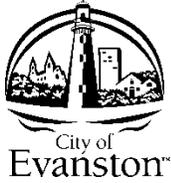
Adjourned 7:34 pm

Respectfully submitted,

Meagan Jones, Neighborhood & Land Use Planner

1571 Maple Avenue
Major Adjustment to a
Planned Development
22PLND-0032

LUC Recommending Body



Memorandum

To: Chair and Members of the Land Use Commission

From: Johanna Nyden, Community Development Director
Meagan Jones, Neighborhood and Land Use Planner

Subject: Major Adjustment to a Planned Development
1571 Maple Avenue, #22PLND-0032

Date: June 17, 2022

Request

1571 Maple Avenue, LLC, applicant, submits for a major adjustment to the planned development approved by ordinance 19-O-15, and amended by ordinances 61-O-16 and 147-O-18 in the D3 Downtown Core Development District. The applicant is requesting to modify the number of required leased parking spaces from 55 to 0, terminating the existing parking lease between the applicant and the City of Evanston. The Land Use Commission makes a recommendation to the City Council, the determining body for this case, in accordance with Section 6-3-9-8 of the Evanston Zoning Code and Ordinance 92-O-21.

Notice

The application has been filed in conformance with applicable procedural and public notice requirements in the Evanston Review on May 5, 2022. The case was subsequently continued at the May 25, 2022 and June 8, 2022 meetings to a date certain of the next scheduled meeting.

General Information

Applicant/Owner: 1571 Maple Avenue LLC
225 W. Hubbard St. Suite 650
Chicago, IL 60654

PINs: 11-18-310-004-0000, 11-18-310-006-0000, 11-18-310-007-0000,
11-18-310-008-0000, 11-18-310-019-0000, 11-18-310-020-0000,

Background



1571 Maple Planned Development

The development, approved on April 13, 2015, was constructed in 2017 south of Davis Street between Maple Avenue and Elmwood Avenue. The planned development was amended by ordinance on June 13, 2016 in order to extend the time for the applicant to obtain a building permit to begin construction on the development. The development consists of a 12-story mixed-use building with 101 residential units, 3,696 square feet of commercial space on the ground floor, and a 13-space surface parking lot on the west side of the site along Maple Avenue that includes two car-share spaces and an electric vehicle charging station. The surface lot is to be available to the public for short term use with a maximum two (2) hour time limit between 10:00 a.m. and 5:00 p.m. on any given Monday through Friday. Outside of these hours, the spaces are available to building residents and visitors. At the time of its approval, the development was also to provide 101 parking spaces at the Maple Avenue garage through a lease agreement with the City. The Maple Avenue garage is located 1,003 feet from the property and was chosen for use due to the Sherman Avenue parking garage, which is approximately 325 feet from the property, not having enough available parking spaces at the time the project was approved (currently, however, there are available parking spaces within this garage).

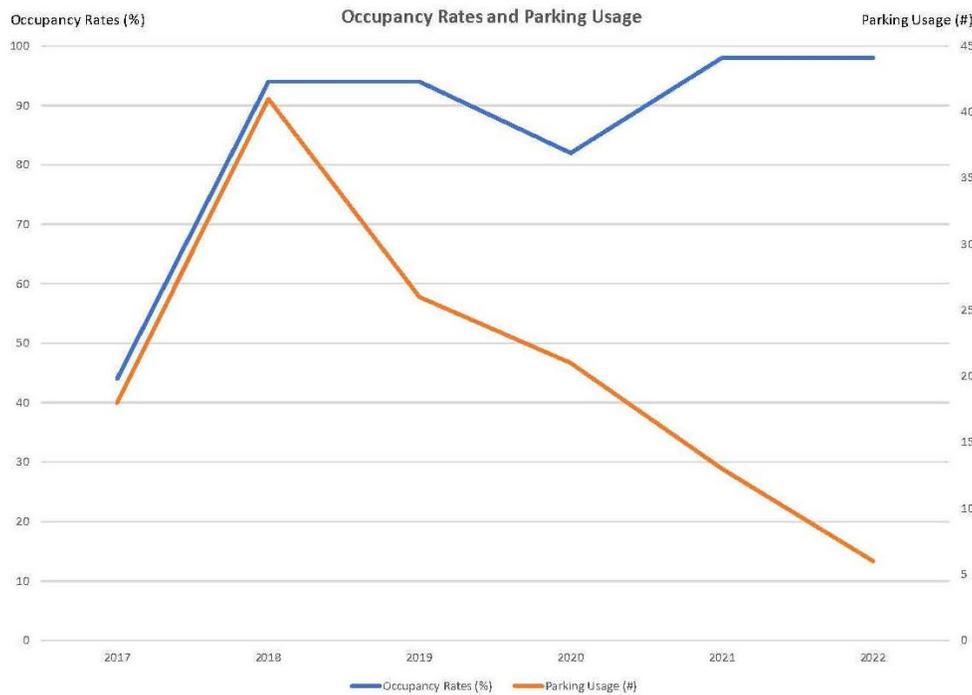
In 2018, the applicant applied for a Major Adjustment to the approved planned development in order to reduce the number of leased parking spaces from 101 to 50 and to modify the affordable housing requirement from two on-site units at 100% Area Median Income (AMI) to one affordable on-site unit at 60% AMI. The adjustment approved by Ordinance 147-O-18 reduced the number of required leased parking spaces to 55 (for a total of 68 parking spaces) and allowed the applicant to provide one

on-site affordable unit at or below 60% Area Median Income (AMI) in lieu of two originally required by ordinance 19-O-15.

Parking Data

The Applicant provided information on the number of residents who are currently leasing spaces within the Maple Avenue garage as of this year. There are currently 6 parking spaces being leased in the garage by the building’s residents despite a building occupancy rate of 98% (see applicant provided chart and graph below). The applicant also noted the downward trend in parking utilization since the development was constructed and despite the Covid-19 pandemic.

Year	Occupancy Rates (%)	Parking Usage (#)	Parking Usage (%)
2017	44	18	40.5
2018	94	41	43.18
2019	94	26	27.38
2020	82	21	25.4
2021	98	13	13.13
2022	98	6	6.06



Parking staff reviewed most recent data from the Secretary of State and found that 19 vehicles were registered to this development with 8 having purchased wheel tax for the season. Information shared with staff also suggests that some of the residents have made private arrangements with residents of condominium building at 1570 Elmwood to park in extra spaces at that property.

The property is located less than 700 feet from the Davis Street Metra and CTA stations. The building is located in close proximity to multiple bus lines (both Pace and CTA), an existing Divvy Bike Station at Benson Avenue and Church Street, and has two Zip-car car sharing stations on-site in the parking lot.

Per ordinance 19-O-15, the development was originally granted a site development allowance to provide 12 on-site parking spaces. Condition Z of that ordinance also required 101 spaces to be leased in the Maple Avenue garage (1.12 parking spaces per dwelling unit and .75 per bedroom). Ordinance 148-O-18 amended this requirement to 55 spaces leased in the Maple Avenue garage (.67 per dwelling unit and .45 per bedroom). Per recently approved TOD parking requirements of .55 spaces per bedroom, 85 parking spaces would now be required (.84 per dwelling unit and .56 per bedroom). If the request is approved, the number of parking spaces provided would total 13 on-site parking spaces (.13 per dwelling unit and .09 per bedroom).

In recent years, developments have requested reductions in their required on-site parking. 811 Emerson St., which has 241 dwelling units, was recently approved for reduction in their required on-site parking from 170 down to 85 parking spaces in order to lease the remaining existing spaces to the public. Similarly, E2 at 1881 Oak Avenue which has 353 dwelling units was approved to reduce its 353 onsite parking space requirement by up to 25% in order to lease those approximately 88 parking spaces to the public. The recently approved Varsity Theater residential development at 1706-10 Sherman Ave, has 35 dwelling units and was granted a variance to provide two on-site parking spaces and lease an additional 16 spaces from the City. The office/commercial building planned development at 1732-40 Orrington Avenue was also recently approved to provide 35 on-site parking spaces and lease 100 parking spaces in one of the City's downtown garages.

Based on the above information and further investigation after the Design and Project Review Committee meeting, staff does not support completely terminating the parking lease. This is a broader policy decision for the City and how large and new developments accommodate parking. With additional developments planned to be constructed, there is less certainty on parking spaces being available in the future for residents of this development. Therefore, staff does not support granting the adjustment to terminate the existing parking lease. Staff recommends modifying the parking lease to relocate the 55 parking spaces currently leased at the Maple Avenue Garage to the Sherman Garage (closer to the property) and only charging the rental company for the parking spaces that are utilized (rather than the entire block). The underlying concern is that if the parking lease is terminated, due to the removal of the parking requirement, individual renters would not be effectively guaranteed parking spaces in these garages and would be competing with other monthly parking arrangements.

DAPR Committee Review

The Design and Project Review Committee reviewed the proposed Adjustment to the Planned Development on May 3, 2022. The Committee voted to recommend a Major Adjustment to the Planned Development to eliminate the parking lease subject to adding additional on-site electric vehicle charging stations.

Since that meeting and as previously stated, staff received updated Secretary of State information which indicates more vehicles in use at the site. This leads to staff's current recommendation to deny the termination of the current parking lease.

Compliance with the Zoning Ordinance

The intent of the D3 Downtown Core Development District is:

"...to provide for the highest density of business infill development and large scale redevelopment within downtown Evanston. The district is also intended to encourage and sustain a mix of office, retail, and residential uses. Planned developments are encouraged as a special use in the D3 district. Where D3 zoned lots or areas are overlaid with the oRD redevelopment overlay district designation, a planned development is required in order to ensure that proposed development in these areas is consistent with the objectives and policies of the adopted plan for downtown Evanston."

As outlined above, the current parking requirement for this TOD building is 85 parking spaces. The completed building has provided infill mixed-use development in the downtown area. The planned development was approved with site development allowances related to number of dwelling units, height, floor area ratio, ziggurat setbacks and number of parking spaces. The proposed adjustment would modify a previously granted condition of approval by eliminating the number of leased parking spaces, leaving 13 on-site parking spaces.

Compliance with the Comprehensive Plan

The proposed Adjustment to the Planned Development is largely consistent with the 2000 Comprehensive General Plan which acknowledges the need to provide adequate parking in the downtown area, mitigating impacts to on-street neighborhood parking, and encouraging use of alternative means of transportation.

The proposal is consistent with the Plan's policy/action to encourage mass-transit and bicycle ridership as alternatives that reduce automobile parking demand. The building itself is a transit oriented development (TOD) that is less than 700 feet away from Metra and CTA stations and is near various bus routes. Car-sharing spaces on-site also encourage less vehicle ownership of the building's residents. The applicant has noted that, since the building reached near capacity, parking numbers have trended downward. Staff does note that recent Secretary of State information indicates that 19 vehicles are registered at this address and, though vehicle ownership is trending downward, there is a possibility for shifts in vehicle ownership within the building and additional spaces may or may not remain available in City parking garages without a

lease agreement holding those spaces.

Per Design and Project Review Committee recommendation, should the reduction in required parking be approved, provisions should be made to provide additional electric vehicle charging stations to further align with the more recently approved City's Climate Action Resilience Plan (CARP) goals. This would be in addition to the continued monitoring of vehicle ownership reports the applicant intends to provide to the City and the offering of car-sharing memberships to building residents.

Compliance with the 2009 Downtown Plan

The proposed adjustment relates to the following objective within the downtown plan:

Objective 5: Maintain a Strong Multi-Modal Transportation System

The proposed adjustment to eliminate the current parking lease can be seen as maintaining consistency with the strategies: 1) "Encourage more residents and visitors to use transit to and from downtown, including possibly changing parking ratios for new development near the train stations". The 2017 TOD Parking Study and subsequent text amendment reduced the parking required for residential uses within transit oriented development areas. This reduced the total required parking for this site to 85 spaces. The proposed adjustment would reduce the amount of parking to the 13 existing on-site parking spaces.

Standards of Approval

The proposed development shall follow the procedures for Adjustments to Development Plan in Section 6-3-6-12. It must maintain the planned development's satisfaction of the Standards for a Special Use (Section 6-3-5-10), the Standard for Planned Development (Section 6-3-6-9) and standards and guidelines established for Planned Developments in the D3 Downtown Core Development District. (Section 6-11-1-10). Staff finds that the proposed major adjustment maintains much of the development's ability to meet the applicable standards for approval; however, there are some standards that warrant additional consideration.

Standards for Special Use (Section 6-3-5-10)

A Planned Development is listed as a permitted special use in the D3 Downtown Core Development District. As indicated above, the proposal is in keeping with the purposes and policies outlined in the Comprehensive Plan and the Zoning Ordinance. The development and corresponding site development allowances were approved in April of 2015. There are no physical or use changes proposed for the site so no impacts are expected with regards to utilities, environmental features or architectural resources. The proposed change would likely not interfere with or diminish the value of other properties in the neighborhood.

The proposal will not cause undue traffic congestion. The existing building is a TOD with 13 on-site parking spaces (including car-sharing), and a number of transportation

options available within a short distance including Metra, CTA Transit stations, and bus routes. There is some concern that, with the elimination of the existing parking lease, as well as future development utilizing City garages, parking spaces may not be readily available should the need arise.

Standards and Guidelines for Planned Developments in D3 District (Sections 6-3-6-9 and 6-11-1-10)

The initial planned development and adjustment were found to be in harmony and comply with general purposes and the intent of the Zoning Ordinance and Comprehensive Plan. As well, it maintained compatibility in bulk, scale and land use with surrounding properties. As noted above, the proposed adjustment maintains the physical compatibility, however, there are some concerns regarding terminating the existing parking lease and full elimination of off-street parking requirements.

Recommendation

Based on the analysis above, staff recommends the Land Use Commission make a recommendation to the City Council for denial of the Major Adjustment to the Planned Development at 1571 Maple Avenue to modify the parking lease condition of approval (Z), eliminating the parking lease and reducing off-street parking spaces from 55 to 0.

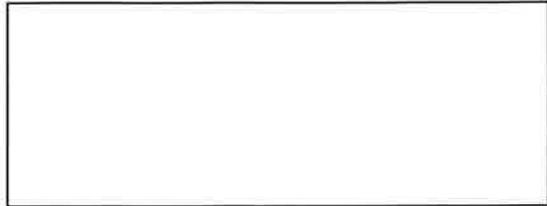
Should the Commission vote to recommend approval of the adjustment, staff recommends that the DAPR conditions of an additional on-site electric vehicle charging station be installed in addition to opening the leased spaces to be located within the Sharman Avenue parking garage, requiring vehicle ownership reports to be provided to the City and subsidized car-share memberships be provided to building residents.

Attachments

- 1571 Maple Avenue Major Adjustment Application
- Updated applicant narrative
- Ordinance 148-O-18
- Minutes from May 3, 2022 DAPR Meeting
- Comments received as of June 17, 2022



PLANNED DEVELOPMENT APPLICATION



Case Number: _____

1. PROPERTY

Address(es)/Location(s)

1571 Maple Avenue; 11-18-310-008-0000, 11-18-310-004-0000, 11-18-310-006-0000, 11-18-310-007-0000, 11-18-310019-0000, 11-18-310-020-0000

Brief Narrative Summary of Proposal:

Applicant seeks to amend Ordinance No. 147-O-18 in order to modify its parking requirements from 55 off-site parking

spaces (within the city-owned parking garage) to 0 to allow the Applicant to terminate its lease with the City of Evanston.

The proposal will not affect the 11 on-site parking spaces.

2. APPLICANT

Name: 1571 Maple Avenue LLC Organization: _____

Address: 225 W. Hubbard Street, Suite 650 City, State, Zip: Chicago, Illinois 60654

Phone: Work: 312-789-5959 Home: _____ Cell/Other: 312-504-1417

Fax: Work: _____ Home: _____

E-mail: mmclean@condorpartners.com

Please circle the primary means of contact.

What is the relationship of the applicant to the property owner?

- same
- architect
- officer of board of directors
- builder/contractor
- attorney
- other: _____
- potential purchaser
- lessee
- potential lessee
- real estate agent

3. SIGNATURE

"I certify that all of the above information and all statements, information and exhibits that I am submitting in conjunction with this application are true and accurate to the best of my knowledge."

See Signature Page Attached

Applicant Signature – **REQUIRED**

Date

Signature Page
Attached to and Made a Part of
Planned Development Application

1571 Maple Avenue, LLC,
an Illinois limited liability company

By: 1571 Maple Avenue Associates, LLC
a Delaware limited liability company

By: CP Maple Avenue Member, LLC,
an Illinois limited liability company,
its Managing Member

By: McLinden Holdings, L.L.C.
an Illinois limited liability company

By: 
Name: John McLinden
Its: Manager

Date: March 22, 2022

4. PRE-SUBMISSION REQUIREMENTS

Prior to actually submitting an application for Planned Development, you must:

- A. Complete a Zoning Analysis of the Development Plan
The Zoning Office staff must review the development plan and publish a written determination of the plan's level of compliance with the zoning district regulations. Apply at the Zoning Office.
- B. Present the planned development at a pre-application conference
Contact the Zoning Office to schedule a conference with Planning & Zoning Division staff.

5. REQUIRED SUBMISSION DOCUMENTS AND MATERIALS

- (This) Completed Application Form
- Application Fee (Planned Development: \$6,000; Major Adjustment: \$2,200; Minor Adjustment: \$1,000)
- Two (2) Copies of Application Binder

Your application must be in the form of a binder with removable pages for copying.
You must submit two application binders for initial review.
The Application Binder must include:

- Certificate of Disclosure of Ownership Interest Form
- Plan drawing illustrating development boundary and individual parcels and PINs
- Plat of Survey of Entire Development Site
- Zoning Analysis Results Sheet
- Preliminary Plat of Subdivision
- Pre-application Conference Materials
- Development Plan
- Landscape Plan (if changes will be proposed)
- Inclusionary Housing Ordinance Application
- Statement addressing how the planned development approval will further public benefits
- Statement describing the relationship with the Comprehensive Plan and other City land use plans
- Statement describing the development's compliance with any other pertinent city planning and development policies
- Statement addressing the site controls and standards for planned developments
- Statement of proposed development's compatibility with the surrounding neighborhood
- Statement of the proposed development's compatibility with the design guidelines for planned developments
- Statements describing provisions for care and maintenance of open space and recreational facilities and proposed articles of incorporation and bylaws
- Restrictive Covenants
- Schedule of Development
- Market Feasibility Statement
- Traffic Circulation Impact Study
- Statement addressing development allowances for planned developments

Notes:

- **Plats of survey** must be drawn to scale and must accurately and completely reflect the current conditions of the property.
- **Building plans** must be drawn to scale and must include interior floor plans and exterior elevations.
- **Application Fees** may be paid by cash, check, or credit card.
- **Mailing Fees** also apply and will be provided to the applicant from the City's mailing vendor.

6. OTHER PROFESSIONAL REPRESENTATIVE INFORMATION

Attorney

Name: Bernard I. Citron Organization: Thompson Coburn LLP
Address: 55 East Monroe, 37th Floor (bberberian@thompsoncoburn.com) City, State, Zip: Chicago, Illinois 60603
Phone: 312-580-2901 Fax: _____ Email: bccitron@thompsoncoburn.com

Architect

Name: N/A Organization: _____
Address: _____ City, State, Zip: _____
Phone: _____ Fax: _____ Email: _____

Surveyor

Name: N/A Organization: _____
Address: _____ City, State, Zip: _____
Phone: _____ Fax: _____ Email: _____

Civil Engineer

Name: N/A Organization: _____
Address: _____ City, State, Zip: _____
Phone: _____ Fax: _____ Email: _____

Traffic Engineer

Name: N/A Organization: _____
Address: _____ City, State, Zip: _____
Phone: _____ Fax: _____ Email: _____

Other Consultant

Name: Center for Neighborhood Technology Organization: _____
Address: _____ City, State, Zip: _____
Phone: _____ Fax: _____ Email: _____

Application Procedure

- (A) **Pre-Application Conference:** Prior to application submittal, an applicant shall meet with the Zoning Administrator, Planning & Zoning staff, and the Alderman of the ward in which the proposed planned development is located. Where applicable, a representative of the Preservation Commission shall be present. The purpose of the conference is to present the concept of the proposed plan and discuss procedures and standards for approval. No representation made by City staff or the representative of the Preservation Commission during such conference or at any other time shall be binding upon the City with respect to the application subsequently submitted. The pre-application conference shall be scheduled within 15 calendar days after receiving the applicant's request.
- (B) **Information Needed for Pre-Application Conference:** The applicant shall include the following information at the time of request for the meeting:
1. Narrative summary of proposal.
 2. Conceptual site plan.
 3. Plat of survey (including the location of utilities).
 4. Proposed elevations.
 5. Photographs of the subject and surrounding properties.
 6. Description of adjacent land uses and neighborhood characteristics.
 7. Description of critical historical structures, details or characteristics (if applicable).
- (C) **Zoning Analysis Application:** Prior to review of the project, the applicant must submit the project for "zoning analysis." The Zoning Division's response to this application is a Zoning Analysis Review Sheet addressing specific regulatory areas, and indicating compliance or deficiency.
- (D) **Results of Pre-Application Conference and Zoning Analysis:** Following the pre-application conference, the Zoning Administrator shall be available to suggest modifications to the site plan as discussed during the pre-application conference. Within 7 calendar days, minutes of the pre-application conference shall be sent to the applicant and shall be available upon request to interested parties by the Zoning Administrator.

Review Procedure - Decision

- (A) **Review Procedure:** Upon the review of an application for a planned development the Zoning Administrator shall notify the developer of any deficiencies or modifications necessary to perfect the planned development application.
- (B) **Public Hearing:** After determining that the application is complete, the Zoning Administrator shall schedule a public hearing to be held by the Plan Commission at which time a formal presentation of the planned development application will be presented. The public hearing shall be held not less than 15 calendar days and no more than 30 calendar days from the date of receipt of the complete application. In addition, a sign shall be posted on the property for a minimum of 10 working days prior to the public hearing indicating the place, time and date of the hearing.
- (C) **Mailed Notices Required:** Notice shall also be given by first class mail to all owners of property within a 1,000-foot radius of the subject property as provided by the City. The failure of delivery of such notice, however, shall not invalidate any such hearing. The City, through its Geographic Information System, will supply the names and addresses of the owners of property within the 1,000-foot radius. A third party mailing service mails notice of the hearing to the neighboring property owners. The applicant must pay any and all fees and postage associated with mailing this notice. The City publishes a notice of the hearing in a locally circulating newspaper, generally the Evanston Review, no less than 15 days nor more than 30 days prior to hearing.
- (D) **Recommendation:** The Plan Commission holds a public hearing and makes a recommendation, based on findings of fact, to the City Council within 60 days of the close of the public hearing. The Planning and Development Committee of the City Council considers the Plan Commission's recommendation and forwards it to the full Council with or without a recommendation. The City Council considers the Plan Commission's recommendation and may introduce an ordinance granting the planned development. The City Council may adopt an ordinance granting the planned development at the following or any subsequent City Council meeting. The developer shall record the ordinance granting the planned development and the development plan with the Cook County Recorder.

Submittal Requirements

- 1) Planned Development Application Form.
- 2) Certificate of Disclosure of Ownership Interest Form listing each individual lot contained within the proposed development identified by parcel identification number and each owner having legal or equitable interest in each

individual parcel. Connection to the ownership interest in the property must be documented in the form of a title insurance policy, deed, lease or contract to lease or purchase.

- 3) Exhibit illustrating the boundaries of each individual parcel contained within the property(ies) proposed for development with coincide parcel identification numbers.
- 4) Plat of Survey, drawn to scale, showing dimensions and areas of the parcel(s), lot(s), block(s), or portions thereof, according to the recorded plat of the subject property(ies).
- 5) Preliminary Plat of Subdivision, if necessary, showing the development consists of, and is coterminous with, a single lot legally described in a recorded plat of subdivision or proposed subdivision or consolidation.
- 6) Pre-application Conference Materials:
 - a) Conceptual site plan, showing parking and bicycle facilities where appropriate;
 - b) Plat of survey showing location of utilities;
 - c) Elevations;
 - d) Photographs of the subject and surrounding properties;
 - e) Description of adjacent land uses and neighborhood characteristics; and
 - f) Description of critical historical structures, details or characteristics.
- 7) Zoning Analysis Results Sheet, if available.
- 8) Development Plan showing:
 - a) Location, dimensions and total area of site;
 - b) Location, dimensions, floor area, construction type and use of each structure;
 - c) Number, type and size of dwelling units, and the overall dwelling unit density;
 - d) Number and location of parking spaces and loading docks, with means of ingress and egress;
 - e) Traffic circulation pattern, location and description of public improvements, streets and access easements to be installed or created;
 - f) All existing and proposed dedications and easements;
 - g) Drainage plan;
 - h) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - i) Significant topographical or physical features, including trees;
 - j) Soil or subsurface conditions; and
 - k) Historical structures or features.
- 9) Landscape Plan, including:
 - a) Location, dimensions and total area of site;
 - b) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - c) Landscaping location and treatment, plant material types, size and quantity, open spaces, and exterior surfaces of all structures with sketches of proposed landscaping;
 - d) Topographic and physical site features including soils and existing trees and vegetation;
 - e) Location, type and size of trees to be removed, and preservation plan for existing trees to remain through construction; and
 - f) Location, type, height and material of all fences and walls.
- 10) Inclusionary Housing Ordinance Application.
- 11) Statement addressing how the planned development approval will further public benefits including:
 - a) Preservation and enhancement of desirable site characteristics, open space, topographic and geologic features, and historic and natural resources;
 - b) Use of design, landscape, and architectural features to create a pleasing environment;
 - c) Provide a variety of housing types in accordance with the City's housing goals;
 - d) Eliminate blighted structures or incompatible uses through redevelopment or rehabilitation;
 - e) Business, commercial, and manufacturing development to enhance the local economy and strengthen the tax base;
 - f) Efficiently use land resulting in more economic networks of utilities, streets, schools, public grounds, and other facilities; and
 - g) Incorporate recognized sustainable design practices and building materials to promote energy conservation and improve environmental quality.
 - h) Additional benefits related to transit alternatives, public art, public space improvements, etc.

- 12) Statement describing the relationship between the proposed development and the Comprehensive General Plan and other City land use plans.
- 13) Statement describing the developments compliance with the Zoning Ordinance and any other pertinent city planning and development policies.
- 14) Statement addressing the site controls and standards for planned developments in the subject property's zoning district regarding the following:
 - a) Minimum area
 - b) Tree preservation
 - c) Landscaped strip
 - d) Open space
 - e) Walkways
 - f) Parking and loading
 - g) Utilities
 - h) Stormwater treatment
- 15) Statement of proposed developments compatibility with the surrounding neighborhood.
- 16) Statement of proposed developments compatibility with the design guidelines for planned developments.
- 17) Statement describing provisions for care and maintenance of open space and recreational facilities and, if owned by an entity other than a government authority, proposed articles of incorporation and bylaws.
- 18) Restrictive Covenants to be recorded against proposed development.
- 19) Schedule of Development phases or stages stating beginning and completion time for each phase.
- 20) Market Feasibility Statement indicating the consumer market areas for all proposed uses in the development, the population potential of the area to be served by the proposed uses and other pertinent information concerning the demand for such uses of land.
- 21) Traffic Circulation Impact Study showing the effect of the development upon adjacent roadways, anticipated vehicular trips and traffic flow, and what road improvements and traffic control upgrading might be necessary.
- 22) Statement addressing the development allowances for planned developments in the subject property's zoning district regarding the following:
 - a) Height increases
 - b) Density increases
 - c) The location and placement of buildings varying from that otherwise permitted in the district
 - d) Floor area ratio increases

Standing

The applicant must own, lease, or have legal or equitable interest in the subject property. The Planning and Zoning Division requires the applicant to demonstrate his or her connection to the ownership interest in the property. Documentation can be in the form of a title insurance policy or a deed, and a lease or contract to lease or purchase.

Certificate of Disclosure of Ownership Interest



City of Evanston
DISCLOSURE STATEMENT
FOR ZONING HEARINGS

OFFICE USE	
	CASE #
	DATE RECEIVED

The Evanston City Code, Title 1, Chapter 18, requires any persons or entities who request the City Council to grant zoning amendments, variations, or special uses, including planned developments, to make the following disclosures of information. The applicant is responsible for keeping the disclosure information current until the City Council has taken action on the application. For all hearings, this information is used to avoid conflicts of interest on the part of decision-makers.

1. Address or location of property for which zoning relief is sought, if applicable: 1571 Maple Avenue, Evanston, IL Zip: 60201

2. Name of applicant for zoning relief: 1571 Maple Avenue LLC

3. If applicant is an agent or designee, list the name, address, phone, fax, and any other contact information of the proposed user of the land for which this application for zoning relief is made: Does not apply.
N/A

4. *If a person or organization owns or controls the proposed land user, name, address, phone, fax, and any other contact information of person or entity having constructive control of the proposed land user. Same as number _____ above, or indicated below. (An example of this situation is if the land user is a division or subsidiary of another person or organization.)*

1571 Maple Avenue Associates, LLC; 225 W. Hubbard Street, Suite 501
Chicago, IL 60654, 312-789-5959

5. Name, address, phone, fax, and any other contact information of person or entity holding title to the subject property. Same as number _____ above, or indicated below.

1571 Maple Avenue LLC; 225 W. Hubbard Street, Suite 650,
Chicago, IL 60654, 312-789-5959

6. Name, address, phone, fax, and any other contact information of person or entity having constructive control of the subject property. Same as number _____ above, or indicated below.

If Applicant or Proposed Land User is a Corporation

7.

Any corporation required by law to file a statement with any other governmental agency providing substantially the information required below may submit a copy of this statement in lieu of completing a and b below.

a. Names and addresses of all officers and directors.

b. Names, addresses, and percentage of interest of all shareholders, if there are fewer than 33 shareholders, or shareholders holding 3% or more of the ownership interest in the corporation, if there are more than 33 shareholders.

If Applicant or Proposed Land User is not a Corporation

8.

Name, address, percentage of interest, and relationship to applicant, of each partner, associate, person holding a beneficial interest, or other person having an interest in the entity applying, or in whose interest one is applying, for the zoning relief.

I certify that all of the above statements and all statements, information and exhibits that I am submitting in conjunction with this application for relief from the requirements of the Zoning Ordinance or for an appeal from the Zoning Administrator's decision are true to the best of my knowledge.

See Signature Page Attached

Applicant's signature Date

Applicant's signature Date

Applicant's signature Date

Signature Page
Attached to and Made a Part of
Planned Development Application

1571 Maple Avenue, LLC,
an Illinois limited liability company

By: 1571 Maple Avenue Associates, LLC
a Delaware limited liability company

By: CP Maple Avenue Member, LLC,
an Illinois limited liability company,
its Managing Member

By: McLinden Holdings, L.L.C.
an Illinois limited liability company

By: 
Name: John McLinden
Its: Manager

Date: March 22, 2022

**1571 MAPLE AVENUE - APPLICATION FOR MAJOR ADJUSTMENT
TO PLANNED DEVELOPMENT**

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SECTION 1

PROJECT NARRATIVE

The Applicant, 1571 Maple Avenue LLC (“Applicant”), is the owner of the subject property, located at 1571 Maple Avenue (“Subject Property”).

The requested major adjustment is for the existing mixed-use building, which was approved as a Planned Development pursuant to Ordinance No 19-O-15 approved by the Evanston City Council on April 13, 2015.

The existing building contains 101 apartments, approximately 3,979, square feet of ground floor retail, 13 on-site parking spaces and 55 off-site parking spaces in the city owned parking garage at 1800 Maple Avenue, in accordance with Ordinance 147-O-18, which reduced the off site parking to the current level. The development original provided 101 parking spaces at 1800 Maple Avenue. The on-site parking includes two car-share spaces and an electric car charging station. These spaces are intended to serve the commercial spaces during the day, and are available for residents and visitors overnight. The Applicant will continue to offer reduced membership rates for use of the car-share vehicles on site to residents of the development.

As the development became fully stabilized, it became clear that the 101 parking spaces leased in the 1800 Maple Avenue parking garage were not necessary to serve the tenants. The parking count at the off-site location was reduced by ordinance number 147-O-18 to 55 off-site parking spaces. Because the building is currently 95% rented, and only 6 of the 55 off-site places are currently used by residents of the development, the Applicant now seeks to eliminate the parking lease at the 1800 Maple parking garage and reduce the total required parking in the development to the existing 11 on site parking spaces.

Based on the amount of off-street parking available in the neighborhood, in parking lots that are closer to the subject property than the 1800 Maple Avenue parking garage, the Applicant no longer needs to maintain a lease for off-site spaces. The Applicant will continue to provide an annual vehicle ownership report to the City for record-keeping purposes.

SECTION 2

STATEMENTS ADDRESSING RELIEF STANDARDS

STATEMENT OF COMPLIANCE WITH ZONING ORDINANCE AND OTHER PERTINENT CITY POLICIES

Statement addressing the development's compliance/any other pertinent city planning and development policies regarding the following:

The amendment is in compliance with the "2000 Evanston Comprehensive General Plan," adopted May 8th, 2000 ("Comprehensive Plan") by the City Council as set forth below.

A. Promoting Use of Mass Transit and Pedestrian-Friendly Development

The Comprehensive Plan states the following:

"Promote higher-density residential and mixed-use development in close proximity to transit nodes (e.g., train stations) in order to support non-automobile dependent lifestyles." (Comprehensive Plan, page 108).

- The Subject Property is located within a five-minute walk (1320-ft radius) from The Davis Street purple line station and the Davis Street Metra station. By providing ample bicycle parking and on-site car sharing spaces, the amendment is designed to support non-automobile dependent lifestyles.

"New developments should be integrated with existing neighborhoods to promote walking and the use of mass transit." (Comprehensive Plan, page 8).

- The Subject Property is located within a five-minute walk (1320-ft radius) from various retail stores and restaurants. In fact, the Subject Property earns a "Walk Score" of 96, indicating that daily errands do not require a car. As such, the proposed development promotes walkability within the neighborhood, and reduces the demand for excess parking for residents.
- The Subject Property is located less than 700 feet from the Davis Street CTA station and Metra station. The intersection of Davis and Maple also features stops for the 93, 201, 206, 208 and 213 bus routes. Accordingly, residents of the Subject Property have excellent access to both local and regional transit options.

"New land development can take advantage of renewed interest in pedestrian and transit orientation. For example, slightly higher density residential and residential/commercial mixed-use buildings can be desirable additions along major corridors already very accessible to mass transit." (Comprehensive Plan, page 8).

- The Subject Property is located on a site with convenient accessibility to the CTA purple "L" line and is approximately two blocks away from a Metra station. Further, the property

is in a pedestrian-friendly location, in a transit oriented development area desirable to residents who prefer to walk around downtown. Therefore, the proposed amendment will take advantage of the renewed interest in pedestrian and transit orientation and promote public transportation versus vehicular traffic.

- Since the development was constructed, dining and grocery options have increased in the vicinity, particularly with a Target grocery location 0.3 miles away.

“Promote biking to enhance the character of the community, retail viability, and health of citizens.” (Comprehensive Plan, page 109).

- The development currently provides ample room to store residents’ bicycles in a well-lit and inviting bicycle storage room, encouraging biking as a means of transportation. The number of bicycle parking spaces provided will meet the requirements of the City of Evanston code. The Subject Property also features bike parking for visitors on the corner of Davis and Elmwood Avenue.

STATEMENT OF SITE CONTROLS AND STANDARDS FOR PLANNED DEVELOPMENTS

Statement addressing the site controls and standards for planned developments in the subject property’s zoning district regarding the following:

The requested major adjustment is to a previously-approved planned development. The only requested change is to the parking requirements reflected in the approval ordinance. The applicant seeks to modify the approved number of spaces from 55 off-site spaces and 13 on-site spaces to 13 on-site spaces. The Applicant has determined and substantiated in this application, that the 55 off-site parking spaces are not necessary to serve the needs of the Subject Property’s residents, and that residents who may need off-street parking are so few, that the available off-street parking in the neighborhood will better serve them than the leased spaces in the 1800 Maple Avenue garage.

No other zoning or site controls are being modified.

STATEMENT OF DEVELOPMENT ALLOWANCES FOR PLANNED DEVELOPMENTS

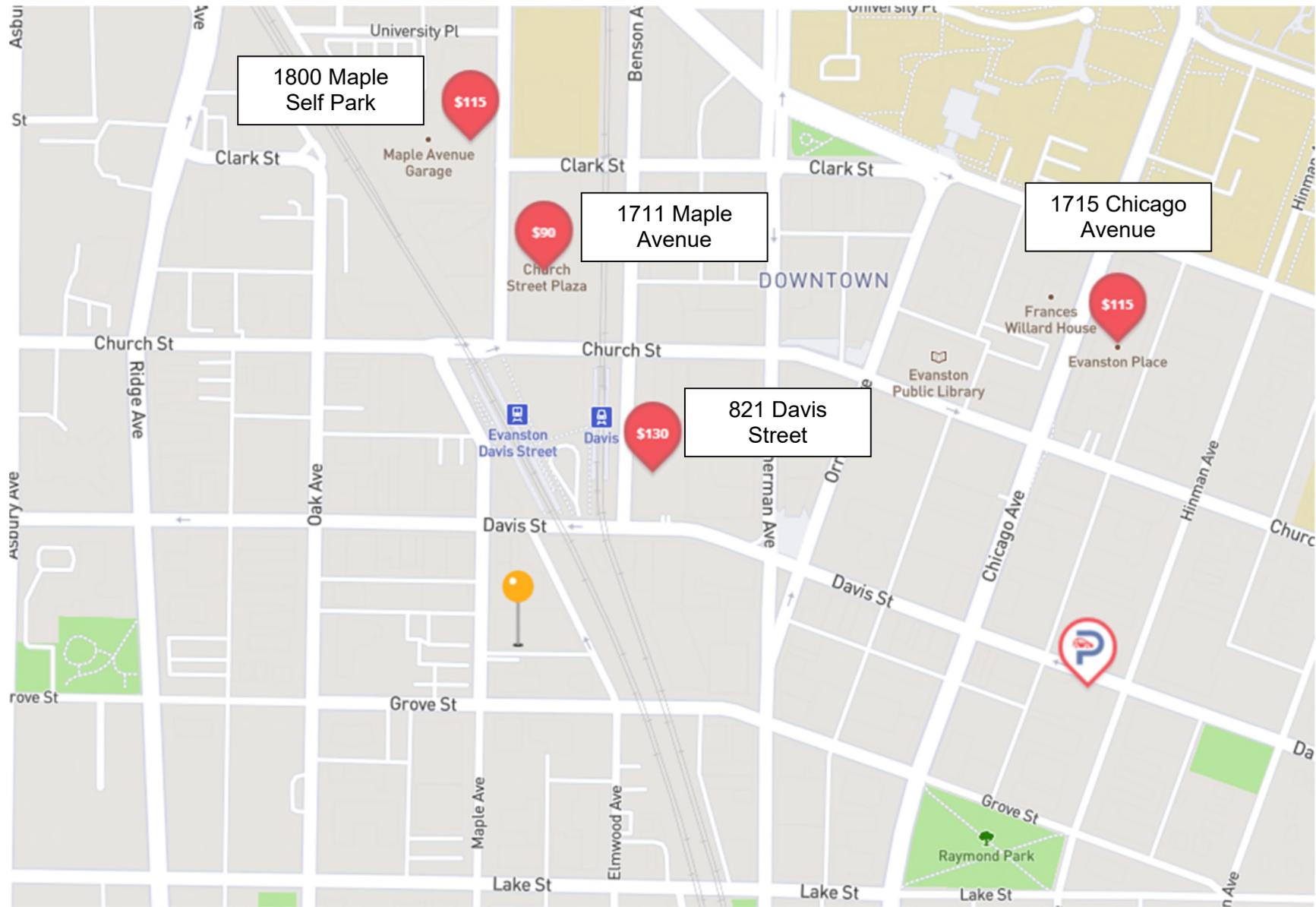
Statement addressing the development allowances for planned developments in the subject property's zoning district regarding the following:

Parking

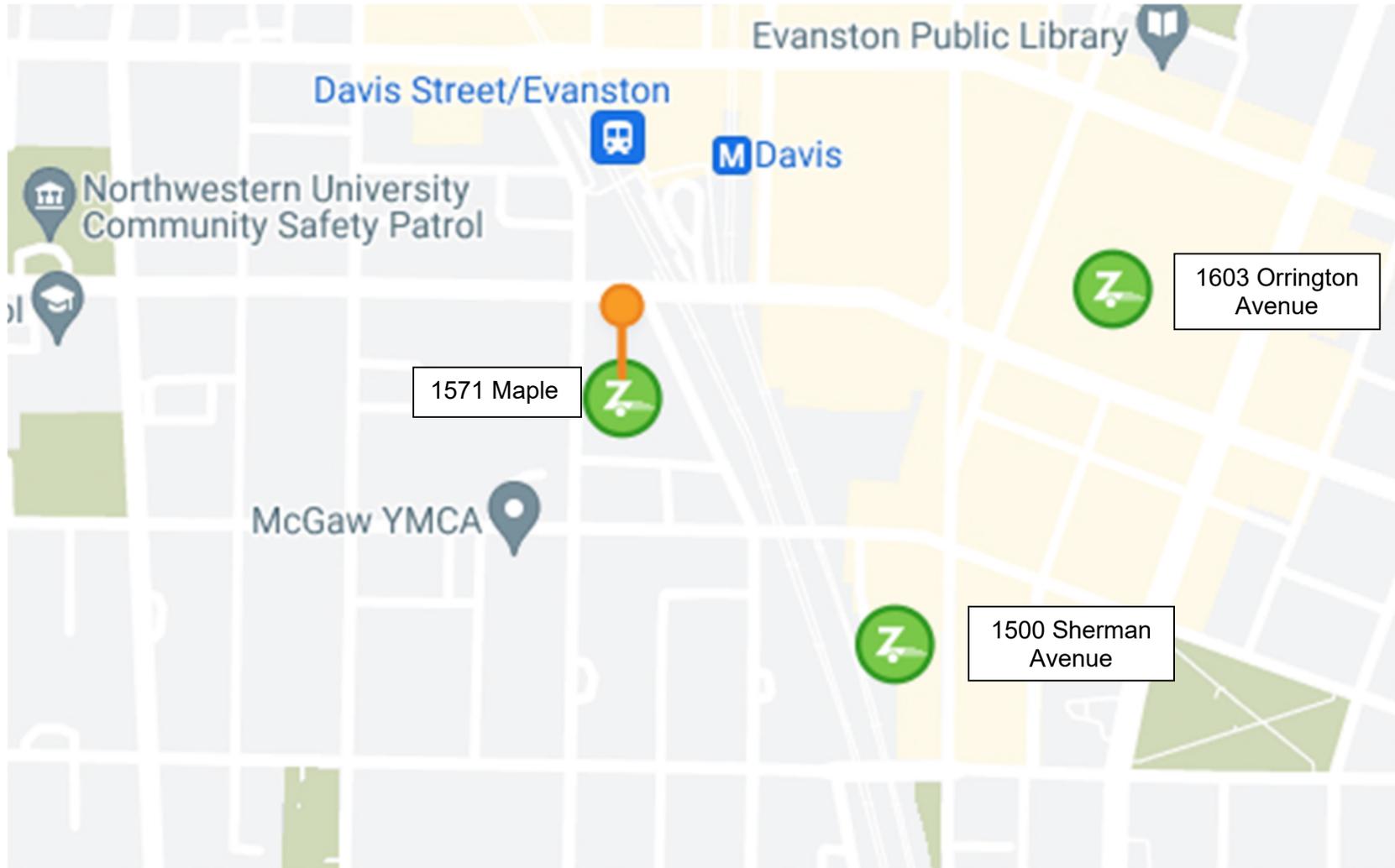
The requested adjustment is to a previously-approved planned development. The only requested change is to the parking requirements reflected in the approval ordinance. The reduction of spaces from 55 off-site spaces and 13 on-site spaces to 13 on-site spaces will allow the Applicant to eliminate a lease that is unnecessary for the operation of the Subject Property. Currently, 6 spaces in the 1800 Maple Avenue garage, where the off-site parking spaces are located, are rented by residents of the Subject Property. This number has decreased steadily since the building was stabilized. Furthermore, the parking resources available in the neighborhood are more than adequate to address any off-street parking needs of residents of the Subject Property.

The requested relief will have no adverse effects on residential uses within or adjoining the development or the overall neighborhood.

Available Monthly Parking Options Near 1571 Maple



Zipcar Locations



1571 Maple Avenue

Major Adjustment

Request and History

The subject proposal will eliminate an existing parking lease between the developer and the City of Evanston, which was established in order to ensure that there would sufficient parking for residents of 1571 Maple Avenue.

The development was planned with just 13 parking spaces, two of which were built into a parklet to enhance the outdoor space and pedestrian experience at the site. The remaining 11 parking spaces are used for serving the on site commercial uses during the day, and can be used by residents and their visitors in the evening. The concept for the development was such that if very limited parking is provided on site, then residents without cars would naturally gravitate toward the building because of the rich transit amenities and amenities of daily living so nearby.

At the time the subject development was entitled, the developer came to a compromise with the City to lease the required number of parking spaces off site in the City's Maple Avenue garage. The garage is more than 1,000 feet away from the subject property, and accordingly does not meet the requirement for off site parking set forth in the City of Evanston's zoning ordinance in Section 6-16-2-1. The developer also agreed to restrict residents from eligibility for on street parking passes ordinarily available to neighborhood residents. Essentially, the requirement to lease spaces in the Maple Avenue garage was a fail-safe, in case residents of the development needed more parking spaces than anticipated by the developer at the time the concept was proposed, so that residents' cars would not create additional stress on the City's on street parking availability. As expected, however, few residents opted to rent space in the building's garage.

The initial lease term was for seven years, and the lease secured 101 parking spaces in the garage for residents – one for each unit. In 2018, an ordinance was approved permitting the developer to lease just 70 spaces rather than 101, and contemplated that the lease would be modified based on usage. Because so few residents were using the leased parking spaces, the requirement has been reduced further, and is now just for 55 parking spaces. As the building's occupancy stabilized, currently at approximately 98%, the parking usage continued to plummet, with just 5 residents opting to rent spaces in the Maple Avenue Garage. All 5 of these residents will be moving out at the end of their lease terms.

In addition, since the development was approved, the City of Evanston has adopted a Transit Oriented Development ("TOD") policy. The subject development is within the City's designated TOD zone. The City has also reduced its parking requirements generally. Because the TOD concept has been tested at this site over the last few years, the development has proven that TOD works and the proposed elimination of the Maple Avenue Garage lease is appropriate for the development.

Existing Parking Leases

Should the parking lease be terminated completely, the Developer will reimburse any prepayments made by residents for those spaces on a pro rata basis. Residents will continue to be free to lease parking space in the Maple Avenue Garage for a monthly fee of \$115 directly from the City, or they may choose to lease parking spaces within several parking garages nearer to the subject property at comparable prices.

Other parking options include:

- 1890 Maple Avenue (E2): \$60/month (\$175 for 2 months)
- 1603 Orrington (parking garage) \$189/month
- 821 Davis (Sherman Plaza Parking Garage): \$130/month

1571 Maple Avenue

[Add scores to your site](#)

Evanston, Illinois, 60201

Commute to **Downtown Evanston**

1 min 4 min 1 min 5 min [View Routes](#)

Favorite **Map** **Nearby Evanston Apartments on Redfin**

[Looking for a home for sale in Evanston?](#)

Walk Score
96

Walker's Paradise

Daily errands do not require a car.

Transit Score
57

Good Transit

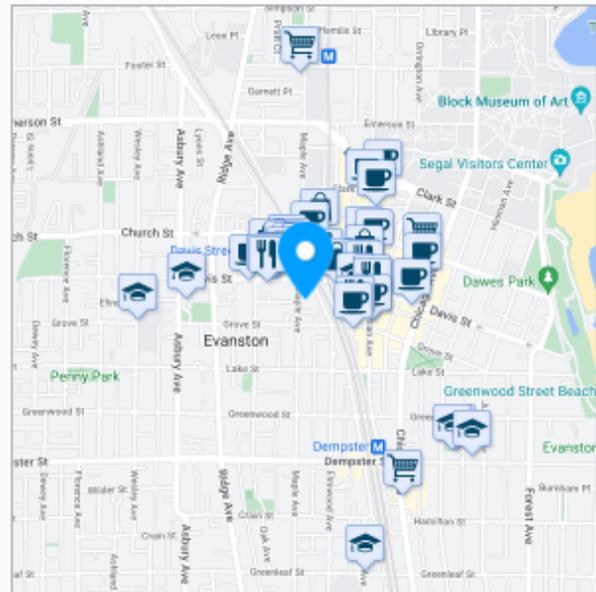
Many nearby public transportation options.

Bike Score
88

Very Bikeable

Biking is convenient for most trips.

[About your score](#)



11/6/2018

147-O-18

AN ORDINANCE

Approving a Major Adjustment Regarding Required Parking and On-Site Affordable Housing at 1571 Maple Authorized by Ordinance 19-O-15 and Amended by Ordinance 61-O-16

WHEREAS, the City of Evanston is a home-rule municipality pursuant to Article VII of the Illinois Constitution of 1970; and

WHEREAS, as a home rule unit of government, the City has the authority to adopt legislation and to promulgate rules and regulations that protect the public health, safety, and welfare of its residents; and

WHEREAS, it is a well-established proposition under all applicable case law that the power to regulate land use through zoning regulations is a legitimate means of promoting the public health, safety, and welfare; and

WHEREAS, Division 13 of the Illinois Municipal Code (65 ILCS 5/11-13-1, *et seq.*) grants each municipality the power to establish zoning regulations; and

WHEREAS, pursuant to its home rule authority and the Illinois Municipal Code, the City has adopted a set of zoning regulations, set forth in Title 6 of the Evanston City Code of 2012, as amended, ("the Zoning Ordinance"); and

WHEREAS, on April 13, 2015, the City Council enacted Ordinance 19-O-15, attached hereto as Exhibit 1 and incorporated herein by reference, which granted a Special Use Permit for a Planned Development (the "Planned Development") at 1571 Maple Avenue (the "Subject Property"), which is legally described in Exhibit 1; and

WHEREAS, Ordinance 19-O-15 approved the construction of a 12-story

mixed use building with 101 residential dwelling units at the Subject Property (the "Project"), which is detailed at length in Exhibit 1; and

WHEREAS, by submitting an application to amend a planned development to the City dated September 12, 2018, the Developer and Applicant, 1571 Maple Avenue, LLC (the "Applicant") requested major adjustments to: (1) reduce the required number of leased parking spaces from one hundred (100) to fifty (50); (2) allow the City to amend the original parking lease; (3) allow the City to provide parking spaces at Sherman Plaza; and (4) seek a change to the affordable housing provisions to allow them to provide one (1) affordable housing unit at sixty percent (60%) of Area Median Income ("AMI") in lieu of two (2) affordable housing units as provided for in Ordinance 19-O-15; and

WHEREAS, in order to remain in compliance with the terms of Ordinance 19-O-15 and 61-O-16 and provide for Applicant to modify its parking requirements and on-site inclusionary housing, the Applicant requests an amendment to the Planned Development; and

WHEREAS, on October 10, 2018, the Evanston Plan Commission ("Plan Commission") held a meeting, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq*), during which it received input from the public, and carefully deliberated on the request and the Applicant was given notice and the opportunity to be heard at the Plan Commission meeting; and

WHEREAS, the Plan Commission recommended that the City council amend the planned development by allowing major adjustments to: (1) reduce the required number of leased parking spaces from one hundred one (101) to seventy (70); (2) allow the City to amend the original parking lease; (3) allow the City to provide

parking spaces at Sherman Plaza; and (4) change the affordable housing provisions to allow the Applicant to provide one (1) affordable housing unit at fifty percent (50%) of Area Median Income ("AMI") in lieu of two (2) affordable housing units as provided for in Ordinance 19-O-15; and

WHEREAS, on November 12, 2018, the Planning and Development Committee ("P&D Committee") held a meeting, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq*), during which it retained jurisdiction over the Planned Development Amendment request; and

WHEREAS, during said meeting, the P&D Committee received input from the public, and carefully deliberated on the major adjustment request and the Applicant was given notice and the opportunity to be heard at the P&D and City Council meetings; and

WHEREAS, at its meeting of November 12, 2018 held in compliance with the provisions of the Illinois Open Meetings Act, the City Council considered the P&D Committee's deliberations and recommendations, heard public comment, made findings and considered this Ordinance 147-O-18,

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS, THAT:

SECTION 1: The foregoing recitals are hereby found as fact and incorporated herein by reference.

SECTION 2: Pursuant to the terms and conditions of this ordinance, the City Council hereby grants an amendment to the Special Use Permit previously authorized by Ordinance 19-O-15 and subsequently amended by Ordinance 61-O-16 to

allow for the amended parking requirements and on-site affordable housing of the Planned Development described herein.

SECTION 3: Pursuant to Subsection 6-3-5-12 of the Zoning Ordinance, the City Council amends conditions (Q) and (Z) on the Special Use Permit granted for the Planned Development in Ordinance 19-O-15, as may be amended by future ordinance(s), and violation of any of which shall constitute grounds for penalties or revocation of said Special Use Permit pursuant to Subsections 6-3-10-5 and 6-3-10-6 of the Zoning Ordinance:

- (Q) **Affordable Housing in the Development:** The Applicant shall provide one (1) one (1) bedroom on-site affordable housing to a household earning at or below sixty percent (60%) of Area Median Income (AMI). The units provided shall be equal in size to the market-rate units within the building. The period of affordability for the units shall be for ten (10) years. The Applicant must submit a compliance report by January 31st of each year to the Housing and Grants Division of the Community Development Department showing the following: (1) unit number; (2) number of bedrooms; (3) tenant name; (4) number of persons in each affordable household unit; (5) annual gross income of each household occupying each affordable housing unit; (6) date of income certification; and (7) monthly unit rent. The compliance report must also include the list of any utilities included in rent.
- (Z) **Parking Lease:** The Applicant must agree and sign an amended long-term parking lease agreement with the City of Evanston to lease fifty-five (55) parking spaces based on the standard current monthly parking fee from the Maple Avenue Parking garage located at 1800 Maple Avenue. The amended lease agreement will mandate that the Applicant pay any increases in the rental rate structure through the term of the lease agreement. The long-term lease agreement shall be set for a minimum period of seven (7) years. For the lifetime of the project, the Applicant must require all Residents to disclose their vehicle ownership and conduct periodic reviews to ensure that all vehicles owned by Residents of the building are accounted for within the Maple Avenue garage. The Applicant must provide the certified vehicle ownership report to the City of Evanston annually by January 31st of each calendar year during the first seven (7) year. The City of Evanston's Department of Administrative Services will monitor the Applicant's certified vehicle ownership reports and the costs incurred by the City of Evanston for such oversight shall be paid for by Applicant's parking lease fees. If at any time during this seven (7) year period such annual vehicle ownership report indicates that the Residents of the building own more than fifty-five (55) cars and require more than fifty-five (55) parking spaces, the Applicant

agrees to amend the parking lease agreement with the City and lease the additional parking spaces necessary. The Applicant also agrees to deny apartment leases to potential Residents who own vehicles until such time as the number of vehicles owned by the Residents of the building and required to park in the Maple Avenue garage by terms of this Ordinance falls below fifty-five (55) or until the surplus parking spaces can be accommodated in the revised lease agreement with the City of Evanston.

Following the seven (7) year anniversary of the parking lease agreement date, the parking lease agreement may be amended. The number of parking spaces leased from the City may be reduced to match the highest number of vehicles owned by the Residents and required to park in the Maple Avenue garage by the terms of this Ordinance in any year during the seven (7) year period per the annual parking reports. The number of parking spaces leased by the City may not be reduced in the first seven (7) years and any reduction after the seven (7) year anniversary shall be approved by the City Council as an amendment to the parking lease agreement.

Following the expiration of the seven (7) year anniversary of the parking lease agreement, the agreement can be modified every five years thereafter but not before, to match the highest number of vehicles owned by the Residents and required to park in the Maple Avenue garage by the terms of this Ordinance during any calendar year in the preceding five (5) year term per the annual parking report. Any amendments to the number of parking spaces leased from the City of Evanston or any other amendments to the lease agreement, including term extensions, shall be approved by the City Council as an amendment to the parking lease agreement.

The Applicant must hold a valid long-term parking lease agreement with the City of Evanston for the lifetime of the project unless this condition is amended by the City Council of the City of Evanston as an amendment to the Planned Development.

SECTION 4: Pursuant to Subsection 6-3-6-10 of the Zoning Ordinance, the Applicant shall, at its cost, record a certified copy of this ordinance, including all exhibits attached hereto, with the Cook County Recorder of Deeds, and provide proof of such recordation to the City, before the City may issue any permits pursuant to the Planned Development authorized by the terms of this ordinance.

SECTION 5: Except as otherwise provided for in this Ordinance 61-O-16, all applicable regulations of the Ordinance 19-O-15, the Zoning Ordinance, and the

entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and/or provisions of any of said documents conflict with any of the terms herein, this Ordinance 61-O-16 shall govern and control.

SECTION 6: When necessary to effectuate the terms, conditions, and purposes of this ordinance, "Applicant" shall be read as "Applicant and its agents, assigns, and successors in interest" and shall mean 1571 Maple Avenue, LLC, and any and all successors, owners, and operators of the Subject Property.

SECTION 7: This ordinance shall be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

SECTION 8: Except as otherwise provided for in this ordinance, all applicable regulations of the Zoning Ordinance and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and provisions of any of said documents conflict with the terms herein, this ordinance shall govern and control.

SECTION 9: All ordinances or parts of ordinances that are in conflict with the terms of this ordinance are hereby repealed.

SECTION 10: If any provision of this ordinance or application thereof to any person or circumstance is ruled unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this ordinance is severable.

SECTION 11: The findings and recitals herein are hereby declared to be prima facie evidence of the law of the City and shall be received in evidence as provided by the Illinois Compiled Statutes and the courts of the State of Illinois.

Ayes: 9

Nays: 0

Introduced: November 12, 2018

Adopted: November 12, 2018

Approved:

November 29, 2018

Stephen H. Hagerty
Stephen H. Hagerty, Mayor

Attest:

Devon Reid
Devon Reid, City Clerk

Approved as to form:

Michelle L. Masoncup
Michelle L. Masoncup, Corporation Counsel

EXHIBIT 1

ORDINANCE 61-O-16

5/17/2016

61-O-16

AN ORDINANCE

Extending the Time for the Applicant to Obtain a Building Permit to Construct the Planned Development at 1571 Maple Authorized by Ordinance 19-O-15

WHEREAS, the City of Evanston is a home-rule municipality pursuant to Article VII of the Illinois Constitution of 1970; and

WHEREAS, as a home rule unit of government, the City has the authority to adopt legislation and to promulgate rules and regulations that protect the public health, safety, and welfare of its residents; and

WHEREAS, it is a well-established proposition under all applicable case law that the power to regulate land use through zoning regulations is a legitimate means of promoting the public health, safety, and welfare; and

WHEREAS, Division 13 of the Illinois Municipal Code (65 ILCS 5/11-13-1, *et seq.*) grants each municipality the power to establish zoning regulations; and

WHEREAS, pursuant to its home rule authority and the Illinois Municipal Code, the City has adopted a set of zoning regulations, set forth in Title 6 of the Evanston City Code of 2012, as amended, ("the Zoning Ordinance"); and

WHEREAS, on April 13, 2015, the City Council enacted Ordinance 19-O-15, attached hereto as Exhibit 1 and incorporated herein by reference, which granted a Special Use Permit for a Planned Development (the "Planned Development") at 1571 Maple Avenue (the "Subject Property"), which is legally described in Exhibit 1; and

WHEREAS, Ordinance 19-O-15 approved the construction of a 12-story

mixed use building with 101 residential dwelling units at the Subject Property (the "Project"), which is detailed at length in Exhibit 1; and

WHEREAS, by letter to the City dated May 17, 2016, the Developer and Applicant, 1571 Maple Avenue, LLC (the "Applicant") requested an extension of the one-year time period to obtain a building permit and start construction for the Planned Development (the "Amendment"); and

WHEREAS, Section 6-11-1-10(A) of the City Code and Section 4(BB) of the Ordinance provides that the Applicant must obtain a building permit and start construction within one (1) year and has not obtained a building permit to date; and

WHEREAS, in order to remain in compliance with the terms of Ordinance 19-O-16 and provide for Applicant to obtain a building permit and start construction, the Applicant requests an amendment to the Planned Development; and

WHEREAS, on May 23, 2016, the Planning and Development Committee ("P&D Committee") held a meeting, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq*), during which it retained jurisdiction over the Planned Development Amendment request; and

WHEREAS, during said meetings, the P&D Committee received input from the public, and carefully deliberated on the Extension request and the Applicant was given notice and the opportunity to be heard at the P&D and City Council meetings; and

WHEREAS, at its meetings of May 23, 2016 and June 13, 2016 held in compliance with the provisions of the Illinois Open Meetings Act, the City Council considered the P&D Committee's deliberations and recommendations, heard public comment, made findings and considered this Ordinance 61-O-16,

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS, THAT:

SECTION 1: The foregoing recitals are hereby found as fact and incorporated herein by reference.

SECTION 2: Pursuant to the terms and conditions of this ordinance, the City Council hereby grants an amendment to the Special Use Permit previously authorized by Ordinance 19-O-15 to allow for the construction and operation of the Planned Development described herein.

SECTION 3: Pursuant to Subsection 6-3-5-12 of the Zoning Ordinance, the City Council imposes the following conditions on the Special Use Permit granted for the Planned Development, may be amended by future ordinance(s), and violation of any of which shall constitute grounds for penalties or revocation of said Special Use Permit pursuant to Subsections 6-3-10-5 and 6-3-10-6 of the Zoning Ordinance:

- (a) Compliance with Applicable Requirements:** The Applicant shall develop and operate the Planned Development authorized by the terms of this ordinance in substantial compliance with the following: the terms of this Ordinance 61-O-16; terms of Ordinance 19-O-15 which have not been amended by this Ordinance; all applicable City Code requirements; the Applicant's testimony to the P&D Committee, and the City Council; and the approved documents on file in this case.
- (b) Changes in Property Use:** Any change as to the property's use in the future must be processed and approved as an additional amendment to the Planned Development.
- (c) Construction Schedule: Construction Schedule:** Pursuant to Subsection 6-11-1-10(A)4 of the Zoning Ordinance, the Applicant must obtain a building permit to within twelve (12) months of the passing of this Ordinance. Additionally, the Applicant must complete the construction of this Planned Development within twenty-four (24) months from the date the Applicant receives its building permit.

(d) Recording: Pursuant to Subsection 6-3-6-10 of the Zoning Ordinance, the Applicant shall, at its cost, record a certified copy of this ordinance, including all exhibits attached hereto, with the Cook County Recorder of Deeds, and provide proof of such recordation to the City, before the City may issue any permits pursuant to the Planned Development authorized by the terms of this ordinance.

SECTION 4: Except as otherwise provided for in this Ordinance 61-O-16, all applicable regulations of the Ordinance 19-O-15, the Zoning Ordinance, and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and/or provisions of any of said documents conflict with any of the terms herein, this Ordinance 61-O-16 shall govern and control.

SECTION 5: When necessary to effectuate the terms, conditions, and purposes of this ordinance, "Applicant" shall be read as "Applicant and its agents, assigns, and successors in interest" and shall mean 1571 Maple Avenue, LLC, and any and all successors, owners, and operators of the Subject Property.

SECTION 6: This ordinance shall be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

SECTION 7: Except as otherwise provided for in this ordinance, all applicable regulations of the Zoning Ordinance and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and provisions of any of said documents conflict with the terms herein, this ordinance shall govern and control.

SECTION 8: All ordinances or parts of ordinances that are in conflict with the terms of this ordinance are hereby repealed.

SECTION 9: If any provision of this ordinance or application thereof to any person or circumstance is ruled unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this ordinance is severable.

SECTION 10: The findings and recitals herein are hereby declared to be prima facie evidence of the law of the City and shall be received in evidence as provided by the Illinois Compiled Statutes and the courts of the State of Illinois.

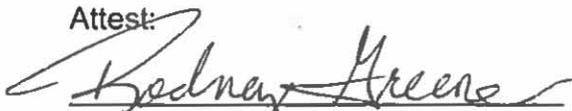
Ayes: 8

Nays: 1

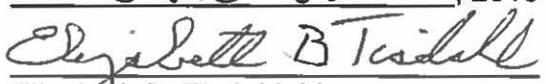
Introduced: May 23, 2016

Adopted: _____, 2016

Attest:


Rodney Greene, City Clerk

Approved:

June 22, 2016

Elizabeth B. Tisdahl, Mayor

Approved as to form:


Michelle Mason, Acting City Attorney
W. Grant Farrar, Corporation Counsel

EXHIBIT 1
ORDINANCE 19-O-15

4/7/2015
3/19/2015
1/27/2015

19-O-15

AN ORDINANCE

**Granting a Special Use Permit for a Planned Development
Located at 1571 Maple Avenue in the D3 Downtown Core
Development District**

WHEREAS, the City of Evanston is a home-rule municipality pursuant to Article VII of the Illinois Constitution of 1970; and

WHEREAS, as a home rule unit of government, the City has the authority to adopt ordinances and to promulgate rules and regulations that protect the public health, safety, and welfare of its residents; and

WHEREAS, Article VII, Section (6)a of the Illinois Constitution of 1970, states that the "powers and functions of home rule units shall be construed liberally," was written "with the intention that home rule units be given the broadest powers possible" (*Scadron v. City of Des Plaines*, 153 Ill.2d 164, 174-75 (1992)); and

WHEREAS, it is a well-established proposition under all applicable case law that the power to regulate land use through zoning regulations is a legitimate means of promoting the public health, safety, and welfare; and

WHEREAS, Division 13 of the Illinois Municipal Code (65 ILCS 5/11-13-1, *et seq.*) grants each municipality the power to establish zoning regulations; and

WHEREAS, pursuant to its home rule authority and the Illinois Municipal Code, the City has adopted a set of zoning regulations, set forth in Title 6 of the Evanston City Code of 2012, as amended, ("the Zoning Ordinance"); and

WHEREAS, 1571 Maple Avenue LLC ("Applicant"), the Applicant for the proposed development located at 1571 Maple Avenue, Evanston, Illinois (the "Subject Property"), legally described in Exhibit A, which is attached hereto and incorporated herein by reference, applied, pursuant to the provisions of the Zoning Ordinance, specifically Section 6-3-5, "Special Uses", Section 6-3-6, "Planned Developments", and Subsection 6-11-1-10, "Planned Developments" in Downtown Zoning Districts, to permit the construction and operation of a Planned Development with accessory parking located at the Subject Property in the D3 Downtown Core Development Zoning District ("D3 District"); and

WHEREAS, the Applicant sought approval to construct a new twelve (12) -story one hundred thirty-three and three tenths (133.3) foot tall mixed-use building consisting of up to one hundred one (101) residential units, with a floor area ratio of 4.8, approximately three thousand, six hundred ninety-six (3,696) gross square footage of commercial space and twelve (12) open on-site parking spaces; and

WHEREAS, construction of the Planned Development, as proposed in the application, requires exception from the strict application of the Zoning Ordinance with regards to the number of dwelling units, height, number of parking spaces provided, floor area ratio, ziggurat street side yard setback from the north property line along Davis Street, ziggurat front yard setback from the east property line along Elmwood Avenue, and ziggurat side yard setback from the northwest side property lines; and

WHEREAS, pursuant to Subsection 6-3-6-5 of the Zoning Ordinance, the City Council may grant Site Development Allowances from the normal district regulations established in the Zoning Ordinance; and

WHEREAS, on November 5, 2014, December 17, 2014, and January 14, 2015, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq.*) and the Zoning Ordinance, the Plan Commission held a public hearing on the application for a Special Use Permit for a Planned Development, case no. 14PLND-0118, heard extensive testimony and public comment, received other evidence, and made written minutes, findings, and recommendations; and

WHEREAS, the Plan Commission's written findings state that the application for the proposed Planned Development meets applicable standards set forth for Special Uses in Subsection 6-3-5-10 of the Zoning Ordinance and Planned Developments in the D3 Downtown Core Development District per Subsection 6-11-1-10 of the Zoning Ordinance; and

WHEREAS, on January 14, 2015, the Plan Commission recommended the City Council approve the application with conditions; and

WHEREAS, on March 9, 2015, the Planning and Development ("P&D") Committee of the City Council held a meeting, in compliance with the provisions of the Open Meetings Act and the Zoning Ordinance, received input from the public, carefully considered and adopted the findings and recommendations of the Plan Commission, and recommended approval thereof by the City Council; and

WHEREAS, at its meetings on March 9 and March 23, 2015, held in compliance with the Open Meetings Act and the Zoning Ordinance, the City Council

considered the recommendation of the P&D Committee, received additional public comment, made certain findings, and adopted said recommendation; and

WHEREAS, it is well-settled law that the legislative judgment of the City Council must be considered presumptively valid (see *Glenview State Bank v. Village of Deerfield*, 213 Ill.App.3d 747) and is not subject to courtroom fact-finding (see *National Paint & Coating Ass'n v. City of Chicago*, 45 F.3d 1124),

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS, THAT:

SECTION 1: The foregoing recitals are hereby found as facts and incorporated herein by reference.

SECTION 2: Pursuant to the terms and conditions of this ordinance, the City Council hereby grants the Special Use Permit applied for in case no. 14PLND-0118, to allow construction and operation of the Planned Development for a twelve (12)-story one hundred thirty-three and three tenths (133.3) foot tall mixed-use building consisting of up to one hundred one (101) residential units, with a floor area ratio of 4.8, approximately three thousand, six hundred ninety-six (3,696) gross square footage of commercial space and twelve (12) open on-site parking spaces.

SECTION 3: The City Council hereby grants the following Site Development Allowances:

- (A) Number of Dwelling Units:** A Site Development Allowance is hereby granted for one hundred one (101) residential dwelling units, whereas subsection 6-11-4-4(B) of the Zoning Ordinance allows for a maximum of seventy three (73) residential dwelling units in the D3 District.
- (B) Height:** A Site Development Allowance is hereby granted for a building height of one hundred thirty-three and three tenths (133.3) feet, whereas subsection 6-11-

4-8 of the Zoning Ordinance allows for a maximum building height of eighty-five (85) feet in the D3 District.

- (C) **Number of Parking Spaces:** A Site Development Allowance is hereby granted for a total of twelve (12) on-site parking spaces, whereas subsection 6-16-3-5 of the Zoning Ordinance requires a minimum of one hundred forty two (142) parking spaces for the proposed Planned Development in the D3 District.
- (D) **Floor Area Ratio ("FAR"):** A Site Development Allowance is hereby granted for an FAR of 4.8, whereas subsection 6-11-4-6 of the Zoning Ordinance requires a maximum FAR of 4.5 in the D3 District.
- (E) **Ziggurat Street Side Yard Setback from the North Property Line Along Davis Street:** A Site Development Allowance is hereby granted for a ziggurat setback of twenty-four (24) feet at a height of thirty seven and three tenths (37.3) feet, whereas subsection 6-11-1-4 of the Zoning Ordinance requires a ziggurat setback of forty (40) feet for a structure above forty two (42) feet along Davis Street.
- (F) **Ziggurat Front Yard Setback from the East Property Line Along Elmwood Avenue:** A Site Development Allowance is hereby granted for a ziggurat setback of four (4) feet at a height of thirty seven and three tenths (37.3) feet, whereas subsection 6-11-1-10(C)(1)(c) of the Zoning Ordinance requires a ziggurat setback of thirty (30) feet for a structure above forty two (42) feet from any front lot line or side lot line abutting a street in the D3 District.
- (G) **Ziggurat Side Yard Setback from the Northwest Side Property Lines:** A Site Development Allowance is hereby granted for a ziggurat setback of nine and nine tenths (9.9) feet at a height of thirty seven and three tenths (37.3) feet, whereas subsection 6-11-1-4 of the Zoning Ordinance requires a ziggurat setback of twenty-five (25) feet for a structure above forty two (42) feet from an interior side lot line in the D3 District.

SECTION 4: Pursuant to Subsection 6-3-5-12 of the Zoning Ordinance, the City Council imposes the following conditions on the Special Use Permit granted hereby, which may be amended by future ordinance(s), and violation of any of which shall constitute grounds for penalties or revocation of said Special Use Permit pursuant to Subsections 6-3-10-5 and 6-3-10-6 of the Zoning Ordinance:

- (A) **Compliance with Applicable Requirements:** *The Applicant shall develop and operate the Planned Development authorized by the terms of this ordinance in substantial compliance with the following: the terms of this ordinance; the Site*

and Landscape Plans in Exhibit B and C, attached hereto and incorporated herein by reference; all applicable City Code requirements; the Applicant's testimony and representations to the Site Plan and Appearance Review Committee, the Plan Commission, the P&D Committee, and the City Council; and the approved documents on file in this case.

- (B) **Construction Management Plan:** The Applicant shall sign and agree to a Construction Management Plan (CMP) with the City of Evanston prior to issuance of the Building Permit. The CMP shall include but is not limited to the following: construction staging plan, on-street and on-site construction parking restrictions, hours of operation, a plan including cross sections showing pedestrian access around the site with the use of curb ramps, signage and/or striping, foundation survey of surrounding structures including weekly reporting of seismographs for the duration of construction, submittal of environmental testing report prior to construction, visibility diagram for all construction site access points, proposed schedule for street opening for utility connections with cross section details, and project updates via monthly newsletter and project website.
- (C) **On-Site Parking Spaces:** The on-site parking spaces must be available to the public for short term use with a maximum two (2) hour time limit. The public parking must be available between the hours of 10:00 a.m. and 5:00 p.m. on any given Monday through Friday. The on-site management company must manage the parking lot and arrange for any violators not in compliance with the parking restrictions to be towed.
- (D) **Maple Avenue Signage:** A "Public Parking" Sign must be installed near the parking entrance at Maple Avenue.
- (E) **On-Site Electric Charging Station:** One on-site electric charging station must be installed and available to the public and be free of charge.
- (F) **Mechanical Equipment Located on the Roof:** The Applicant agrees to install sound-abating fences or enclosures around the mechanical equipment area on the roof of the Planned Development.
- (G) **Landscaping on Elmwood Avenue:** Applicant must install and maintain the landscaping materials on the east side of Elmwood Avenue along the railroad embankment directly across from the Subject Property, as depicted in Exhibit C.
- (H) **Maintenance Plan:** Applicant must provide a three (3) year maintenance plan for the landscaping materials installed on the green roofs prior to issuance of a building permit by the City of Evanston.
- (I) **On-Site Car Share Spaces:** Two on-site car share spaces must be available through an arrangement with a common third party commercial car-share company. Applicant must also fully subsidize one car share membership per unit for all residential units.

- (J) **Bicycle Parking Facilities:** The Applicant must install a minimum of sixteen (16) reverse "U"-shaped bicycle parking facilities near the intersection of Maple Avenue and Elmwood Avenue for public use.
- (K) **Sidewalk Streetscape Work:** All sidewalk streetscape work must be constructed of concrete with a brick paver band at the curb in accordance with the downtown streetscape standards.
- (L) **Glass Exteriors:** The Applicant must either demonstrate that the external materials will be of a bird-safe nature or install bird-safe finishes to the glass exteriors.
- (M) **Loading Space:** One parking space within the on-site parking lot must be designated a short-term loading space for the Residents.
- (N) **Landscape Design:** The Applicant shall install and maintain all landscaping materials as depicted in Exhibit C.
- (O) **Streetscape Improvements:** The Applicant shall construct the streetscape improvements inclusive of new street trees along Elmwood Avenue and Maple Avenue per proposed development plans and landscape plans in Exhibit B and Exhibit C.
- (P) **Affordable Housing Contribution:** The Applicant shall pay a one-time contribution of four hundred thousand dollars (\$400,000) to the City's Affordable Housing Fund. The contribution will be made in two (2) installments. The first installment shall be made within ten (10) business days of the issuance of the Final Certificate of Occupancy (FCO) and the second installment shall be made within one (1) year of the FCO issuance date.
- (Q) **Affordable Housing in the Development:** The Applicant shall provide two (2) one (1) bedroom on-site affordable housing units (with a goal of one (1) one-bedroom unit and one (1) two-bedroom unit) to households earning at or below one hundred percent (100%) of Area Median Income (AMI). The units provided shall be equal in size to the market-rate units within the building. The period of affordability for the units shall be for ten (10) years. The Applicant must submit a compliance report by January 31st of each year to the Housing and Grants Division of the Community Development Department showing the following: (1) unit number; (2) number of bedrooms; (3) tenant name; (4) number of persons in each affordable household unit; (5) annual gross income of each household occupying each affordable housing unit; (6) date of income certification; and (7) monthly unit rent. The compliance report must also include the list of any utilities included in rent.
- (R) **Divvy Sponsorship:** The Applicant shall pay a one-time Divvy sponsorship contribution in the amount of fifty six thousand dollars (\$56,000).

- (S) **City of Evanston Employment:** The Applicant agrees to employ at least five (5) Evanston residents, with a goal of ten (10) Evanston residents, during construction.
- (T) **Commercial Space:** The Applicant agrees to incorporate the commercial space along Davis Street to enhance the commercial and pedestrian character of the area per development plans in Exhibit B.
- (U) **LEED Silver Certification:** The Applicant agrees to comply with the City of Evanston Green Building Ordinance and obtain a LEED Silver Certification Rating or higher for the Planned Development on the Subject Property.
- (V) **Pervious Parking Lot:** The Applicant agrees to install a pervious parking lot on the Subject Property.
- (W) **Green Roof Construction:** The Applicant shall construct multiple green roofs as depicted in the development plans in Exhibit B and landscape plans in Exhibit C.
- (X) **Landscaped Seating Areas:** The Applicant agrees to install two landscaped seating areas along Maple Avenue per landscape plans in Exhibit C
- (Y) **Easement:** The Applicant agrees to prepare and record an easement for a six-foot wide area along the north edge of the on-site parking lot for the use of commercial properties at the southeast corner of Maple Avenue and Davis Street to accommodate trash pick-up on Maple Avenue rather than Davis Street. A copy of the recorded easement document must be submitted prior to issuance of a building permit by the City of Evanston.
- (Z) **Parking Lease:** The Applicant must agree and sign a long-term parking lease agreement with the City of Evanston to lease one hundred one (101) parking spaces based on the standard current monthly parking fee from the Maple Avenue Parking garage located at 1800 Maple Avenue. The lease agreement will mandate that the Applicant pay any increases in the rental rate structure through the term of the lease agreement. The long-term lease agreement shall initially be set for a minimum period of seven (7) years. For the lifetime of the project, the Applicant must require all Residents to disclose their vehicle ownership and conduct periodic reviews to ensure that all vehicles owned by Residents of the building are accounted for within the Maple Avenue garage. The Applicant must provide the certified vehicle ownership report to the City of Evanston annually by January 31st of each calendar year during the first seven (7) year period from the issuance of the Final Certificate of Occupancy. The City of Evanston's Department of Administrative Services will monitor the Applicant's certified vehicle ownership reports and the costs incurred by the City of Evanston for such oversight shall be paid for by Applicant's parking lease fees. If at any time during this initial seven (7) year period such annual vehicle ownership report indicates that the Residents of the building own more than one hundred one

(101) cars and require more than one hundred one (101) parking spaces, the Applicant agrees to amend the parking lease agreement with the City and lease the additional parking spaces necessary. The Applicant also agrees to deny apartment leases to potential Residents who own vehicles until such time as the number of vehicles owned by the Residents of the building and required to park in the Maple Avenue garage by terms of this Ordinance falls below one hundred one (101) or until the surplus parking spaces can be accommodated in the revised lease agreement with the City of Evanston.

Following the seven (7) year anniversary of the initial parking lease agreement date, the parking lease agreement may be amended. The number of parking spaces leased from the City may be reduced to match the highest number of vehicles owned by the Residents and required to park in the Maple Avenue garage by the terms of this Ordinance in any year during the initial seven (7) year period per the annual parking reports. The number of parking spaces leased by the City may not be reduced in the first seven (7) years and any reduction after the seven (7) year anniversary shall be approved by the City Council as an amendment to the parking lease agreement.

Following the expiration of the seven (7) year anniversary of the parking lease agreement, the agreement can be modified every five years thereafter but not before, to match the highest number of vehicles owned by the Residents and required to park in the Maple Avenue garage by the terms of this Ordinance during any calendar year in the preceding five (5) year term per the annual parking report. Any amendments to the number of parking spaces leased from the City of Evanston or any other amendments to the lease agreement, including term extensions, shall be approved by the City Council as an amendment to the parking lease agreement.

The Applicant must hold a valid long-term parking lease agreement with the City of Evanston for the lifetime of the project unless this condition is amended by the City Council of the City of Evanston as an amendment to the Planned Development.

- (AA) **South Elevation of Development:** Applicant will exercise reasonable commercial efforts to work with City staff and the Winthrop Club Condominium Association to modify the south elevation to maximize privacy for the residents in condominium units which will face the development and to minimize the impact on their building located at 1570 Elmwood Avenue.
- (BB) **Construction Schedule:** Pursuant to Subsection 6-11-1-10(A)4 of the Zoning Ordinance, the Applicant shall obtain a building permit within twelve (12) months of the passing of this Ordinance. Additionally, the Applicant must complete the construction of this Planned Development within twenty-four (24) months from the date the Applicant receives its building permit.

(CC) Recordation: Pursuant to Subsection 6-3-6-10 of the Zoning Ordinance, the Applicant shall, at its cost, record a certified copy of this ordinance, including all exhibits attached hereto, with the Cook County Recorder of Deeds, and provide proof of such recordation to the City, before the City may issue any permits pursuant to the Planned Development authorized by the terms of this ordinance.

SECTION 5: When necessary to effectuate the terms, conditions, and purposes of this ordinance, "Applicant" shall be read as "Applicant's tenants, agents, assigns, and successors in interest."

SECTION 6: This ordinance shall be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

SECTION 7: Except as otherwise provided for in this ordinance, all applicable regulations of the Zoning Ordinance and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and provisions of any of said documents conflict with the terms herein, this ordinance shall govern and control.

SECTION 8: All ordinances or parts of ordinances that are in conflict with the terms of this ordinance are hereby repealed.

SECTION 9: If any provision of this ordinance or application thereof to any person or circumstance is ruled unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this ordinance is severable.

SECTION 10: The findings and recitals herein are hereby declared to be prima facie evidence of the law of the City and shall be received in evidence as provided by the Illinois Compiled Statutes and the courts of the State of Illinois.

Introduced: March 23, 2015

Approved:

Adopted: April 13, 2015

April 15, 2015

Elizabeth B. Tisdahl
Elizabeth B. Tisdahl, Mayor

Attest: Rodney Greene

Approved as to form:

Rodney Greene, City Clerk (RB)

W. Grant Farrar
W. Grant Farrar, Corporation Counsel

EXHIBIT A**Legal Description**

PARCEL 1: THE SOUTH 50 FEET OF THE NORTH 200 FEET OF THE WEST 120 FEET OF BLOCK 63 IN EVANSTON (EXCEPT SO MUCH OF SAID PREMISES, IF ANY, WHICH LIES NORTH OF THE SOUTH 300 FEET OF SAID BLOCK 63) TOGETHER WITH THE SOUTH 10 FEET OF THE NORTH 200 FEET OF SAID BLOCK 63 (EXCEPT THEREFROM THE WEST 120 FEET THEREOF AND EXCEPT THAT PART THEREOF DEDICATED FOR STREET PURPOSES); ALSO THAT PART OF SAID BLOCK 63, IF ANY, LYING BETWEEN THE NORTH 200 FEET AND THE SOUTH 250 FEET 1 ½ INCHES OF SAID BLOCK 63 MEASURED ALONG THE WEST LINE OF SAID BLOCK AND LYING WEST OF THE WESTERLY LINE OF ELMWOOD AVENUE ALL IN EVANSTON, A SUBDIVISION OF THE EAST HALF OF THE SOUTHEAST QUARTER OF SECTION 13, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND PART OF SECTION 18, TOWNSHIP 41 NORTH, RANGE 14, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 2: THE NORTH 150 FEET OF THAT PART OF BLOCK 63 IN EVANSTON WHICH LIES WEST OF THE RIGHT OF WAY OF THE CHICAGO AND MILWAUKEE RAILROAD (KNOWN AS THE CHICAGO AND NORTHWESTERN RAILROAD) (EXCEPT THEREFROM THE WEST 83 ½ FEET AND EXCEPT THAT PART THEREOF DEDICATED FOR STREET PURPOSES) SAID BLOCK 63 BEING SITUATED IN THE SOUTHWEST QUARTER OF SECTION 18, TOWNSHIP 41 NORTH, RANGE 14, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS

PARCEL 3: THE SOUTH 40 FEET OF THE NORTH 190 FEET OF THAT PART OF BLOCK 63 IN EVANSTON WHICH LIES WEST OF THE RIGHT OF WAY OF THE CHICAGO AND NORTHWESTERN RAILROAD COMPANY (EXCEPT THEREFROM THE WEST 120 FEET THEREOF AND EXCEPT THAT PART THEREOF DEDICATED FOR STREET PURPOSES) SAID BLOCK 63 BEING SITUATED IN THE SOUTHWEST QUARTER OF SECTION 18, TOWNSHIP 41 NORTH, RANGE 14, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 4: THE WEST 83 ½ FEET OF THE SOUTH 20 FEET OF THE NORTH 150 FEET OF BLOCK 63 IN THE VILLAGE OF EVANSTON, BEING A SUBDIVISION OF PARTS OF SECTION 13, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, AND IN SECTION 18, TOWNSHIP 41 NORTH, RANGE 14, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 5: THE EAST 20 FEET 10 INCHES OF THE WEST 83 FEET 5 ½ INCHES OF THE NORTH 130 FEET OF BLOCK 63 IN THE VILLAGE OF EVANSTON IN SECTION 18, TOWNSHIP 41 NORTH, RANGE 14, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PINs: 11-18-310-004-0000
11-18-310-006-0000
11-18-310-007-0000
11-18-310-008-0000
11-18-310-019-0000
11-18-310-020-0000

COMMONLY KNOWN As: 1571 Maple Avenue, Evanston, IL 60201

EXHIBIT B

Development Plans

ALTA/ACSM LAND TITLE SURVEY

Legal Description:

Parcel 1: The South 50 feet of the North 200 feet of the West 120 feet of Block 63 in Evanston (except so much of said premises, if any, which lies North of the South 200 feet of said Block 63) together with the South 10 feet of the North 200 feet of said Block 63 (except therefrom the West 120 feet thereof and except that part thereof dedicated for street purposes); also that part of said Block 63, if any, lying between the North 200 feet and the South 250 feet of said Block 63 measured along the West line of said Block and lying West of the West line of Erwood Avenue in Evanston, a subdivision of the East half of the Southeast quarter of Section 13, Township 41 North, Range 13, East of the Third Principal Meridian, and part of Section 18, Township 41 North, Range 14, East of the Third Principal Meridian, in Cook County, Illinois.

NOTE:

Area = 22,112 sq. ft.

Bearings are assumed, based on the East line of Maple Avenue being due North-South.

Flood Zone Designation:

Per Flood Insurance Rate Map No. 17031C0270 J, Effective date August 19, 2003.

The Surveyed Property is located in Zone "X", an area determined to be outside of the 0.2% annual chance floodplain.

Reference:

For matters of title we have relied on Chicago Title Insurance Company's Commitment for Title Insurance, Order No. 1410 NW1700685 MI, Effective Date December 27, 2013, for all recorded documents affecting the described parcels.

Note: Due to excessive snow and ice cover at the time field work was performed, some ground features and utilities may not be shown.

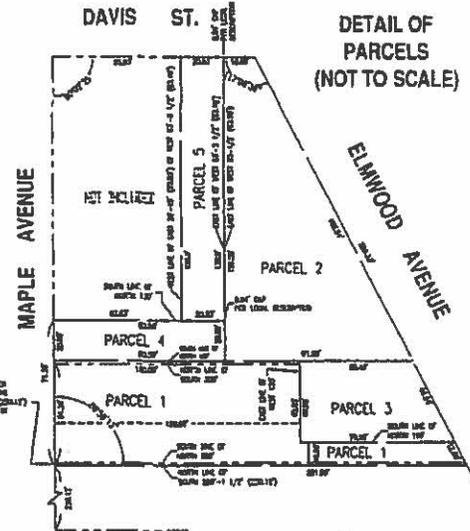
Parcel 2: The North 150 feet of that part of Block 63 in Evanston which lies West of the right of way of the Chicago and Milwaukee Railroad (known as the Chicago and Northwestern Railroad) (except therefrom the West 63 1/2 feet and except that part thereof dedicated for street purposes) said Block 63 being situated in the Southwest quarter of Section 18, Township 41 North, Range 14, East of the Third Principal Meridian, in Cook County, Illinois.

Parcel 3: The South 40 feet of the North 190 feet of that part of Block 63 in Evanston which lies West of the right of way of the Chicago and Northwestern Railroad Company (except therefrom the West 120 feet thereof and except that part thereof dedicated for street purposes) said Block 63 being situated in the Southwest quarter of Section 18, Township 41 North, Range 14, East of the Third Principal Meridian, in Cook County, Illinois.

Parcel 4: The West 63 1/2 feet of the South 20 feet of the North 150 feet of Block 63 in the Village of Evanston, being a subdivision of parts of Section 13, Township 41 North, Range 13, East of the Third Principal Meridian, and in Section 18, Township 41 North, Range 14, East of the Third Principal Meridian, in Cook County, Illinois.

Parcel 5: The East 20 feet 10 inches of the West 63 feet 5 1/2 inches of the North 130 feet of Block 63 in the Village of Evanston in Section 18, Township 41 North, Range 14, East of the Third Principal Meridian, in Cook County, Illinois.

Commonly known as: 1571 Maple Avenue; Evanston, Illinois.



GENERAL NOTES:

All information provided to the surveyor is shown or noted hereon.

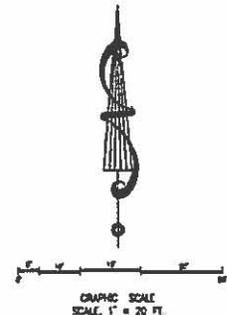
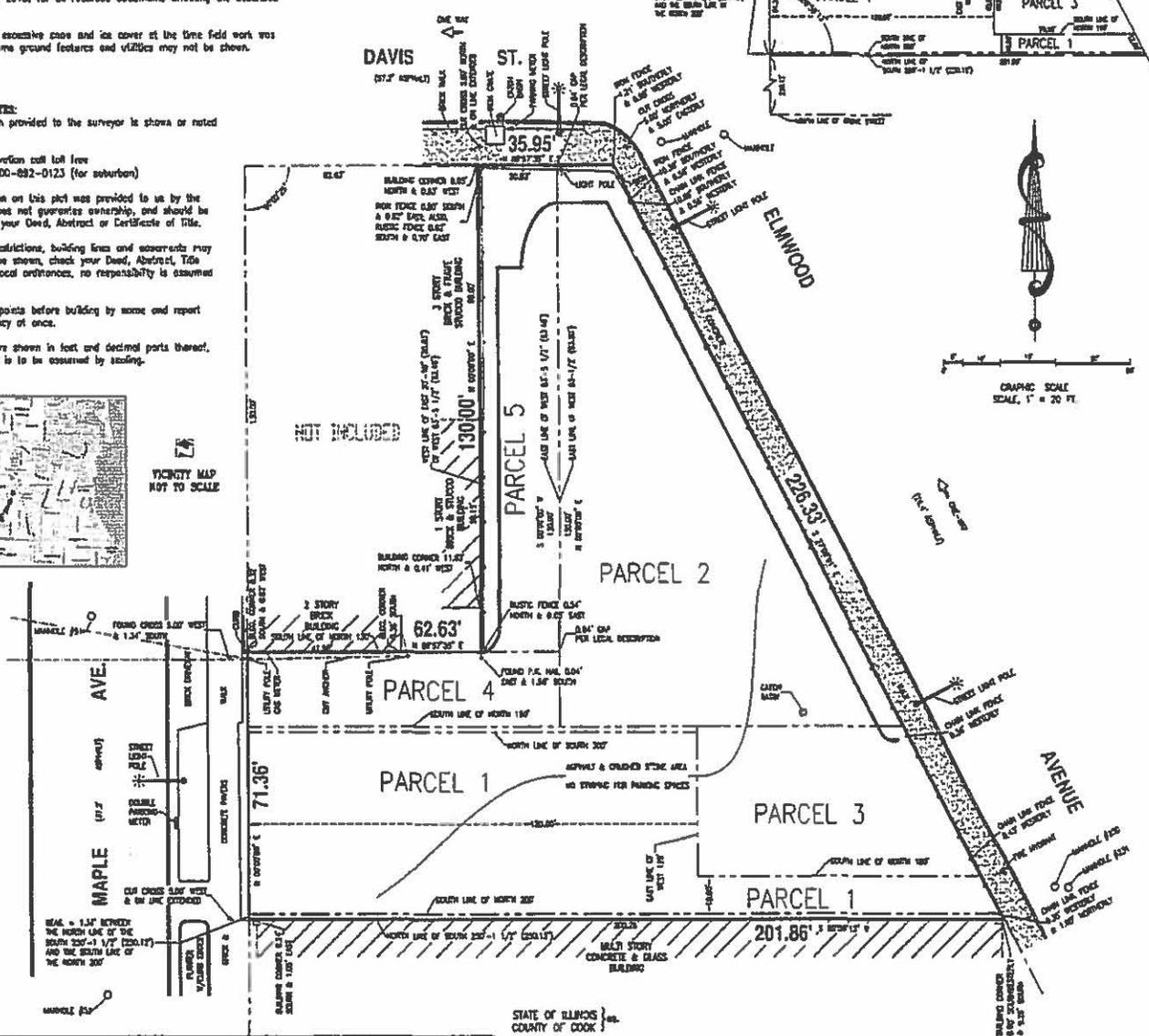
For information call toll free 714.741.1-1-800-882-0123 (for suburban)

The description on this plot was provided to us by the client, and does not guarantee ownership, and should be compared to your Deed, Abstract or Certificate of Title.

All building restrictions, building lines and easements may or may not be shown, check your Deed, Abstract, Title Report, and local ordinances, no responsibility is assumed by Surveyor.

Compare all points before building by name and report any discrepancy at once.

Dimensions are shown in feet and decimal parts thereof, no dimension is to be assumed by scaling.



B.H. SUHR & COMPANY, INC.

SURVEYING ESTABLISHED 1911

140 CUSTER AVENUE, EVANSTON, ILLINOIS 60201
 TEL: (847) 864-6315 / FAX: (847) 864-9341
 E-MAIL: SURVEY@BHSUHR.COM

Professional Design Firm
 License No. 134-028791

LOCATION: 1571 MAPLE AVENUE, EVANSTON, ILLINOIS, FEBRUARY 10, 2014

OFFICE No. 14-28, DESIGNED BY: CENTRUM PARTNERS, LLC

STATE OF ILLINOIS }
 COUNTY OF COOK }

To: Centrum Partners, LLC; Chicago Title Insurance Company.

This is to certify that this map or plot and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 7(a), 8, 11(a), and 14 of Table A thereof. The field work was completed on February 10, 2014.

Dated at Evanston, Ill., this _____ day of _____, A.D. 20____

PRELIMINARY 2/12/14

By: **Raymond A. Buehler**
 Illinois Professional Land Surveyor
 License Number 005-02844, Expiration Date 11/25/14



1571 Maple Avenue

E v a n s t o n , I l l i n o i s

26 September 2014

Centrum Partners LLC

Hirsch Associates LLC
Architecture + Planning



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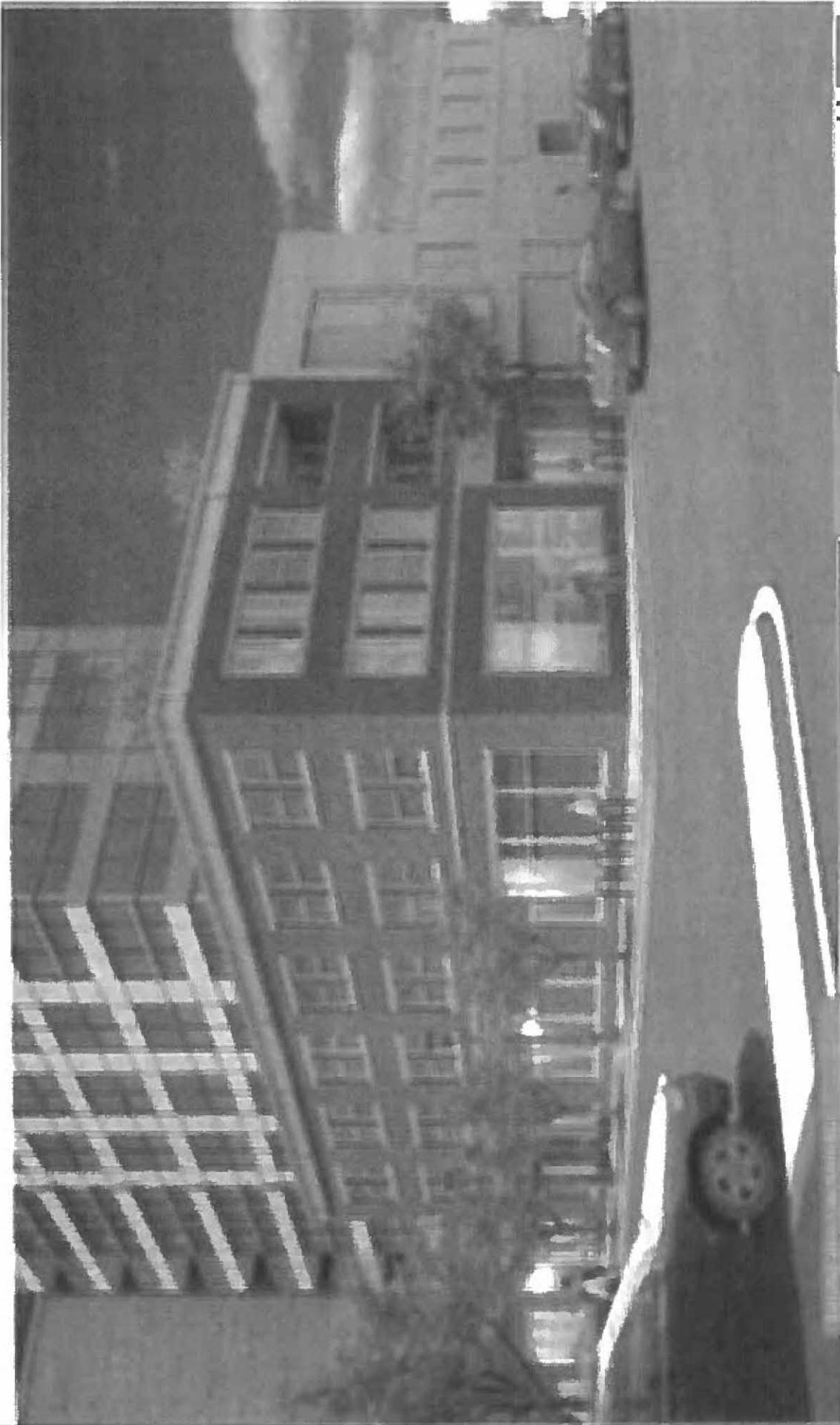
Hirsch Associates LLC
Architecture + Planning

1000 West 10th Street

1000 West 10th Street

1000 West 10th Street

1000 West 10th Street

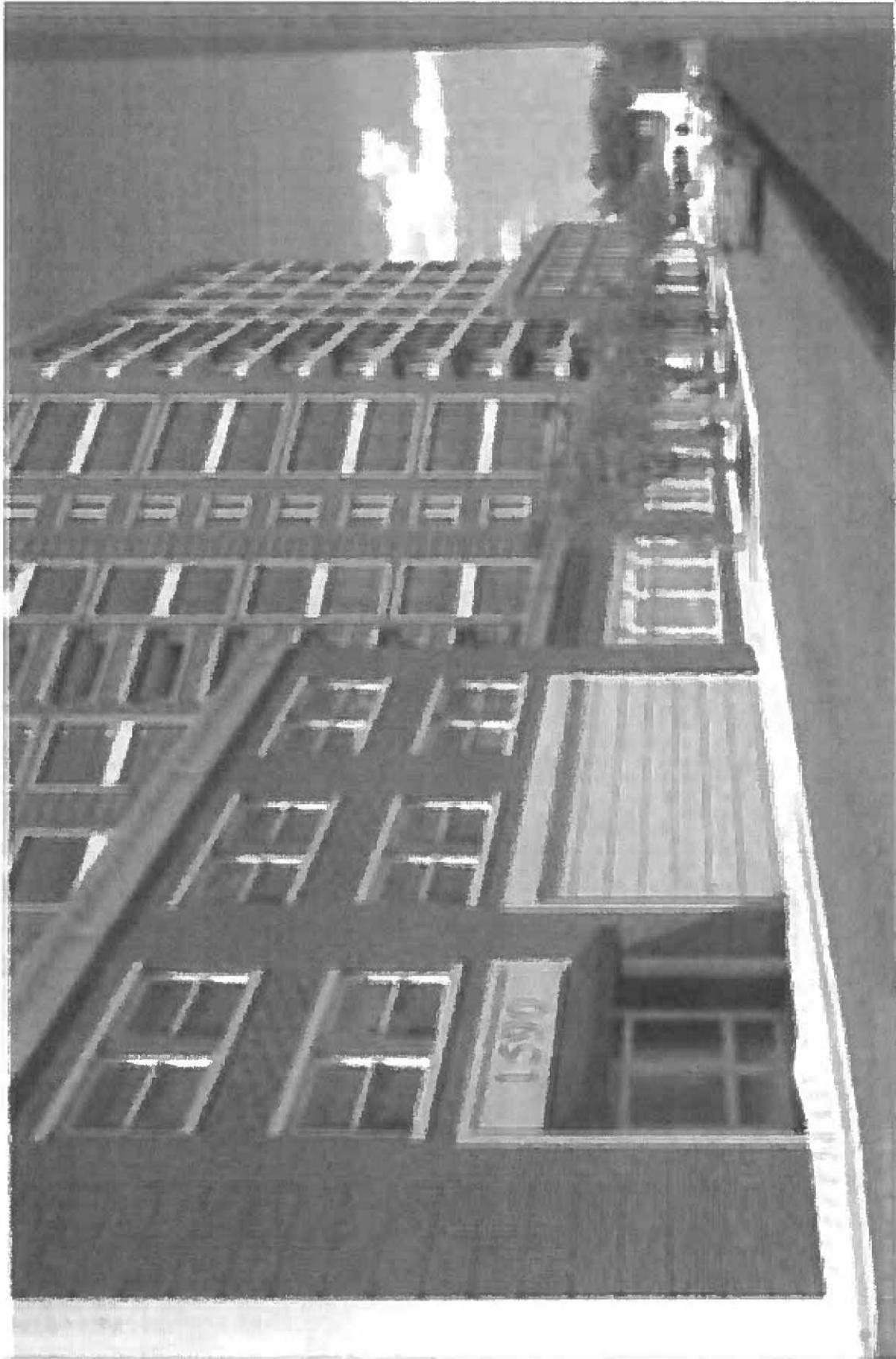


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Hirsch Associates LLC
Architecture + Planning

1234 Main Street
City, State, Zip

Contact: [Name]
Phone: [Number]



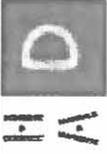
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Hirsch Associates LLC
Architecture + Planning

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Hirsch Associates LLC
Architecture + Planning

1775 MARINE AVENUE

Centrum Apartment LLC

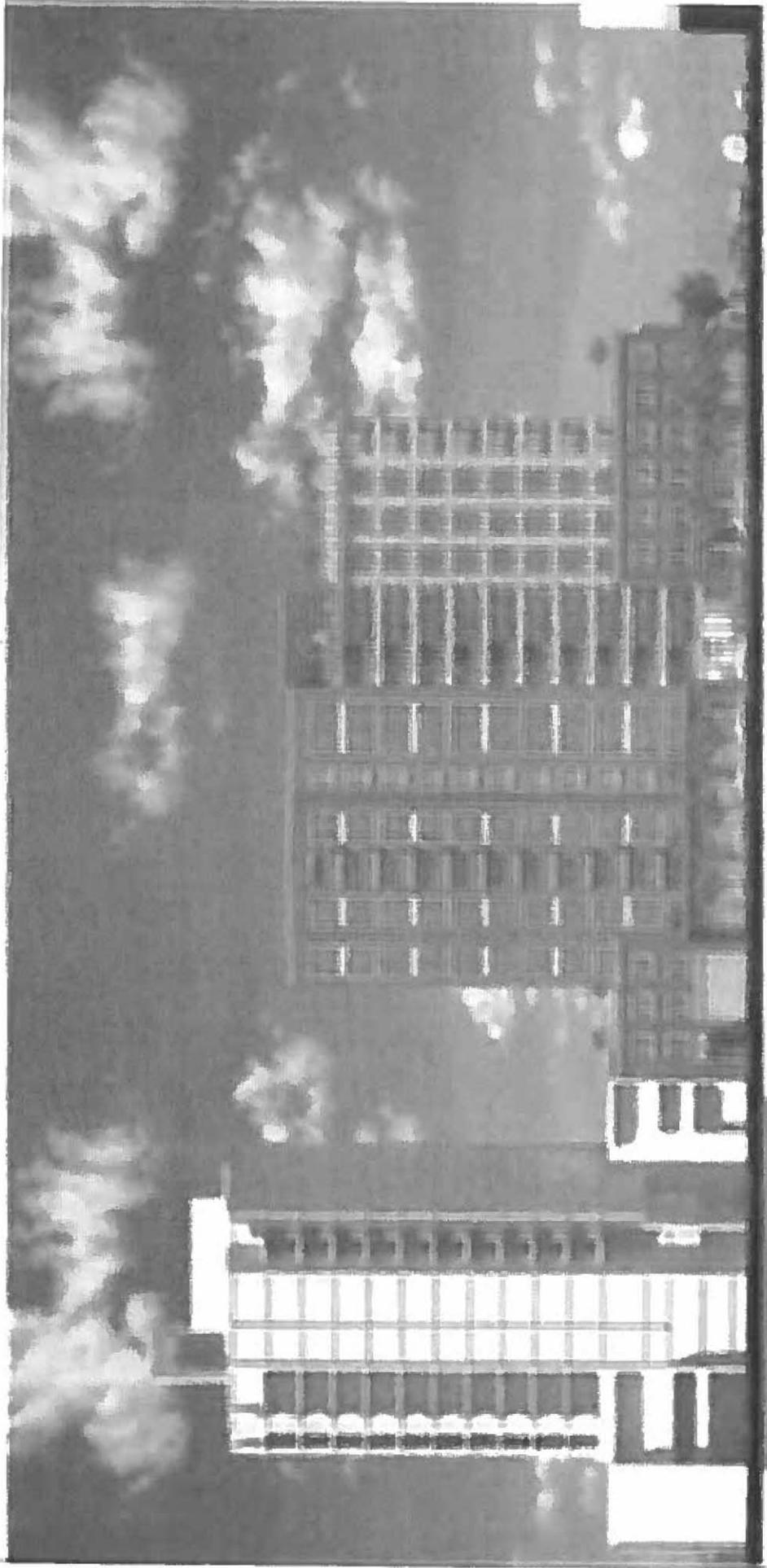


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Hirsch Associates LLC
Architecture + Planning

1271 S. W. 10th St. Suite 100
Miami, FL 33135

Cellular Ventures LLC



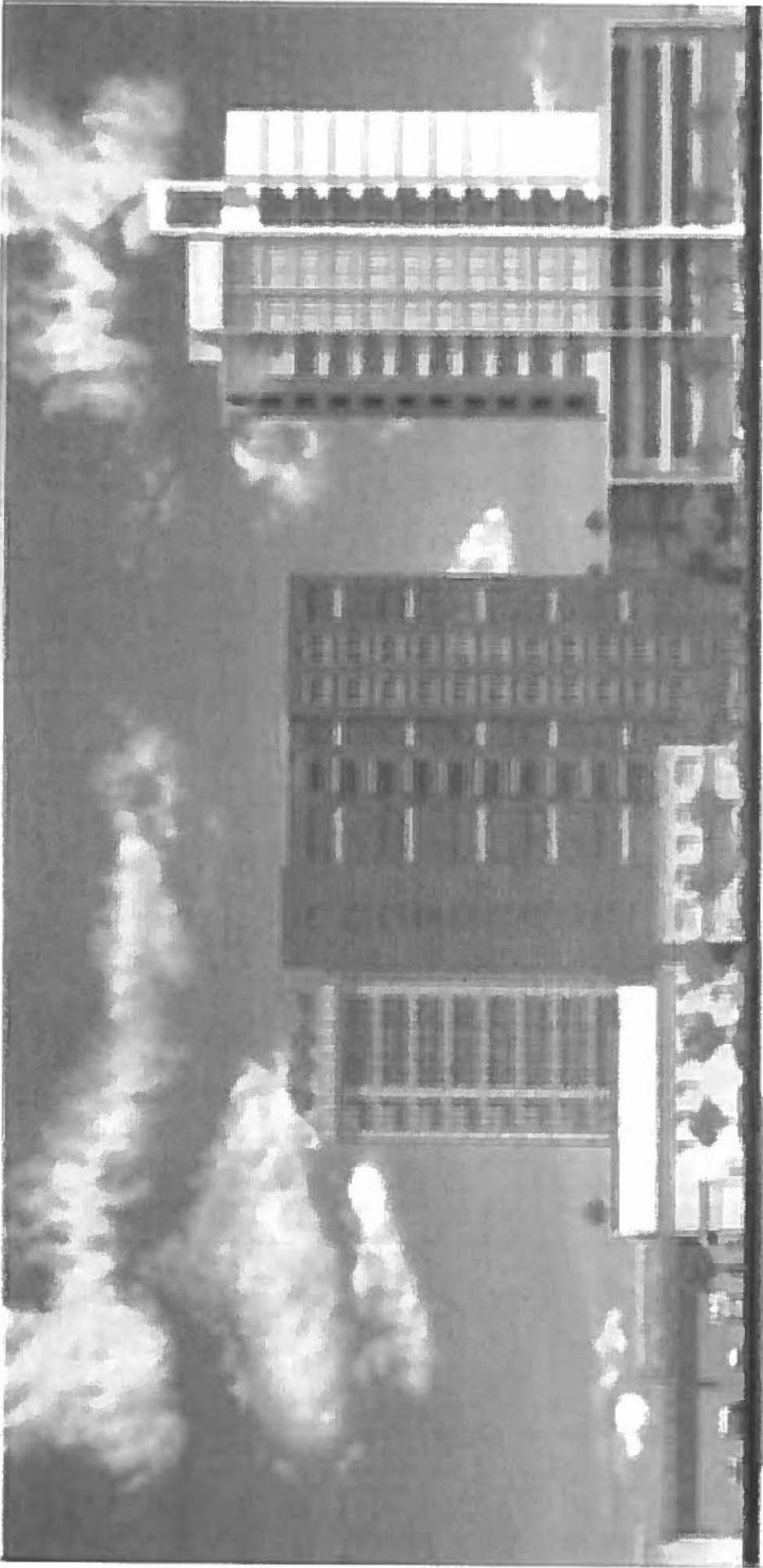
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Hirsch Associates LLC
Architecture + Planning

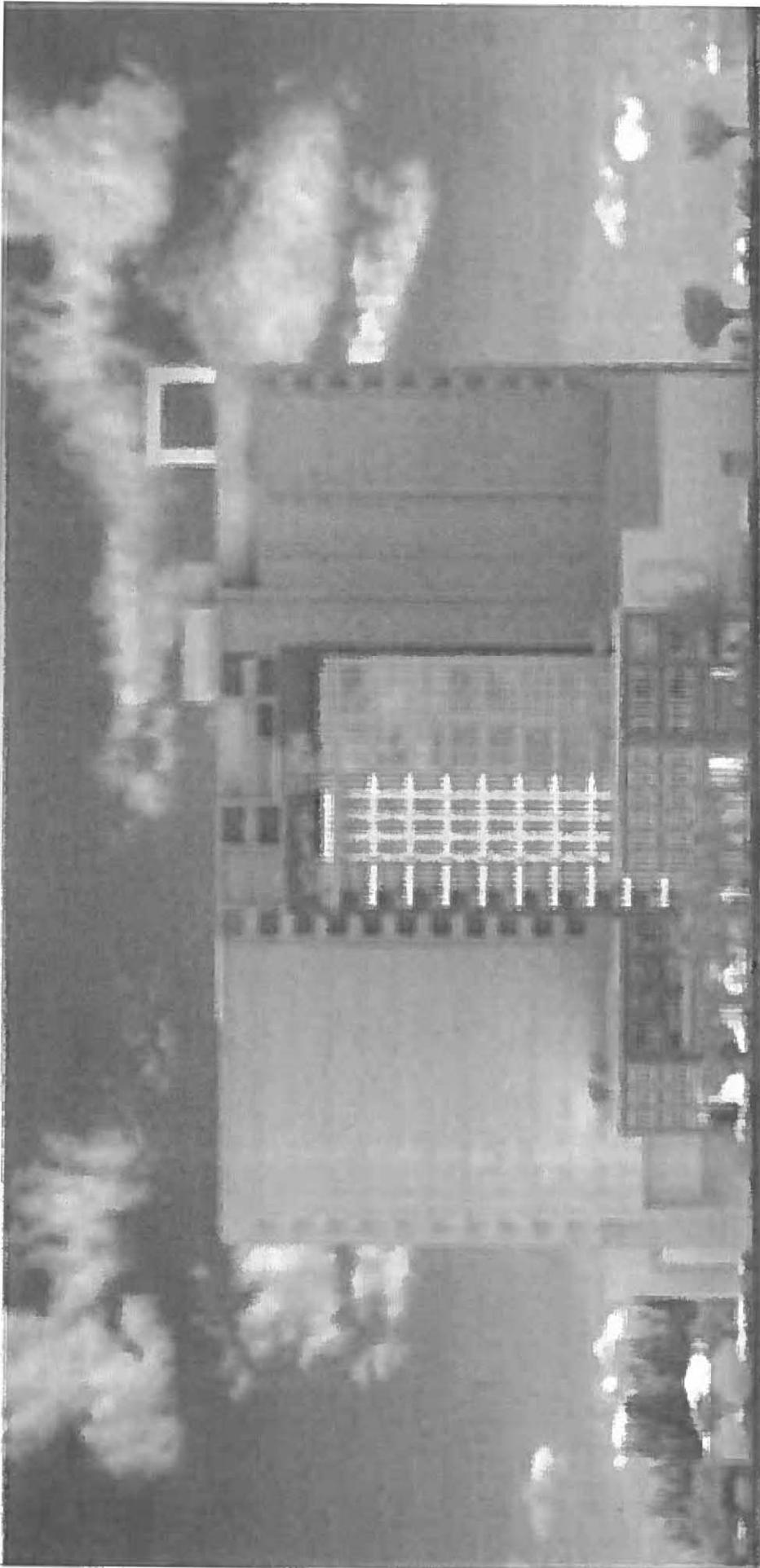
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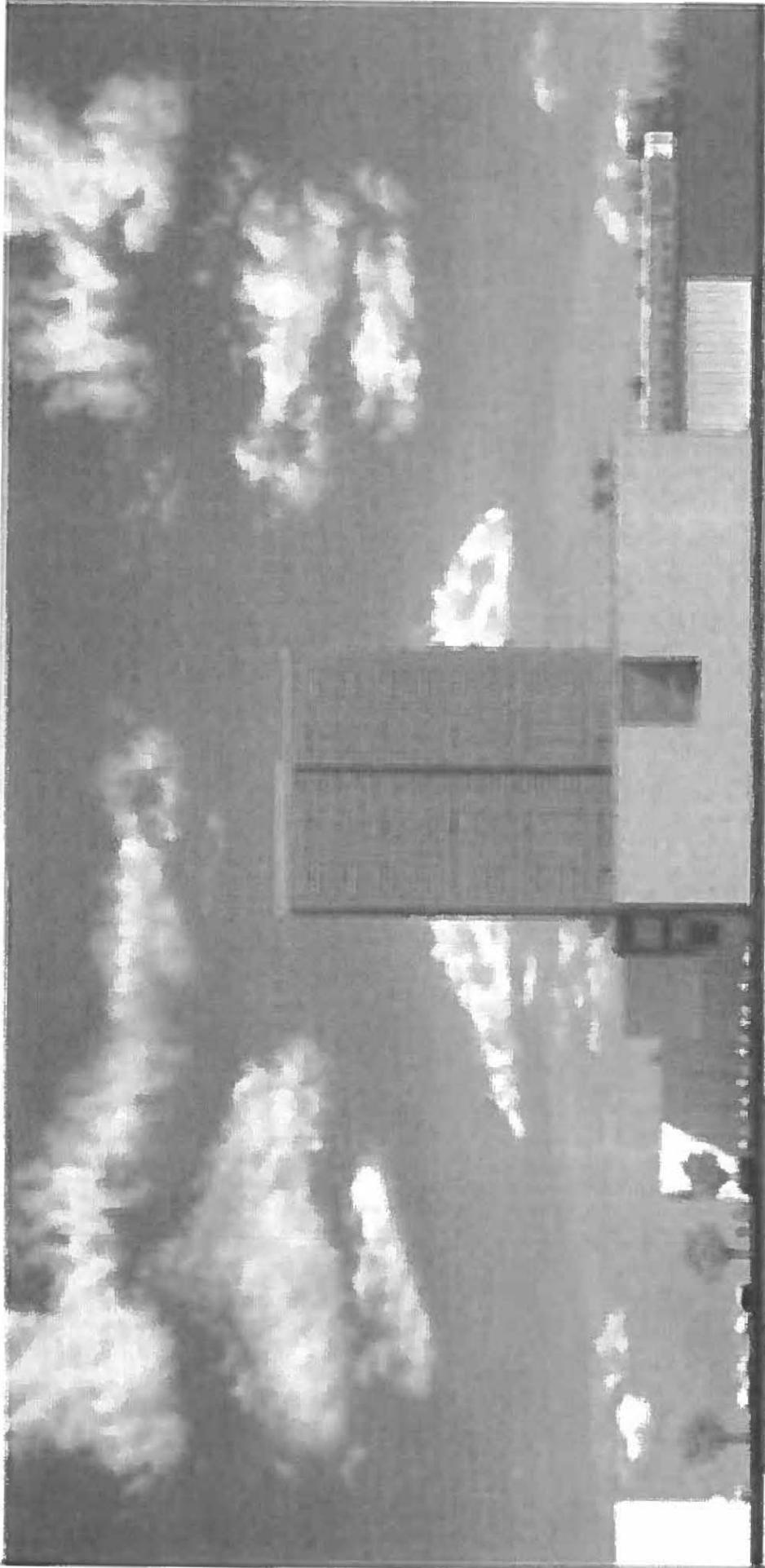
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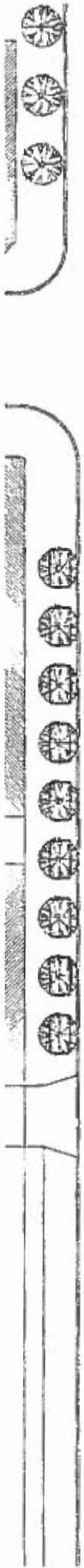
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12500 E. 1st Avenue, Suite 100
Denver, CO 80231
303.733.1111

California Partners LLC





MAPLE AVENUE

DAVIS STREET



0

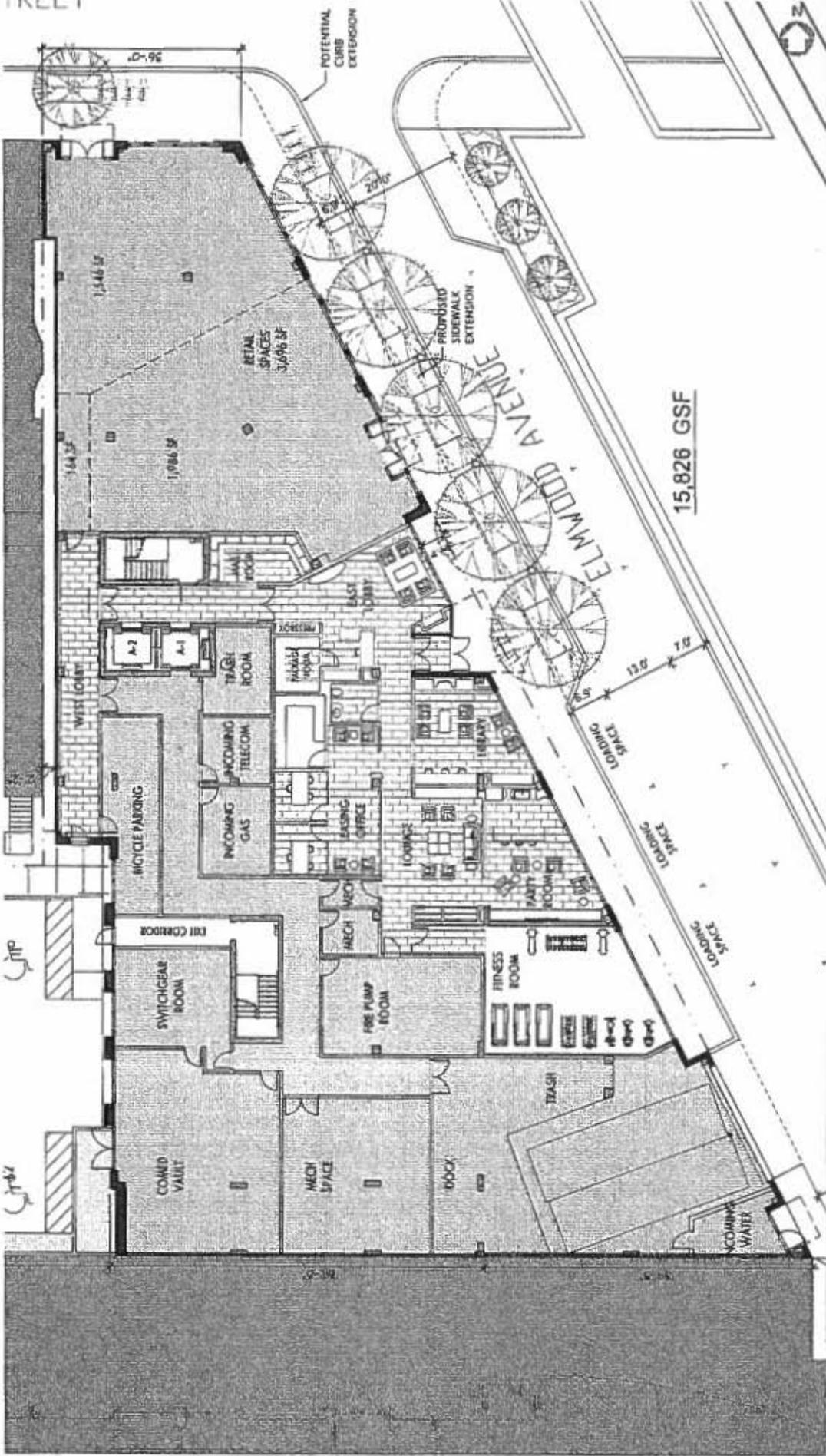


Hirsch Associates LLC
Architecture + Planning

Level 1 Overview Plan

Hirsch Associates LLC

INLET



Scale: 1/16" = 1'-0"

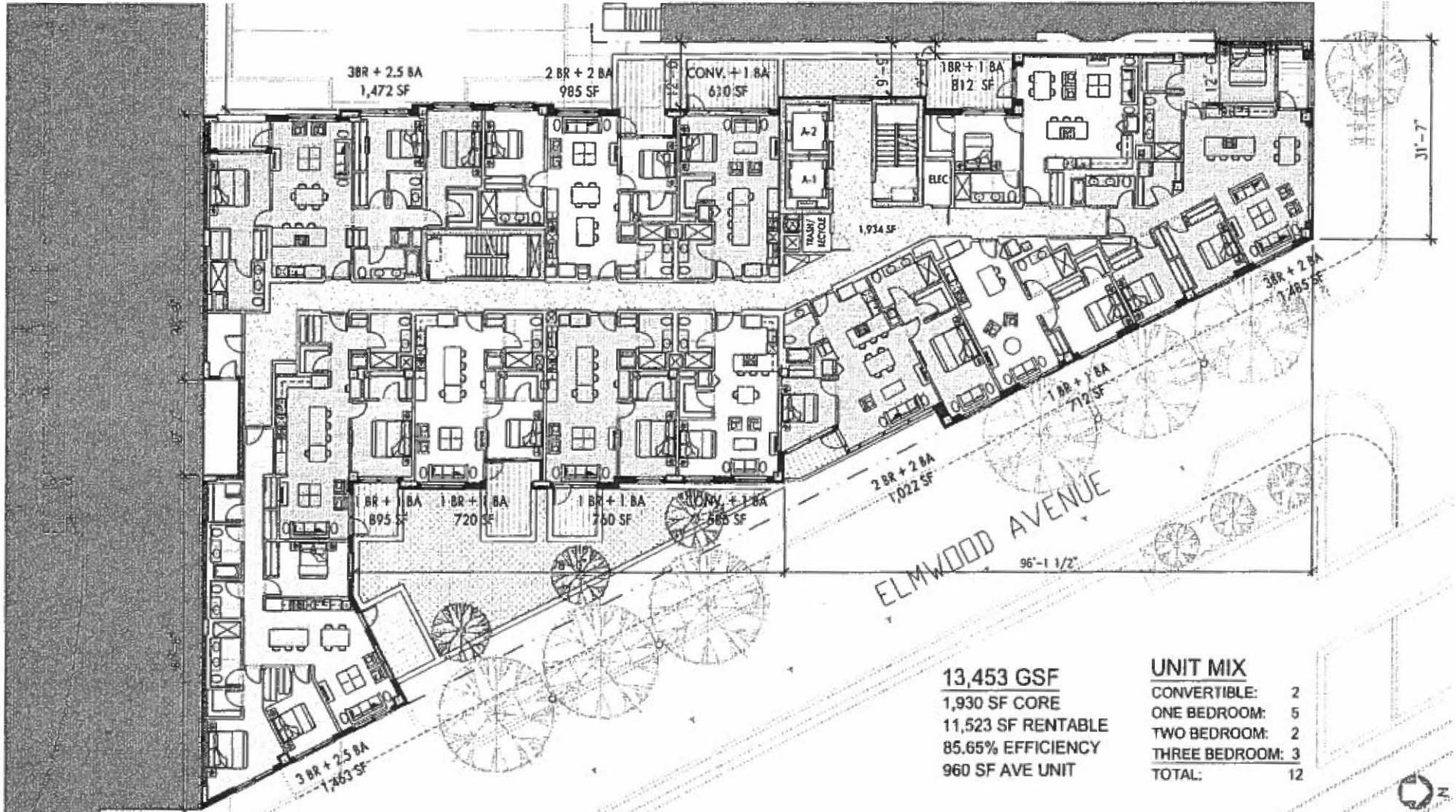


Hirsch Associates LLC
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Level 1 Floor Plan

1573 State Street

Chicago, Illinois, LLC



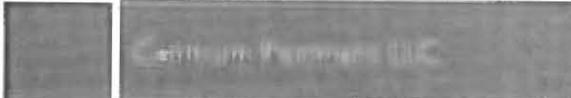
Level 2 Floor Plan

13,453 GSF
 1,930 SF CORE
 11,523 SF RENTABLE
 85.65% EFFICIENCY
 960 SF AVE UNIT

UNIT MIX

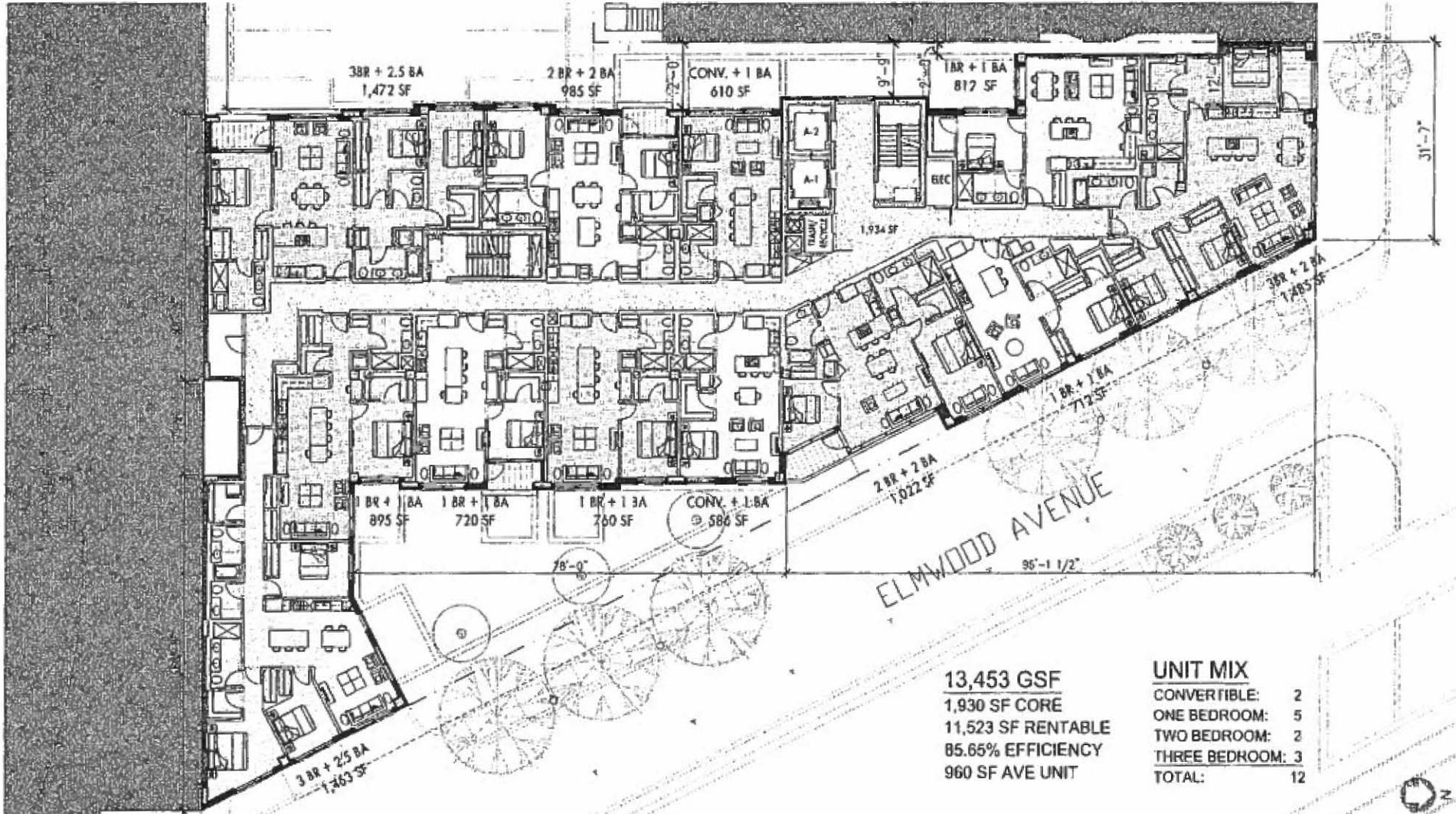
CONVERTIBLE:	2
ONE BEDROOM:	5
TWO BEDROOM:	2
THREE BEDROOM:	3
TOTAL:	12

Scale: 1/16" = 1'-0"



Hirsch Associates LLC
Architecture + Planning





13,453 GSF
 1,930 SF CORE
 11,523 SF RENTABLE
 85.65% EFFICIENCY
 960 SF AVE UNIT

UNIT MIX

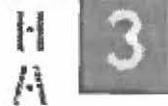
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ONE BEDROOM:	5
TWO BEDROOM:	2
THREE BEDROOM:	3
TOTAL:	12

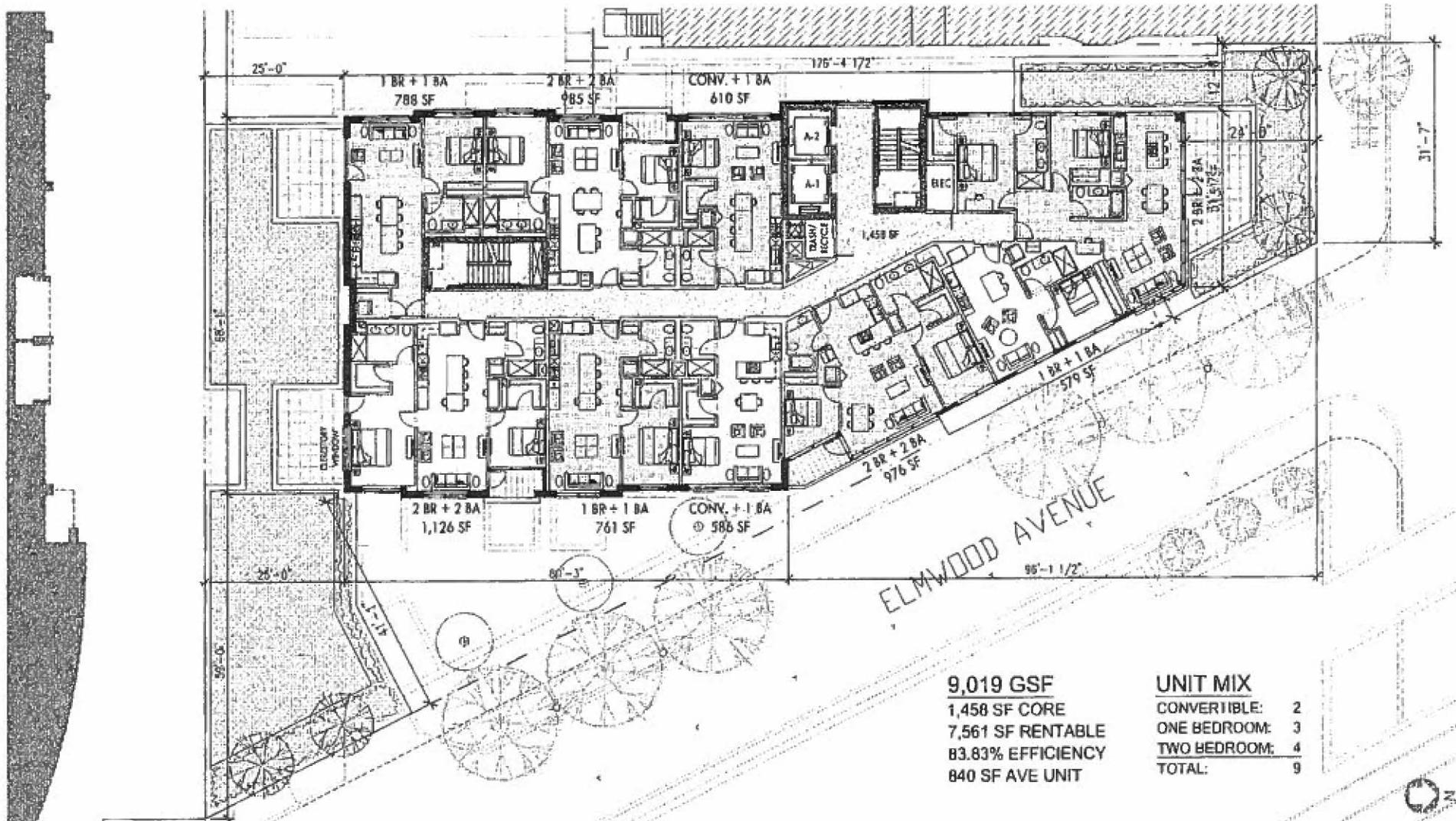
Scale 1/4" = 1'-0"

Capital Partners LLC
 1000 ...

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Hirsch Associates LLC
 Architecture + Planning





9,019 GSF
 1,458 SF CORE
 7,561 SF RENTABLE
 83.83% EFFICIENCY
 840 SF AVE UNIT

UNIT MIX

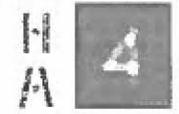
CONVERTIBLE:	2
ONE BEDROOM:	3
TWO BEDROOM:	4
TOTAL:	9

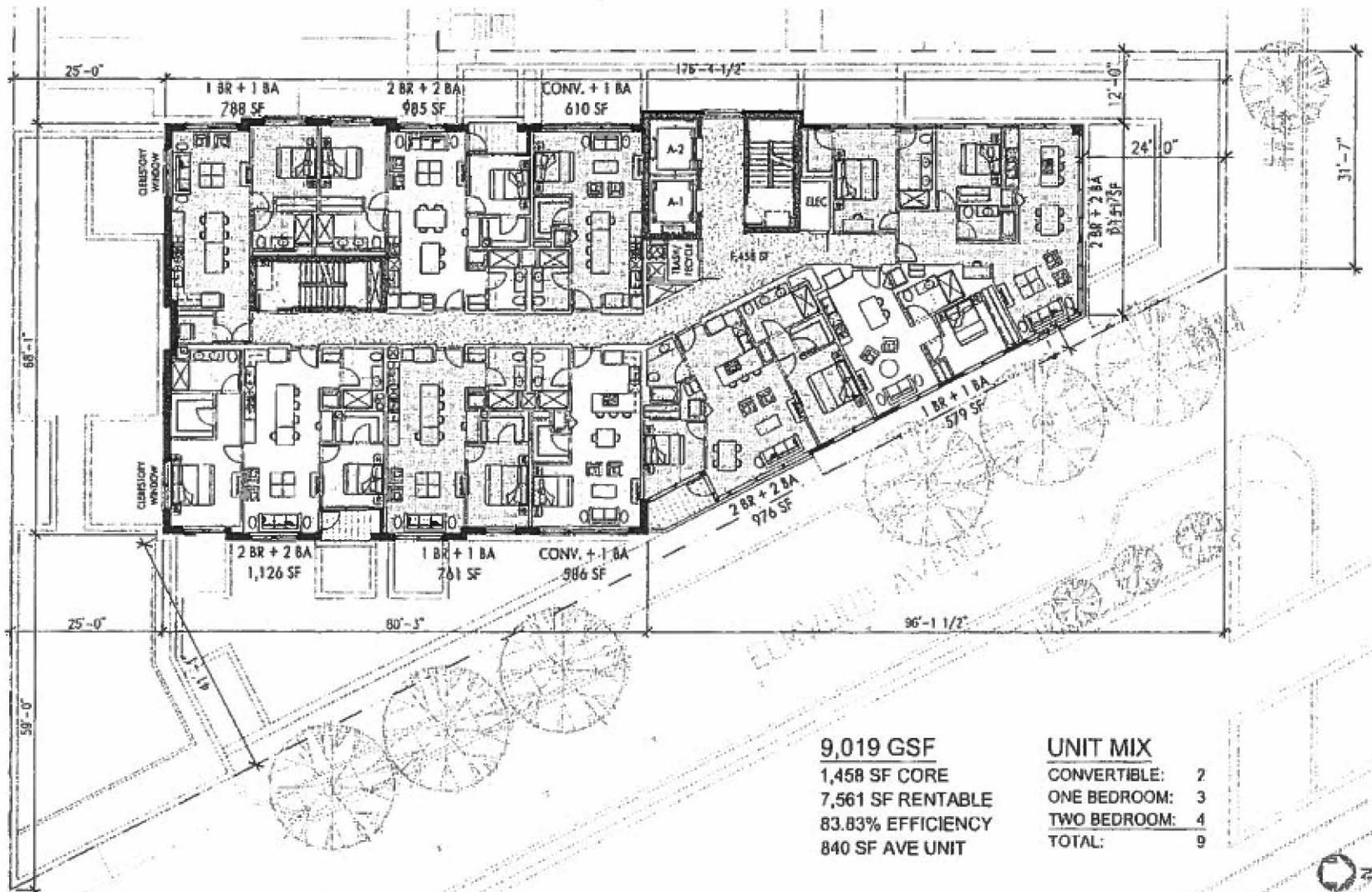
Level 4 Floor Plan

Scale: 1/8" = 1'-0"



Hirsch Associates LLC
 Architecture + Planning





9,019 GSF
 1,458 SF CORE
 7,561 SF RENTABLE
 83.83% EFFICIENCY
 840 SF AVE UNIT

UNIT MIX

CONVERTIBLE:	2
ONE BEDROOM:	3
TWO BEDROOM:	4
TOTAL:	9

Level 5 Floor Plan

Scale: 1/8" = 1'-0"

1001 E. 4th Street



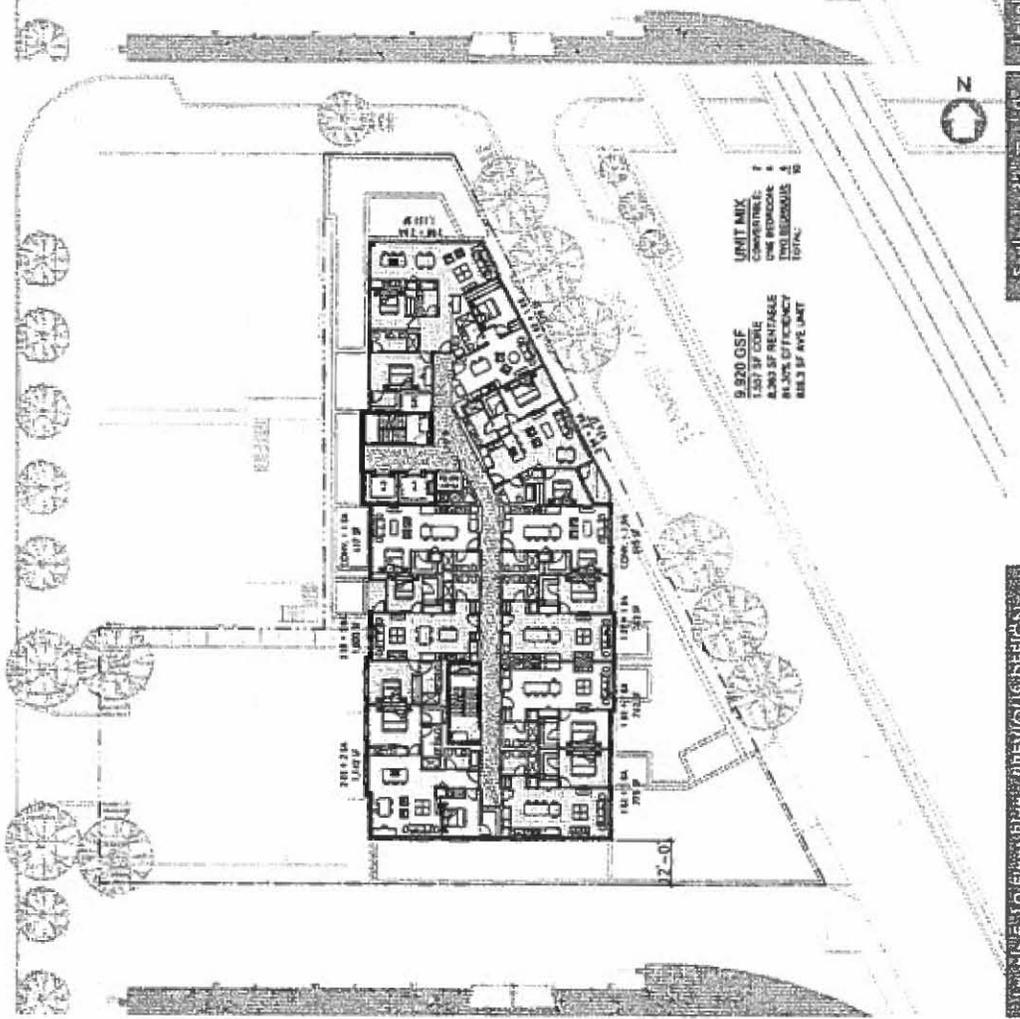
UNIT MIX
 1 UNIT PER FLOOR
 1 ONE BEDROOM
 2 TWO BEDROOMS
 1 TOTAL

9,019 GSF
 7,481 SF FINISHABLE
 1,538 SF PERMISSIBLE
 18.1% EFFICIENCY
 140 SF AVE UNIT

Scale: 1/32" = 1'-0"

Level 5 (11) Floor Plan - REVISED DESIGN

1001 E. 4th Street



UNIT MIX
 1 UNIT PER FLOOR
 1 ONE BEDROOM
 2 TWO BEDROOMS
 1 TOTAL

9,920 GSF
 7,481 SF FINISHABLE
 2,439 SF PERMISSIBLE
 18.1% EFFICIENCY
 140 SF AVE UNIT

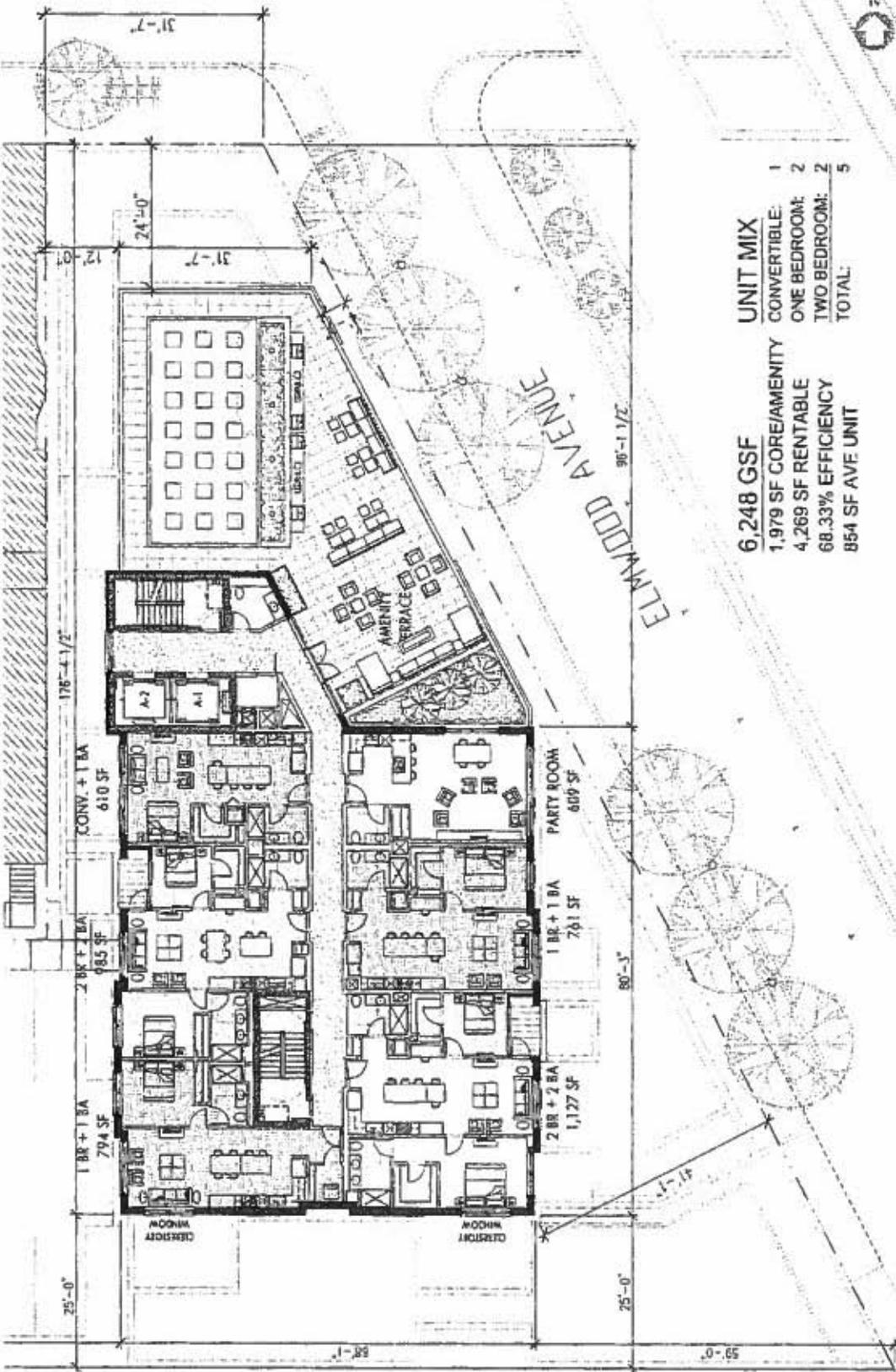
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Level 5 (10) Floor Plan - PREVIOUS DESIGN



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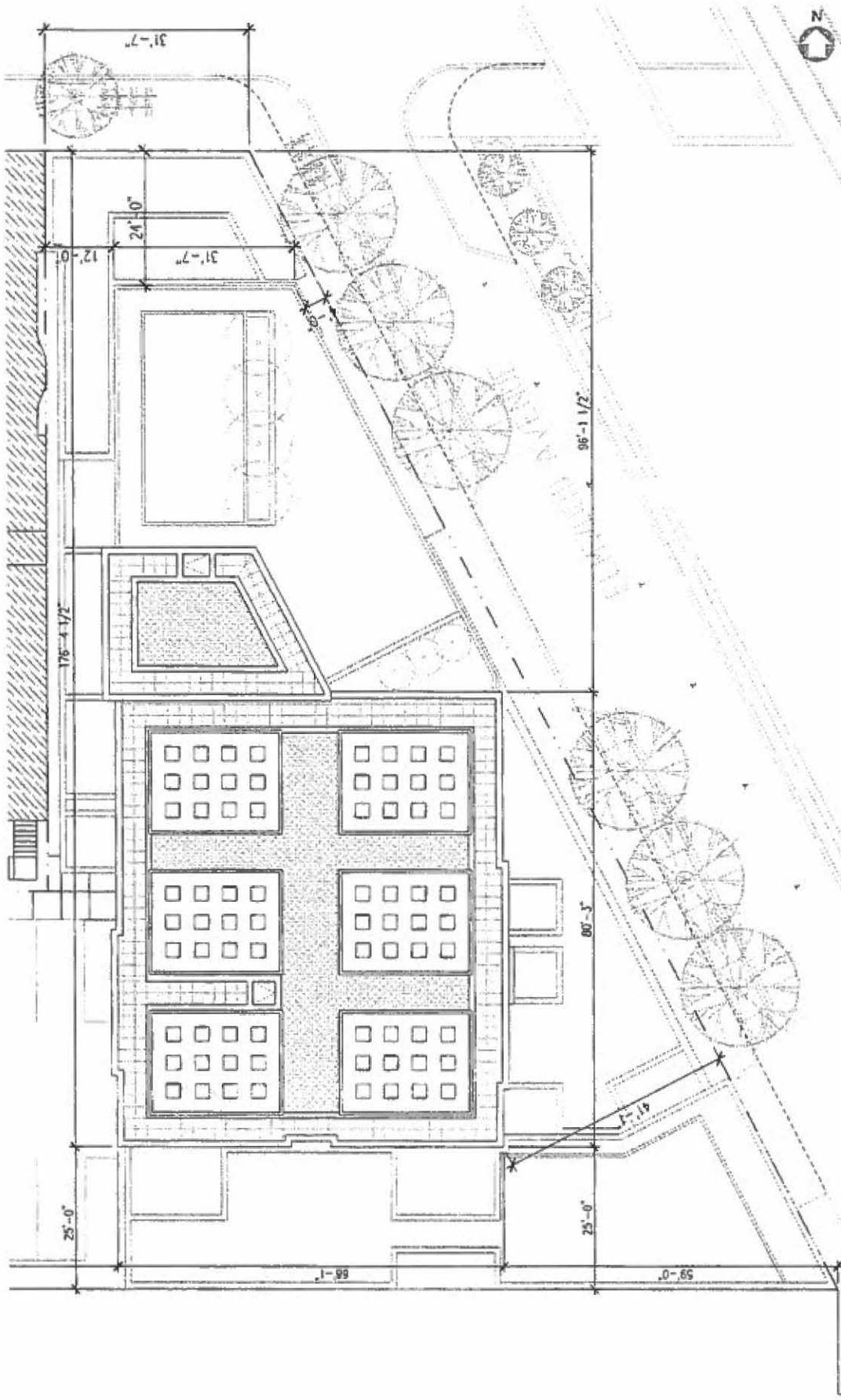




6,248 GSF

6,248 GSF	UNIT MIX
1,979 SF CORE/AMENITY	CONVERTIBLE: 1
4,269 SF RENTABLE	ONE BEDROOM: 2
68.33% EFFICIENCY	TWO BEDROOM: 2
854 SF AVE UNIT	TOTAL: 5





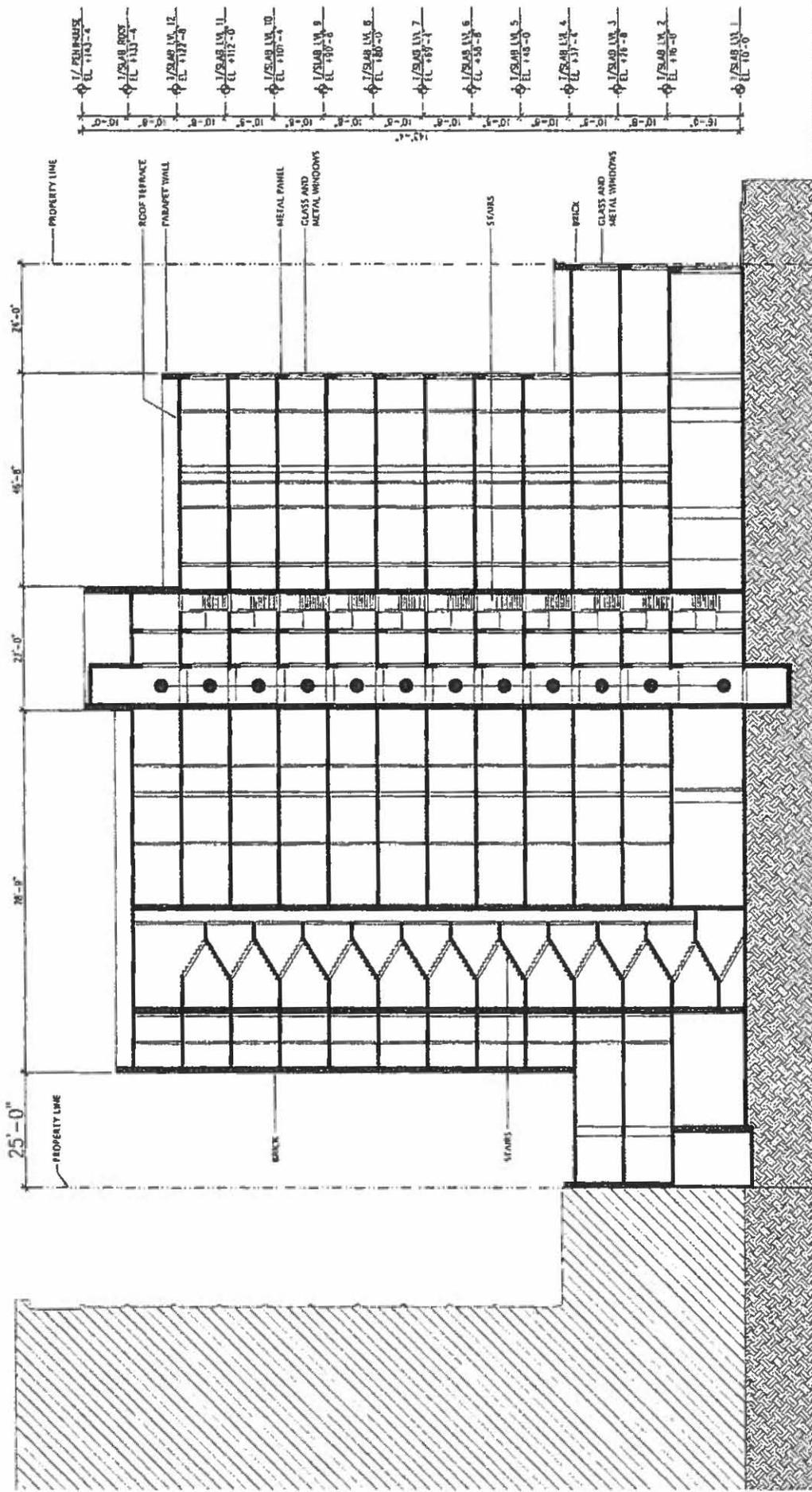
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Level: 3 Ref: Plan





North-South Building Section

8

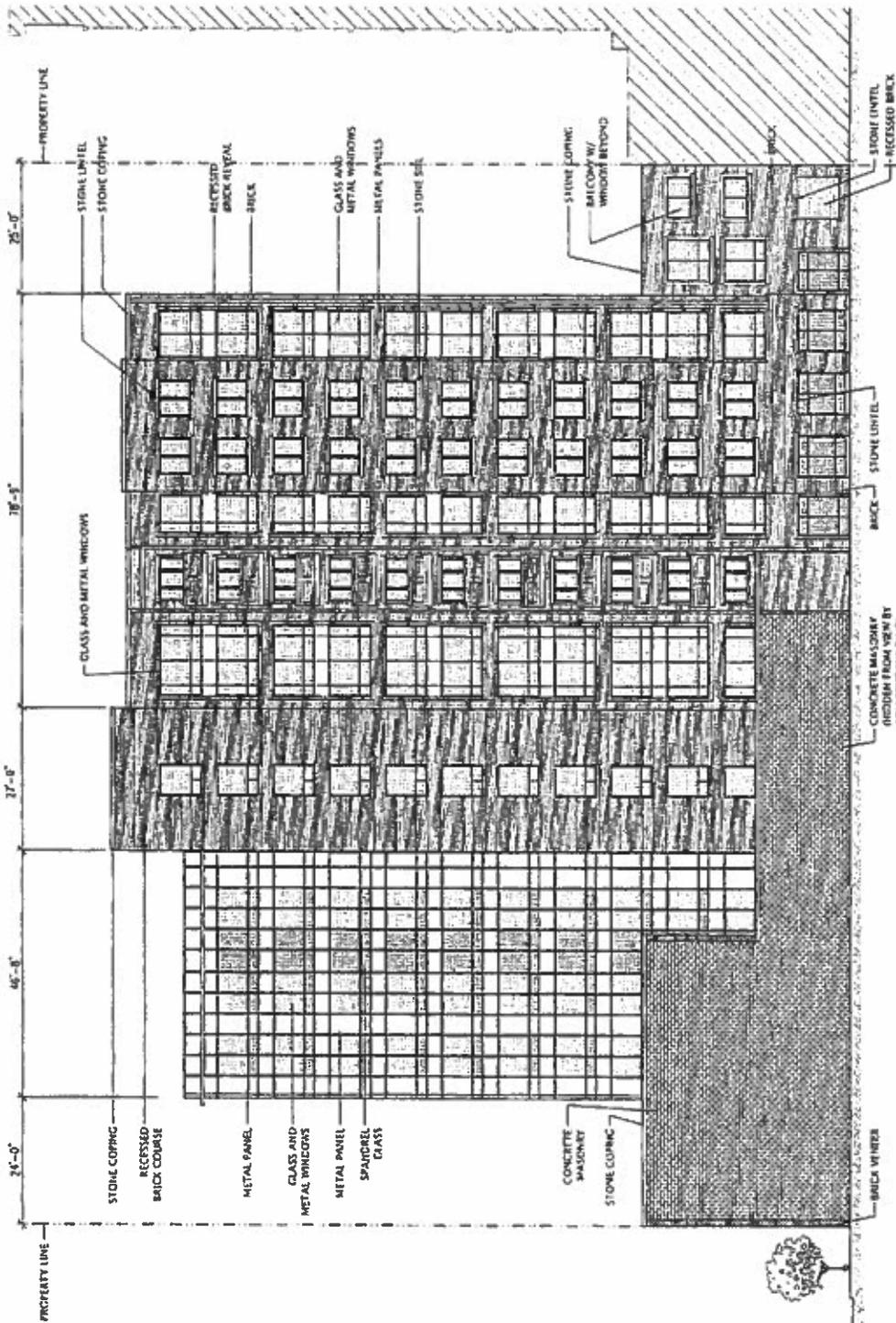
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Hirsch Associates LLC
Architecture + Planning

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PROPERTY LINE

36'-0"

46'-8"

37'-0"

18'-5"

25'-0"

PROPERTY LINE

STONE COPING

RECESSED BRICK COURSE

METAL PANEL

GLASS AND METAL WINDOWS

METAL PANEL

STONE SILL

CONCRETE MASONRY

STONE COPING

RECESSED BRICK

GLASS AND METAL WINDOWS

METAL PANELS

STONE SILL

STONE LINTEL

STONE COPING

RECESSED BRICK

GLASS AND METAL WINDOWS

METAL PANELS

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GLASS AND METAL WINDOWS

METAL PANELS

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EXHIBIT C

Landscape Plans



Perspective View - Edinwood Avenue

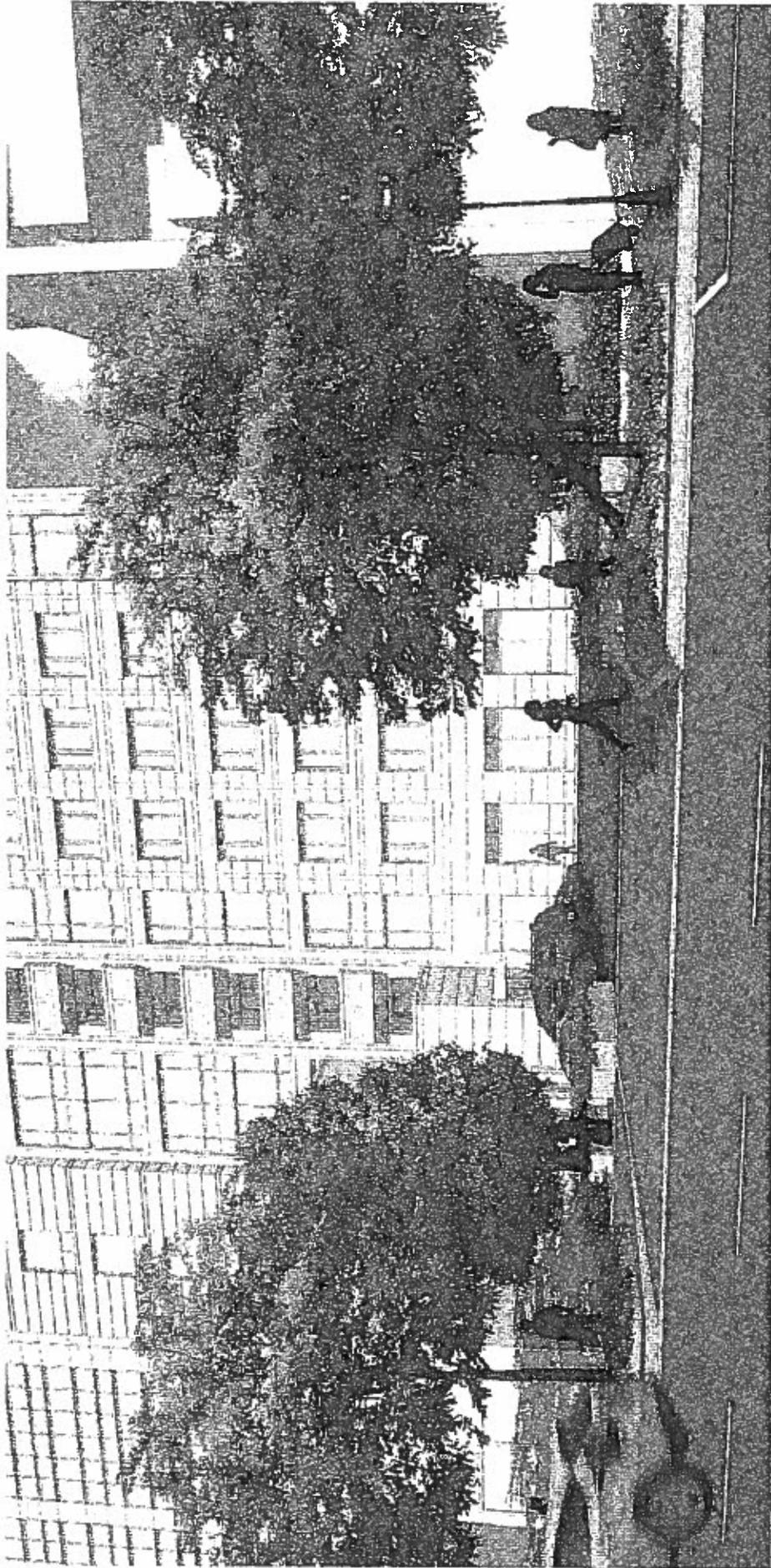
© 2011 Wolff Landscape Architecture

1000 University Avenue

1000

WOLFF LANDSCAPE ARCHITECTURE

1000



WOLFF LANDSCAPE ARCHITECTURE

MAPLE AVENUE

SPECIAL PAVING THROUGH DRIVEWAY

PARKLET AREA W/ ORN. TREE, SHRUB, PERENNIALS AND BENCH

2 GTS

STREET TREE IN RAISED PLANTER
PARKLET AREA W/ ORN. TREE, SHRUB, PERENNIALS AND BENCH

SHRUB HEDGE

PCC

DAVIS STREET

PCC (EX.)
BRICK PAVEMENT
NEW STREET
TREES IN 5X10'
TREE GRATE, TYP.
BIKE RACKS

OUTDOOR DINING
OPPORTUNITY

NEW ORNAMENTAL TREES
WITH MIXED PLANTING

3 PCC
EXISTING PARALLEL
PARKING SPACES

LAY-BY ZONE
FOR DROP-OFF

6 GTS

ELMWOOD AVENUE



MAPLE AVENUE AVENUE

DAVIS STREET

6" DEPTH GREEN ROOF AREA
W/ PERENNIALS AND ORNAMENTAL GRASSES

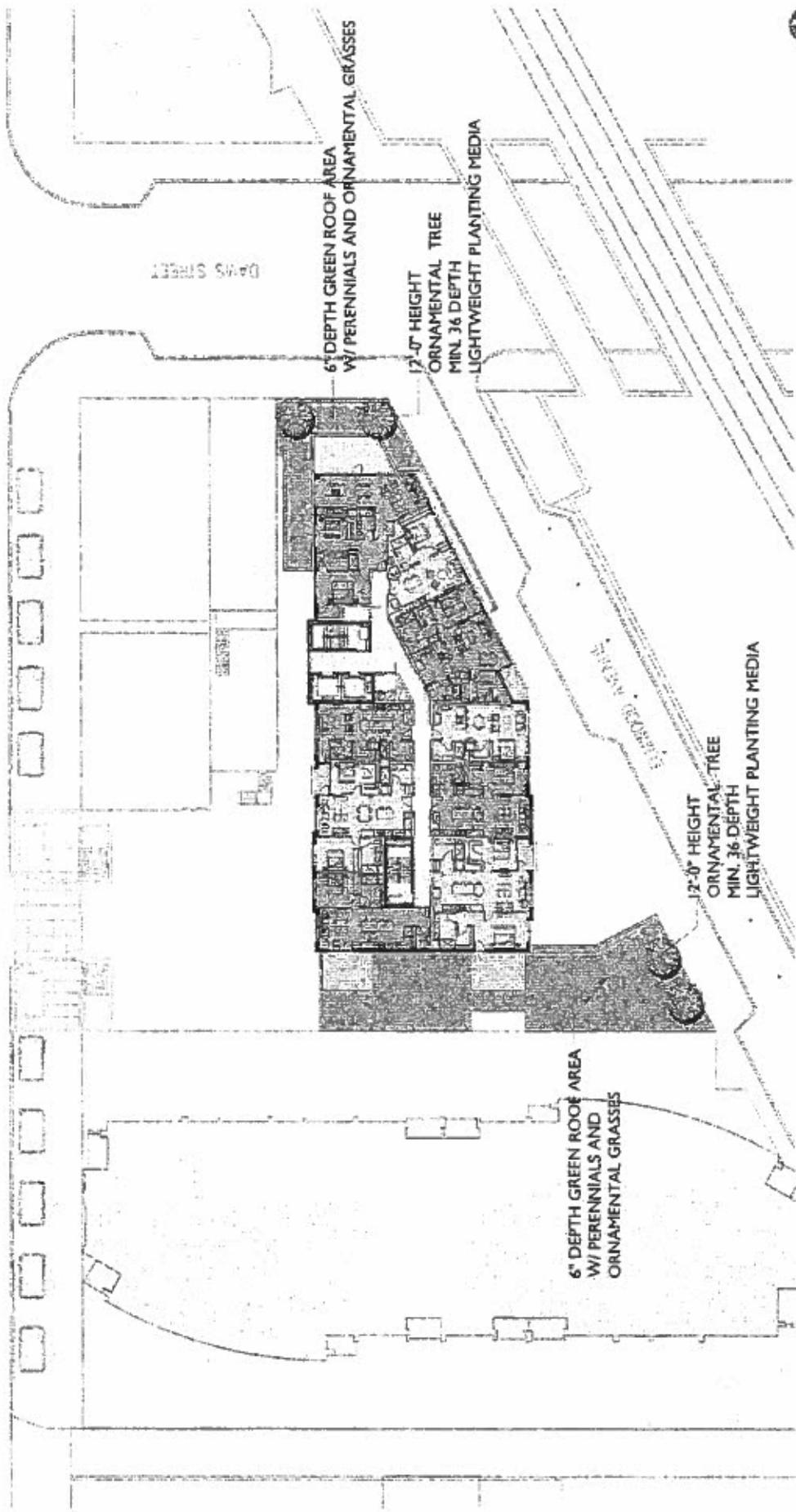
6" DEPTH GREEN ROOF AREA
W/ PERENNIALS AND
ORNAMENTAL GRASSES
12'-0" HEIGHT
ORNAMENTAL TREE

STANFORD AVENUE



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WOLFF LANDSCAPE ARCHITECTURE



Scale: 1" = 30'-0"

6/18

WOLFF LANDSCAPE ARCHITECTURE

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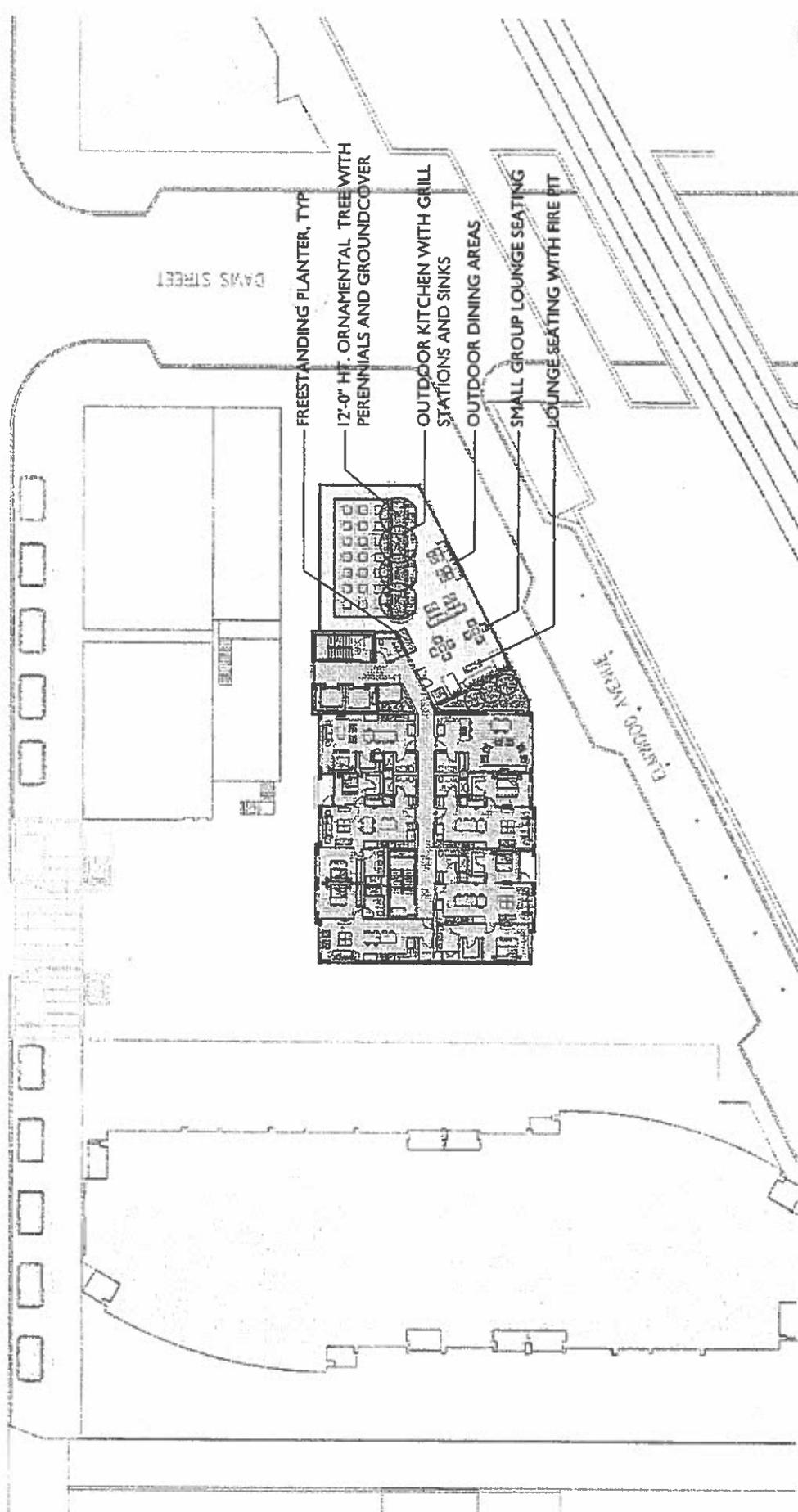
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418 Foot Landscape Plan

Wolff Landscape Architecture, LLC



11th Floor Landscape Plan

Scale: 1" = 30'-0"

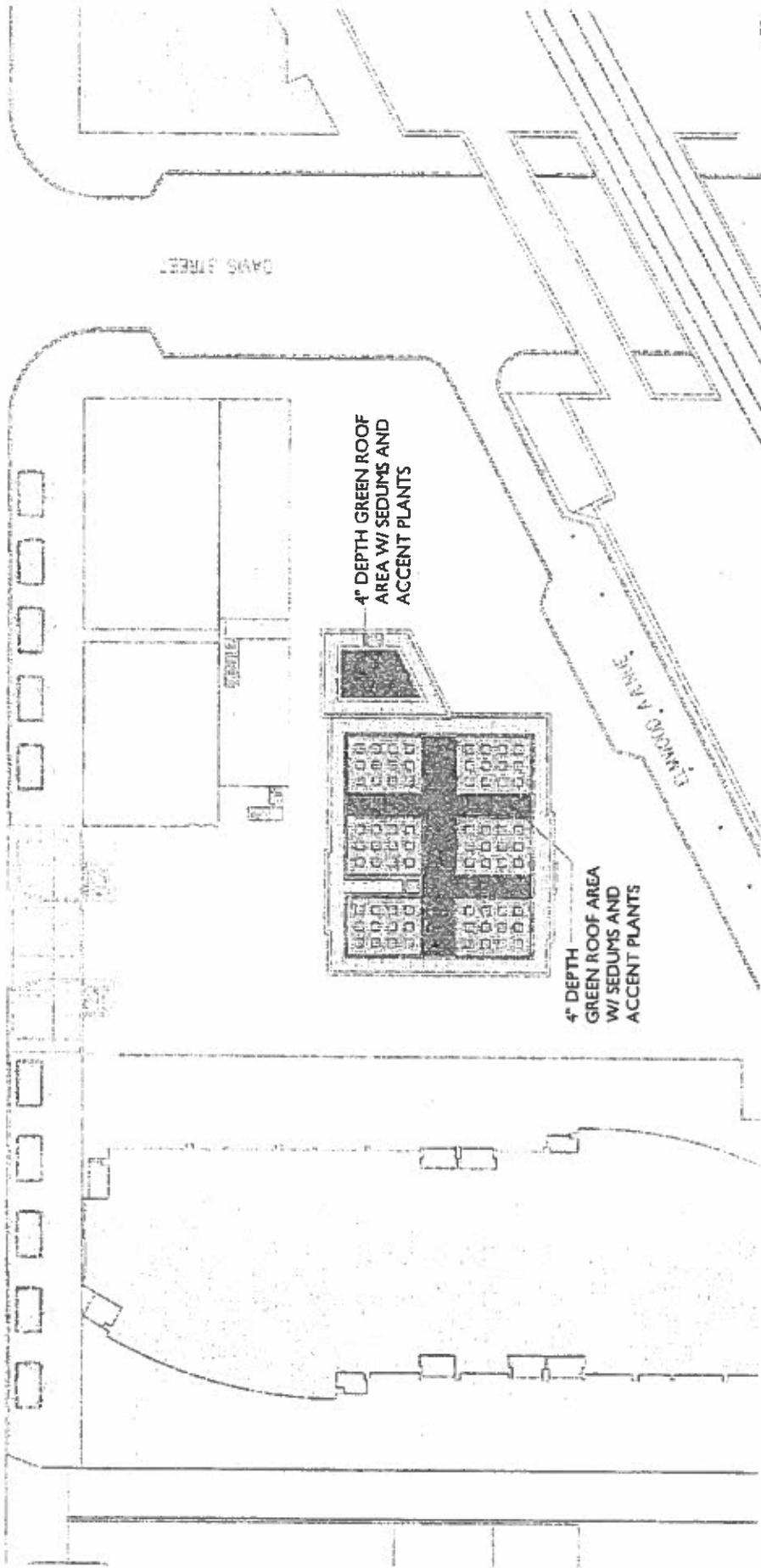
Cambridge Properties LLC

11th Floor Landscape

WOLFF LANDSCAPE ARCHITECTURE

11/18

MAPLE AVENUE AVENUE



Scale: 1" = 30'



WOLFF LANDSCAPE ARCHITECTURE



1215 E. GRAND AVENUE

1215 E. GRAND AVENUE



1215 E. GRAND AVENUE

1571 Maple Avenue Green roof narrative

The green roof areas for 1571 Maple Avenue will be a mix of intensive and extensive green roof systems. Intensive green roofs have increased growing media depths and allow for a wider variety of plant materials and uses while extensive green roofs are thinner in soil depth, lighter in weight and have a limited plant palette.

Intensive green roof areas will be located on the 2nd, 4th and 11th floors. These areas will contain a built up green roof system that will range in soil depths from 6" in the majority of the areas to 36" at areas where small ornamental trees will be planted, typically at parapet wall locations. The soil used will be a lightweight soil media such as Midwest Trading's PM-35 or an approved equal. The 6" depth areas will contain a mix of perennials, ornamental grasses and groundcovers. The 36" depth area will have 12'-0" multi-stem ornamental trees. This plant palette allows for seasonal interest all throughout the year. The layers of the green roof system will consist of the following, starting from the top: vegetation, growing media, drainage mat with filter fabric, Styrofoam (as needed), and root barrier. These layers sit atop the waterproof membrane and structural slab.

The green roof area on the 11th floor will also have outdoor use spaces such as an outdoor kitchen with countertops, barbeque grill stations and outdoor sinks. Seating areas in various sizes will also accommodate users for dining, small group seating and lounging around a fire feature. Freestanding planters with annual plantings will also be located in this area.

Extensive green roof areas will be located on the 12th floor. These areas will contain a built up green roof system that will have a soil depth of 4". The soil used will be a lightweight soil media such as Midwest Trading's PM-35 or an approved equal. The 4" depth areas will contain a sedum carpet mat planted with a mix of sedums varying in height and colors. This plant palette allows for seasonal interest all throughout the year. The layers of the green roof system will consist of the following, starting from the top: vegetation, growing media, drainage mat with filter fabric, Styrofoam (as needed), and root barrier. These layers sit atop the waterproof membrane and structural slab.

**DESIGN AND PROJECT REVIEW COMMITTEE (DAPR) MINUTES
May 3, 2022**

Voting Members Present: J. Nyden, A. Schnur, E. Cano, M. Callahan, I. Eckersberg,
R. Papa, M. Tristan, C. Pratt, M. Jones, K. Ashbaugh

Staff Present: M. Rivera, M. Griffith

Others Present:

Presiding Member: J. Nyden

A quorum being present, J. Nyden called the meeting to order at 2:01 p.m.

New Business

1. 1601 Sherman Avenue Sidewalk Cafe
Michael Klingl, applicant, submits for a sidewalk cafe permit for Sweetgreen in the D2
Downtown Retail Core District.

APPLICATION PRESENTED BY: Michael Klingl, applicant

DISCUSSION:

- Applicant stated they are requesting a permit for a seasonal outdoor dining patio. Applicant met with staff to review dimensions and to confirm cafe area. Applicant stated pedestrian passage is maintained. A handicapped accessible table will be provided, and trash cans provided.
- E. Cano asked if activity and noise from events occurring in Fountain Square had been considered.
- Applicant stated yes.
- C. Pratt asked if composting is offered.
- Applicant stated yes.
- M. Rivera asked the distance between the street curb and cafe.
- Applicant stated it is over 9', the cafe barrier is 9' from the street tree grate.

A. Schnur made a motion to approve the sidewalk cafe subject to Public Works Agency Staff being present during setup to check cafe dimensions, seconded by E. Cano.

Upon a voice vote of 10-0, the Committee approved the outdoor dining patio subject to the condition noted above.

2. 2255 Howad Street Sign Variance, 22SGNA-0001
Bryana Schramer, Applicant, requests a variance from the Evanston Sign Regulations to allow a wall sign to be installed at a height of 23.58 feet above grade where a height of no greater than 15.5 feet is permitted [Section 4-10-10(A)(4)] for Ross Dress For Less in the C1 Commercial District. The Design & Project Review Committee will make a final determination on this request per Section 4-10-17 of the Evanston City Code.

APPLICATION PRESENTED BY: Bill Holly, applicant

DISCUSSION:

- Applicant stated they are seeking a sign variation regarding the sign height. Applicant stated the storefront design calls for a higher sign, and other signs in the center are at similar heights.
- J. Nyden noted that a sign height variation was granted to Target.

A. Schnur made a motion to approve the sign variation, seconded by E. Cano.

Upon a voice vote of 10-0, the Committee approved the sign variation.

3. 1571 Maple Avenue Major Adjustment. to Planned Development
1571 Maple Avenue, LLC, applicant, submits for a major adjustment to the planned development approved by ordinance 19-O-15, and amended by ordinances 61-O-16 and 147-O-18 in the D3 Downtown Core Development District. The applicant is requesting to modify the number of required leased parking spaces from 55 to 0, terminating the existing parking lease between the applicant and the City of Evanston. DAPR makes recommendations on adjustments to planned developments to the Plan Commission.

APPLICATION PRESENTED BY: Michael McClean, applicant

DISCUSSION:

- Applicant stated they are requested to be released from the requirement to lease parking spaces. He stated the property sits in close proximity to both Metra and CTA train stations, and parking demand is down to 6 spaces. Applicant stated the building is and has been fully rented for the past several years and there hasn't been an increase in parking usage.
- Applicant stated the requirement to lease parking spaces was "insurance" to ensure residents had a place to park their vehicle and not park on the street within nearby neighborhoods. Applicant stated he is not aware of any complaints regarding residents parking in the neighborhood.
- J. Nyden asked who parks in the on-site parking spaces.
- Applicant stated the on-site parking spaces are for the retail spaces during the day and are open to residents in the evening. He stated there are 2 car share spaces and one electric vehicle charging station.
- J. Nyden stated additional electric vehicle charging stations should be provided with the parking lease requirement eliminated as a public benefit.
- Applicant stated he will check to see if that is feasible.
- M. Jones stated the development was required to provide 142 parking spaces originally but that number was reduced to 85 with the parking study.
- M. Rivera asked if the leased parking spaces have been marketed.
- Applicant stated yes they marketed the available parking spaces but there hasn't been the demand for those spaces. He stated they didn't see an increase in parking demand during the COVID pandemic.

A. Schnur made a motion to recommend approval subject to looking into adding electric vehicle charging stations, seconded by E. Cano.

Upon a voice vote of 10-0, the Committee recommended approval subject to the condition noted above.

Adjournment

A. Schnur made a motion to adjourn, seconded by C. Pratt. The Committee voted by voice vote, 10-0, to adjourn. The Committee adjourned at 2:27 p.m.

Respectfully submitted,
Michael Griffith, Planner



Meagan Jones <mmjones@cityofevanston.org>

Case 22PLND-0032 Major Adjustment to a Planned Development

1 message

GAIL TRAUGER <gaisie115@comcast.net>

Mon, May 23, 2022 at 5:11 PM

To: "mmjones@cityofevanston.org" <mmjones@cityofevanston.org>

Meagan,

I spoke to my councilman Jonathan Newsama and he suggested I reach out to you.

There is an application [Case #22PLND-0032](#) to the land use commission to reduce the 55 leased spaces from the City of Evanston to 0 for the existing development at 1571 Maple Ave/[1590 Elmwood Ave](#). I live at 1570 Elmwood Ave. I strongly ask that you and the commission vote against this proposal. The developer should continue to be financially responsible for the previous agreement to lease spaces from the City of Evanston rather than build & provide on site parking in their plans. There were numerous public hearings regarding the development prior to the building being built. The attorney for the developer stated numerous times that the potential renters will not own cars as concluded from their research. He convinced the city council to approve very few parking spaces based on their assumptions. I know for a fact the residents of 1571 Maple/1590 Elmwood Ave own cars because they are routinely looking for parking spaces closer to their apartments. I don't think the City of Evanston is in the position to forfeit any of the income from the agreement and should continue to hold the developer responsible for payment with no reduction! Please advise how to communicate concerns to the rest of your committee.

Thanks for your consideration
Gail Roitman-Trauger



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

1 message

noreply@formstack.com <noreply@formstack.com>

Tue, May 24, 2022 at 10:34 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 05/24/22 11:34 AM

Name: Noah Rosenblatt

Address of Residence: 1570 Elmwood Avenue, 1209

Phone: (847) 578-8425

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I am not completely aware of the specifics for this case, but I live in the building next door and I know there was a big debate about the building going up in the first place. Indeed, it completely blocked our view despite the fact that we were told no tall buildings could go up on that spot. It is my recollection that approval of the building was contingent upon providing parking spots since the builder did not include any (which was also a big complaint from our building). Assuming this was part of the initial agreement I see no reason that the stipulation should be removed for the builder other than a desire to save money. This is not acceptable given the original discontent with the building and the fact that this means the city would lose money. I strongly oppose changing the rules as this is a very slippery slope an unfair to the residents of Evanston and particularly of my building.

Agenda Item (or comment on item not Case #22PLND-0032

**on the
agenda):**

**Position on
Agenda
Item:**

Opposed

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Formstack, [11671 Lantern Road, Suite 300, Fishers, IN 46038](#)



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

2 messages

noreply@formstack.com <noreply@formstack.com>

Sun, Jun 5, 2022 at 2:51 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/05/22 3:51 PM

Name: Gary Auerbach

Address of Residence: 412 greenwood

Phone: (312) 961-8153

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: 1571 I don't think the city should agree to eliminate the parking requirement and agreement. The developer agreed to this when the development was approved though the spaces are being underutilized now does not mean that this will not change also the city incurs expense and lost revenue from this proposal which is not fair to the city or it's taxpayers. A deal is a deal. This has become a pattern that developers have employed. That is agree to provided parking so they don't have to build parking and then try to terminate the agreement. Bad precedent.

Agenda Item (or comment on item not on the agenda): 1571 parking

Position on Opposed

**Agenda
Item:**

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Formstack, [11671 Lantern Road, Suite 300, Fishers, IN 46038](#)

noreply@formstack.com <noreply@formstack.com>

Sun, Jun 5, 2022 at 8:28 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/05/22 9:28 PM

Name: Julie Rosen

Address of Residence: 1570 Elmwood Ave, Unit 806

Phone: (847) 840-9209

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I would like to oppose the change to the parking lease agreement. The city made several concessions to allow this development to proceed.

The rental of these spaces is a source of revenue for the city. What is the plan to offset the deficit from the decreased revenue? I attended the first meeting where the developers were granted the initial decrease to the lease of the parking spots. The developer indicated that the spaces were a drain on this cash flow as he may prepare to sell the development at some point. As a

taxpayer, I don't feel that it's our problem regarding the status of the revenues and/or deficits generated by the developer. I felt that the city did not conduct proper due diligence during the first request. The decision to reduce the parking spaces was based solely on the information provided by the developer. The city representative indicated that the city had no way to determine if the information provided by the developers was correct or that the city had any mechanism to support the need to decrease parking allocation.

As a health care, provider I am still perplexed as to how the city could allow a property to be built where a tenant does not have access to parking on-site. This does not seem very inclusive. On your notice that was sent out to the public, it states clearly for individuals who need accommodations for either hearing impaired or physical needs to reach out. Individuals who need accommodations do not have the opportunity to reside in the building. I have seen the mayor and the city talk about inclusivity - it seems like we missed the message with this development. I know that during the original negotiations we were told that this was an "experiment" that individuals who would live there would not need cars. Do we know that to be a fact?

Thanks

Agenda Item (or comment on item not on the agenda):

1571 Parking Spots

Position on Agenda Item:

Opposed

[Quoted text hidden]

Crawford Ave. & Gross Point Rd.
Map Amendment
22PLND-0038

LUC Recommending Body



Memorandum

To: Chair and Members of the Land Use Commission

From: Johanna Nyden, Director of Community Development
Melissa Klotz, Zoning Administrator

Subject: Zoning Ordinance Map Amendment
Crawford Ave. & Gross Point Rd.
22PLND-0038

Date: June 16, 2022

Request

A City-initiated Map Amendment to the Zoning Ordinance to remove the existing oCSC Central Street Corridor Overlay District and revert to the existing underlying B1a Business District zoning at the following properties:

2600 Gross Point Rd., PIN 05-33-318-032-0000
2608-2620 Gross Point Rd./2620 Crawford Ave., PINs 05-33-318-033-0000, 05-33-318-034-0000
2628-2636 Gross Point Rd., PINs 05-33-311-054-0000, 05-33-311-040-0000, 05-33-311-053-0000
2600 Crawford Ave., PIN 05-33-319-001-0000

The rezoning is primarily intended to allow for the construction of a permanent outdoor seating area/tent at Sarkis, and future façade improvements at other identified properties.

Notice

The Application has been filed in conformance with applicable procedural and public notice requirements including publication in the Evanston Review on June 2, 2022.

Analysis

Proposal Overview

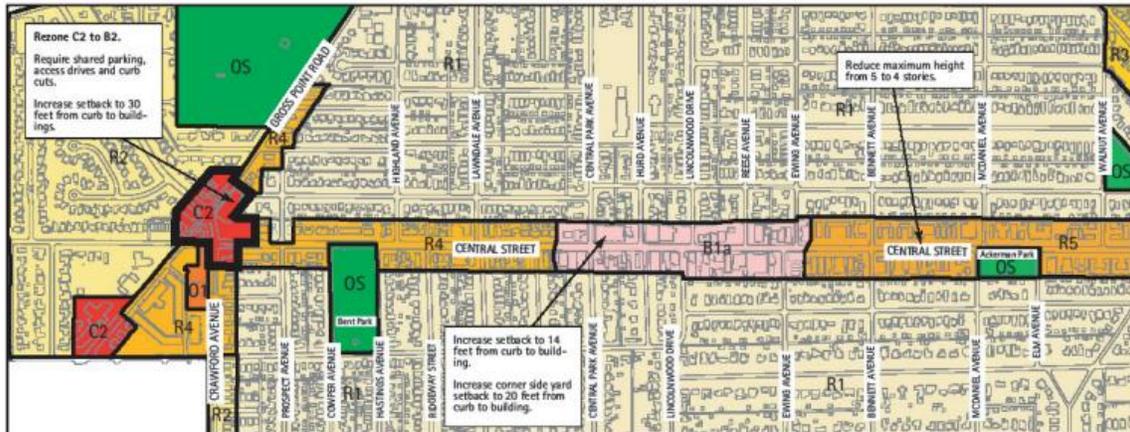
The Central Street Corridor Master Plan was adopted in 2007 and called for the creation of the oCSC Central Street Overlay District, which was adopted in 2008. The oCSC Overlay District is a form-based code that consists of seven subareas that each contain specific use, siting, appearance, bulk, and height requirements that are codified and required by zoning.

Planning & Zoning staff identified 7 parcels (4 zoning lots/businesses) at the western

edge of the oCSC Overlay District around Crawford Ave. and Gross Point Rd. that feature unique circumstances that make the regulations of the oCSC Overlay District detrimental to nearly any improvements proposed on the lots and creates burdensome zoning issues that should not be in place.

The Master Plan originally called for rezoning of the Crawford Ave. & Gross Point Rd. area from the C2 Commercial District to the B2 Business District.

Zoning Recommendations



Zoning recommendations for the western half of Central Street include rezoning the C2 district at the intersection of Central/Gross Point/Crawford to B2, and reducing the height of the R5 districts to 4 stories/45 feet.

The Master Plan identifies the intersection area as a commercial area with redevelopment potential. The Plan states:

Commercial District (C2) – Gross Point/Crawford/Central intersections: Buildings should be set back 30 feet from the curb to provide room for wider sidewalks, additional landscaping, and improved sight lines for motorists.

The Plan also explains how development would occur to be successful:

The new development shown in the Master Plan will be largely driven by the private sector. While the City can encourage and facilitate property improvements and new development by investments in infrastructure, the Plan envisions property owners improving or redeveloping their sites or cooperating to create larger, more viable redevelopment opportunities.

The Plan envisioned redevelopment of the area as shown on the right.



However, when the oCSC Overlay District and corresponding form-based code was adopted, the Crawford Ave. & Gross Point Rd. area, or Subarea 6, was rezoned to the B1a Business District instead of the B2 Business District as the Plan called for. The B1a Business District is a down-zone of B2, as seen in the following comparison:

	Density	FAR	Front Yard	Street Side Yard	Interior Side Yard	Rear Yard	Building Height
B1a	900 sq. ft. per DU	2.0	3'	3'	10'	25'	40' or 3 stories
B2	400 sq. ft. per DU	2.0	3'	3'	10'	15'	45'

When comparing the two districts, the B1a District allows less than half of the density, a larger rear yard setback, and shorter building height than the B2 District.

It has been nearly 15 years since the adoption of the oCSC Overlay District, and while the regulations of the Overlay work well in the pedestrian-oriented Central Street Business District, the Overlay regulations combined with the downzoning to the B1a Business District on the identified properties have proved ineffective and instead are prohibiting positive activity in the area such as improved storefront facades and new permanent structures.

The purpose of the oCSC Overlay District is (Section 6-15-14-1):

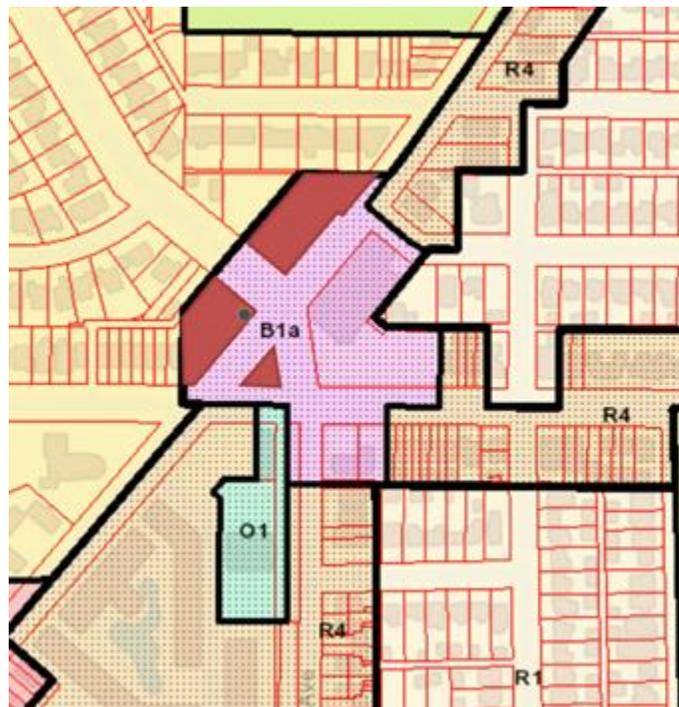
The oCSC district is intended as a primary means to implement the recommendations contained in the Central Street Master Plan (2007). The geographical scope of the plan extended the length of Central Street from Gross Point Road in the west to Ridge Avenue in the east, and includes intersecting portions of Gross Point Road, Crawford Avenue and Green Bay Road. Specifically, this overlay district seeks to:

- A. Preserve existing character and scale.
- B. Encourage a healthy mix of uses along the corridor; preserve independent and unique uses.
- C. Sustain and enhance the corridor as a location for diverse, unique, small scale, pedestrian oriented retail shops, services, and restaurants.
- D. Encourage retail uses close to transit.
- E. Allow a wide, consistent sidewalk width.
- F. Ensure wider, landscaped parkways as a transition between retail frontages and residential side streets.
- G. Provide improved sightlines for motorists.
- H. Ensure consistent building placement and create a pedestrian friendly and human scaled "street wall."
- I. Articulate buildings and reduce the perceived height and mass of new development by using building setbacks at upper stories.
- J. Establish new sidewalk standards for improved sidewalk widths, sightlines, and streetscapes.
- K. Encourage buildings with clearly defined bases, middles, and tops.

- L. Allow the intuitive identification of storefronts through the use of appropriate store windows and fenestration for retail and mixed use buildings.

The 12 points of the purpose statement are admirable, and have proven to be effective planning and zoning tools with the success of the oCSC Overlay District further east on Central St. However, while properties further east on Central St. have been successfully able to construct new storefronts and small or moderate additions, improvements have not been feasible at the identified properties due to the combined downzoning to B1a, and Overlay District regulations such as extensive street frontage and Overlay setbacks – including the 30’ from curb setback for sidewalks and street furniture that is required along all street frontages where every identified property has at least two street frontages.

The properties identified for removal from the oCSC Overlay District are all along the very western edge of the oCSC Overlay District and are shown in red:



- Sarkis Restaurant, 2628-2636 Gross Point Rd.: Sarkis currently has temporary outdoor tents on the property that are not allowed to remain per the Zoning Ordinance. The property owner would like to construct a permanent tent structure that meets all City Code requirements, which may entail footings, wood posts or beams, and a heavy duty canvass or hardtop roof. Such structure is not feasible within the oCSC Overlay District since it would trigger a substantial number of variations including 30’ wide sidewalk and street furniture zones along the entire length of the Sarkis property (3 parcels, including the parking lot parcel that is owned by CVS) along both Gross Point Rd. and Crawford Ave.
- Little Island Restaurant, 2600 Gross Point Rd.: Little Island does not plan to make improvements in the near future. With three street frontages, any changes to the

property would trigger significant variations within the Overlay District. Once removed, variations would still be triggered for moderate to substantial improvements or redevelopment, but simple façade and parking lot improvements could be done without the need for zoning relief.

- Wil-Ridge Complete Auto Repair, 2608-2620 Gross Point Rd./2620 Crawford Ave.: Wil-Ridge does not currently have plans to improve their property (2 parcels) but attempted to in the past only to find the existing zoning regulations made property modifications so difficult that they did not proceed. Removal of the Overlay District would allow Wil-Ridge to move forward in the future with façade and/or parking lot improvements.
- Evanston Paper, 2600 Crawford Ave.: The property is triangular in shape with the widest portion at the street frontage, where the Overlay District prohibits structures within 30' of the curb. With that Overlay District requirement, as well as the extensive façade requirements, this property could not be improved or redeveloped without substantial zoning relief.

The oCSC Overlay District includes the following specific regulations that are problematic and infeasible at the identified properties:

- Pedestrian Area Requirements (6-15-14-12):
Minimum Width Along Central, Gross Point, Crawford: 15' sidewalk clear zone, then 15' parkway/street furniture zone, for a minimum building and parking setback of 30' from all street frontages. At the identified properties, this setback amounts to over 1/3 of the property areas. For example, the Sarkis property (including the parking lot parcel owned by CVS) is estimated at 29,205 sq. ft. and the pedestrian area requirement amounts to over 10,000 sq. ft. of private property that cannot be used for buildings or parking. The +10,000 sq. ft. estimate does not include side or rear yard setbacks that further reduce the buildable area.
- Fenestration (6-15-14-14):
Minimum 65% fenestration (windows) required along the ground floor facing the street frontage, and continuing around the corner of a building for at least 40' or the length of the building, whichever is shorter; lower edge of lite (sill) no higher than 3 ft. above grade; upper edge of lite (header) is no lower than 10' above grade; glass entrances only count towards fenestration if the qualifying fenestration extends above the entrance to no lower than 10' above grade. The existing structures at the identified properties do not comply with these regulations, so if any change to a façade is proposed, all fenestration regulations must be met or variations are triggered. Given the heavy automobile-oriented intersection, it may not be appropriate for existing or new structures to feature largely glass facades.
- Building Façade Articulation (6-15-14-15):
For all building facades facing public streets, the length of façade without intervening fenestration or entryway shall not exceed 20'; building floors shall be delineated through the use of windows, belt courses, cornice lines or similar architectural detailing; facades wider than 25' shall be vertically articulated to give the appearance of being composed of multiple buildings by using projections, recesses, material changes, parapets, cornices, varying roof heights/lines, or other similar architectural features; no more than 25' of width of any building façade shall form a sheet wall or a vertical unbroken plane of façade that is unarticulated in depth. While

such design criteria may lead to exceptional new construction, the requirements are prohibitive to any moderate façade improvements to existing structures and have instead encouraged the identified properties to not make building upgrades.

- Building Height (6-15-14-8):

Transitional height plane required at the location of the defined setback closest to the adjacent lower intensity district, and having a height the same as the defined maximum building height of the adjacent lower intensity residential district that extends upward away from the residential district at a 45 degree angle from horizontal; a stepback of 10% of the lot depth or width from the required pedestrian area is required for upper stories for front and street side yards.

The oCSC Overlay District does regulate uses. Changes to use regulations at the identified properties include:

	Additional Permitted Uses	Additional Special Uses	Prohibited Uses
oCSC Subarea 6	Automobile Service Station, Specialty Food Store	Cultural Facility, Performance Entertainment Venue	Drive-Through Facility (except at 2628 Gross Point Rd.)

This means that with the oCSC Overlay District removed from the identified properties, those properties revert to the underlying B1a Business District zoning only and could no longer redevelop into an Automobile Service Station (gas station), Specialty Food Store Establishment moves from permitted to only permitted outside of midnight-6am or all hours by special use, Cultural Facility becomes a permitted use instead of a special use, and Performance Entertainment Venue is no longer an eligible use.

Of note, the oCSC Overlay District currently prohibits Drive-Through Facilities at all properties in the Overlay but allows a Drive-Through Facility as a special use specifically at 2628 Gross Point Rd. Removal of the Overlay means all of the identified properties change to Drive-Through Facility as a special use (rather than a prohibited use). Planning & Zoning staff acknowledge that proper queueing for a Drive-Through Facility at any of the properties is likely infeasible, and new or additional curb cuts to the identified properties are also likely infeasible due to the surrounding busy automobile intersections, the Public Works Agency’s curb cut requirements, and IDOT regulations. The City did see an application for a Drive-Through Facility in conjunction with a special use request for a Type 2 Restaurant and a significant amount of zoning relief required for a proposal for Nic’s Organic Fast Food restaurant in 2018-2019 (2 application attempts). In both applications, it was infeasible for the property to fit all of the required parking plus the Drive-Through queueing without blocking drive-aisles and access to the site. The first application was denied by the City Council and the second application was withdrawn by the applicant. If an application for a special use for a Drive-Through Facility is ever requested at the identified properties, City staff is aware of the severe site constraints and safety issues and will not recommend approval. The Public Works Agency agrees that Drive-Through Facilities at corner properties can be problematic

and should not be approved, and follows curb cut regulations within the City Code in Section 7-3-8 and specifies that no curb cut shall be granted within 20' of any street intersection. Staff is not aware of any potential request for a Drive-Through Facility at the identified properties, and the property owner of Sarkis has stated there is no plan to add a Drive-Through Facility at Sarkis. Additionally, as a special use, any Drive-Through Facility would require City Council approval.

Also, the oCSC Overlay District requires Active Ground-Floor Uses (6-15-14-7) within the first 50' of any property's street frontage. Removal of the oCSC Overlay District regulations would mean there is no requirement for an Active Ground-Floor Uses. However, the pending text amendment to make Ground-Floor Office Uses into Administrative Review Uses would effectively establish the same regulation since Administrative Review Uses look closely at and typically require storefront activation.

Overall, the use requirements of Subarea 6 of the oCSC Overlay District are similar to those of the B1a Business District so the proposed rezoning would produce minimal or no change in uses. The proposed rezoning would reduce the extensive zoning regulations for setbacks, fenestration, and façade requirements so that current existing businesses in the area are able to move forward with appropriate site improvements.

Standards of Approval

Pursuant to Section 6-3-4-5 Standards for Amendments:

The wisdom of amending the text of the Zoning Ordinance or the Zoning Map is a matter committed to the sound legislative discretion of the City Council and is not controlled by any one standard. In making their determination, however, the City Council should, in determining whether to adopt or deny, or to adopt some modification of the Plan Commission's recommendation consider, among other factors, the following:

- (A) Whether the proposed amendment is consistent with the goals, objectives, and policies of the Comprehensive General Plan, as adopted and amended from time to time by the City Council. **Standard met** – The Comp Plan specifically encourages the improvement to existing businesses and developed properties as well as redevelopment of certain properties.
- (B) Whether the proposed amendment is compatible with the overall character of existing development in the immediate vicinity of the subject property. **Standard met** – The area proposed for removal from the Overlay District is not connected to the primary Central Street Business District area and is instead surrounded by low-density residential. The existing built environment is unique to the intersection design and therefore should not be overly limited by the form-based code requirements of the oCSC Overlay District.
- (C) Whether the proposed amendment will have an adverse effect on the value of adjacent properties. **Standard met** – The proposed amendment is likely to have a positive effect on adjacent properties since it will allow existing commercial structures to update facades and/or replace temporary tents with permanent structures.

(D) The adequacy of public facilities and services. **Standard met** – Adequate public facilities and services currently exist to all properties in question.

The proposed rezoning is consistent with the goals, objectives, and policies of the Comprehensive General Plan including:

- Promote the growth and redevelopment of business, commercial, and industrial areas.
- Recognize and support the strong role neighborhood business districts play in Evanston's economy and its identity.

The proposed rezoning is compatible with the overall character of existing development in the immediate vicinity by maintaining similar bulk structure and use regulations while reducing the extensive setbacks along street frontages given the highly automobile-oriented intersection, thereby allowing existing businesses to appropriately make façade improvements or construct additional permanent structures without the need for extensive zoning relief.

Recommendation

Staff recommends the Land Use Commission make a positive recommendation for approval of the proposed map amendment to remove the identified properties located around the Crawford Ave. and Gross Point Rd. intersection from the existing oCSC Central Street Corridor Overlay District so that the properties revert to the existing underlying B1a Business District regulations.

Attachments

Central Street Overlay Maps

Zoning Maps of Proposed Map Amendment Properties

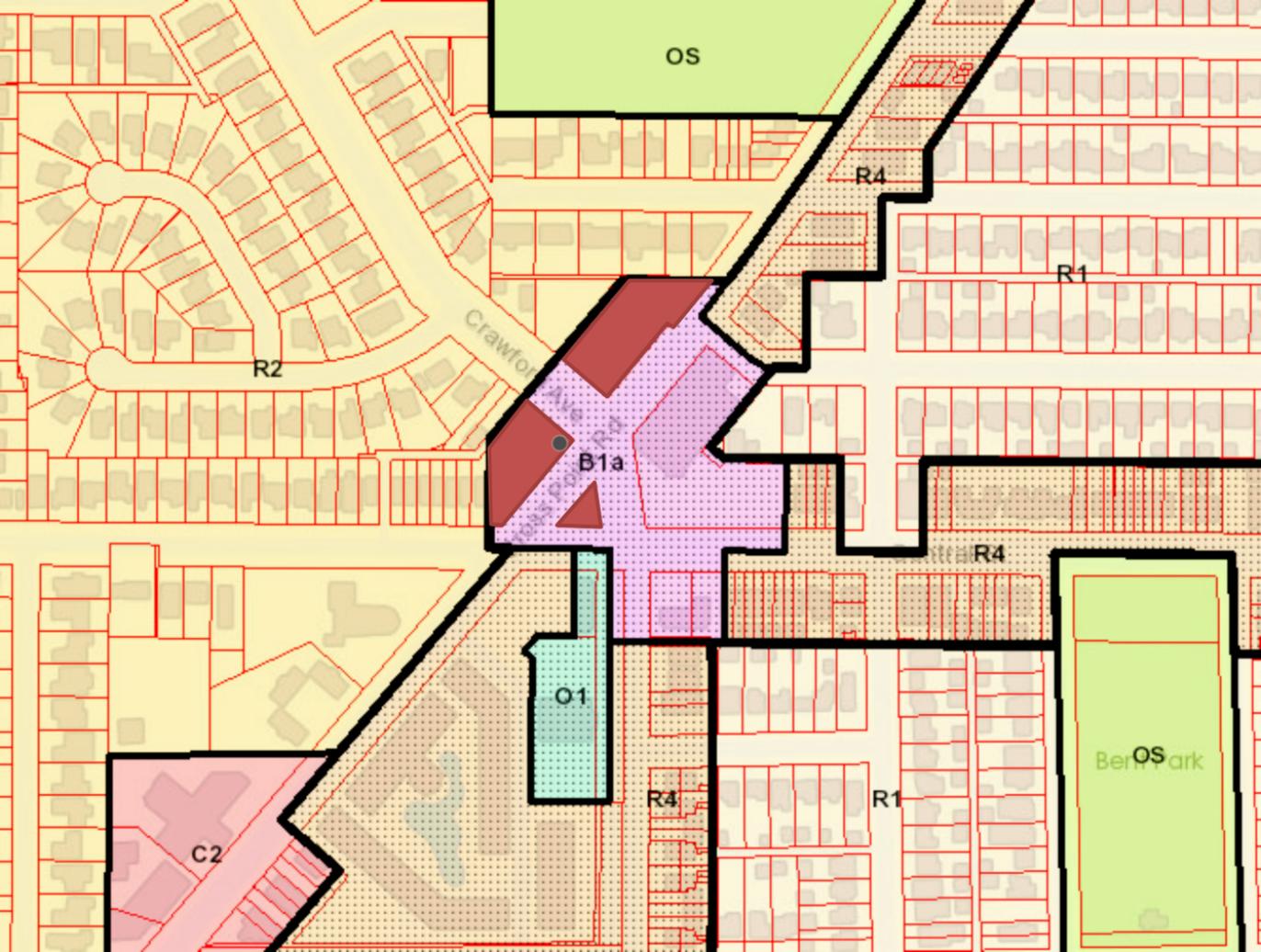
Public Notice Postcard

Letter to Property Owners

oCSC Central Street Overlay District Regulations

B1a Business District Regulations

Public Comments



OS

R4

R1

R2

Crawford Ave

B1a

R4

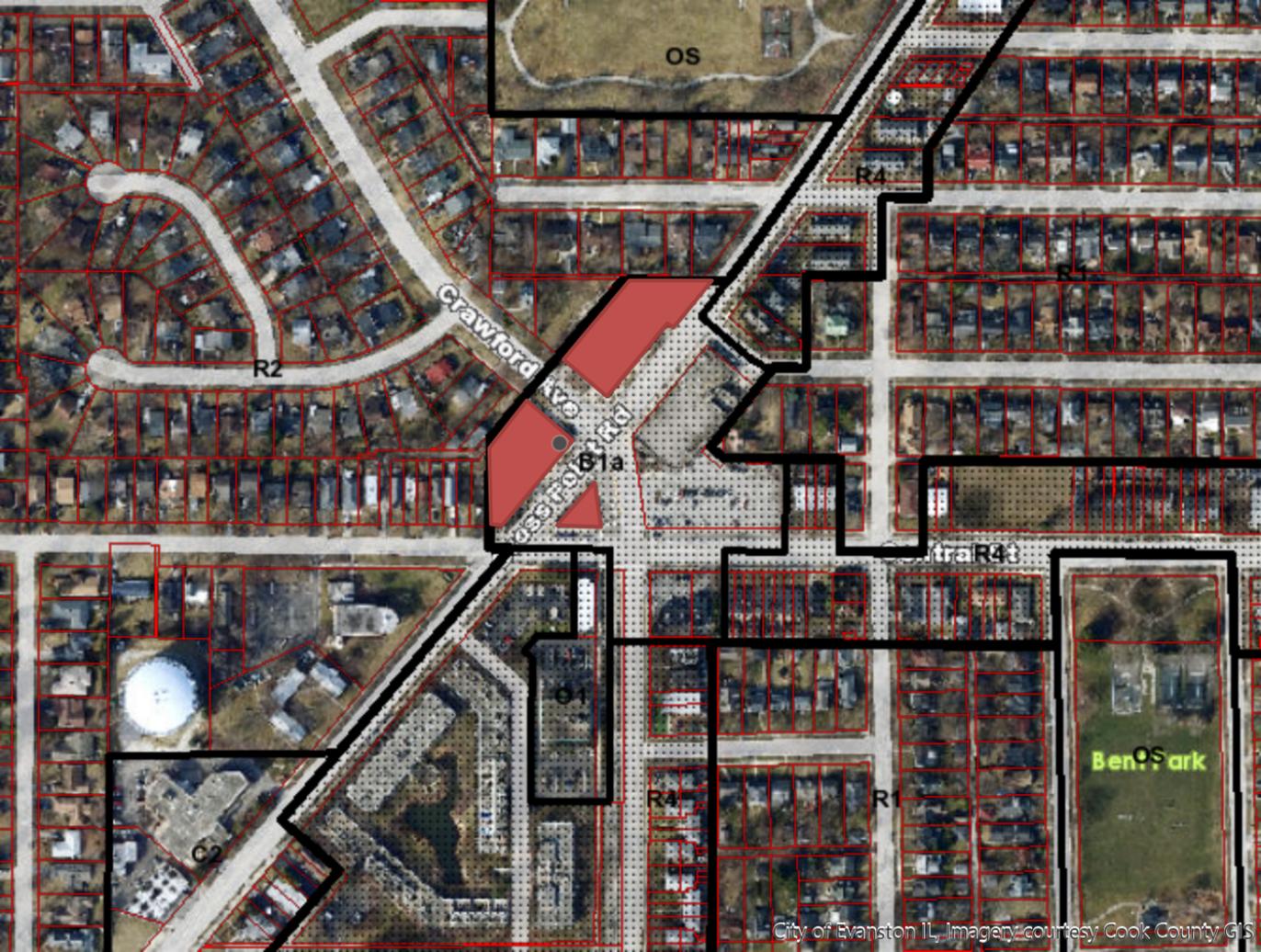
O1

R4

R1

C2

Ber OS ark



OS

R2

Crawford Ave

Crawford Blvd

C1a

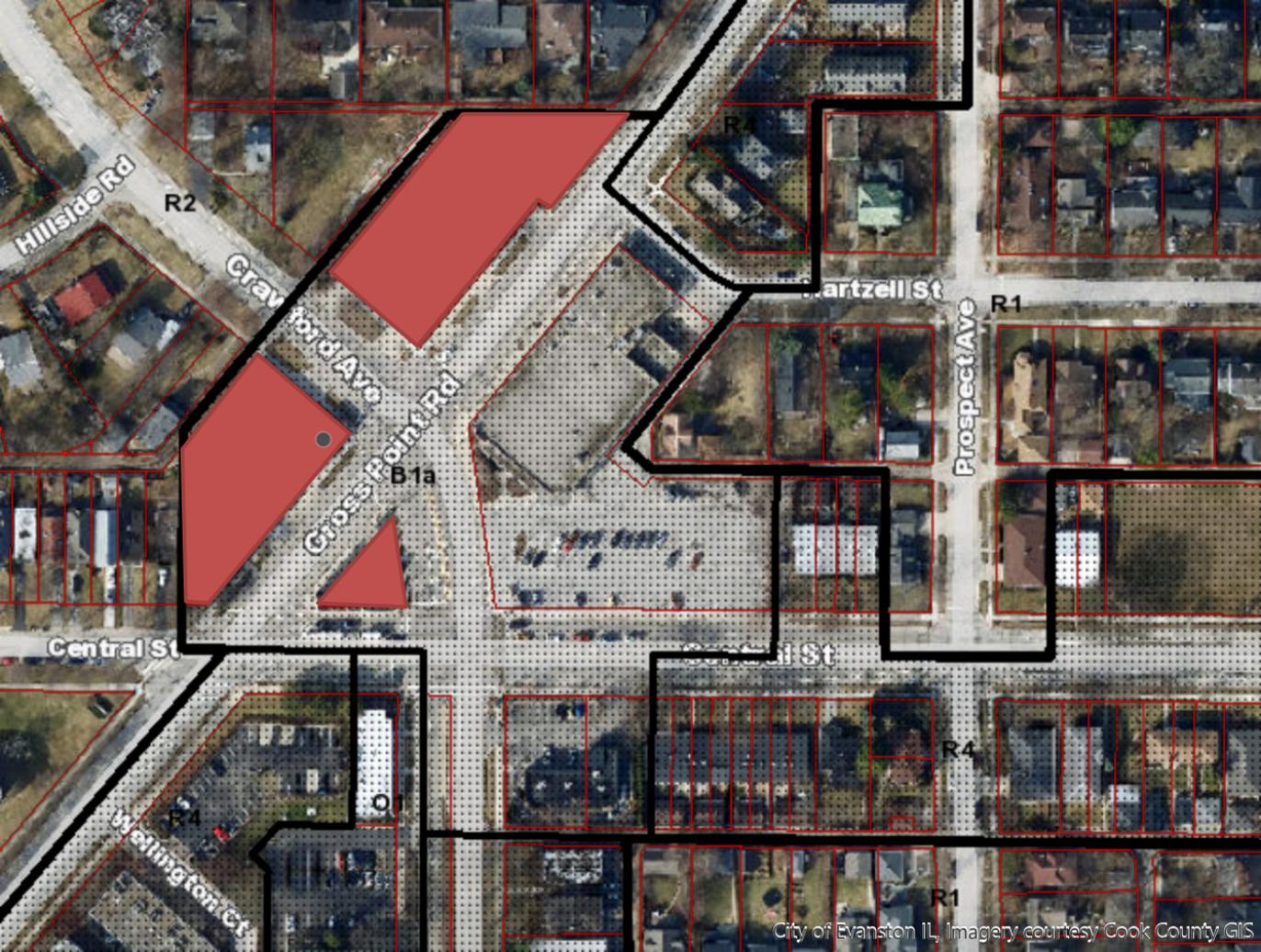
Central R4

C1

R4

R1

Ben OS Ark



Hillside Rd

R2

Crayford Ave

Gross Point Rd

B1a

Hartzell St

Prospect Ave

R1

Central St

Hartzell St

R4

Wellington St

O1

R1

NOTICE OF A PUBLIC MEETING

Land Use Commission

Wednesday, June 22, 2022, 7:00 pm

Morton Civic Center, 2100 Ridge Avenue

Council Chambers

Please be advised, as you own, or otherwise may have interest in a property within 500 ft. of the addresses listed below, the following zoning application will be discussed:



Crawford Ave. & Gross Point Rd. | 22PLND-0038 | Map Amendment

City initiated Map Amendment to the Zoning Ordinance, Title 6 of the City Code, to rezone properties known as: 2600 Gross Point Rd., PIN 05-33-318-032-0000; 2608-2620 Gross Point Rd./2620 Crawford Ave., PINs 05-33-318-033-0000, 05-33-318-034-0000; 2628-2636 Gross Point Rd., PINs 05-33-311-054-0000, 05-33-311-040-0000, 05-33-311-053-0000; 2600 Crawford Ave., PIN 05-33-319-001-0000. The properties are proposed to be removed from the existing oCSC Central Street Corridor Overlay District, and will remain within the existing underlying B1a Business District. The Land Use Commission makes a recommendation to the City Council, the determining body for this case in accordance with Section 6-3-5-8 of the Evanston Zoning Ordinance and Ordinance 92-O-21.

Those unable to attend in person may submit written comments in advance or sign up to provide public comment by calling/texting 847-448-4311 or completing the Land Use Commission online comment form available online here: https://arts.formstack.com/forms/land_use_commission_public_comment. Information about the Land Use Commission is available online at www.cityofevanston.org/government/land-use-commission. Questions can be directed to Melissa Klotz, Zoning Administrator, at mklotz@cityofevanston.org. The City of Evanston is committed to making all public meetings accessible to persons with disabilities. Any citizen needing mobility or communications access assistance should contact the Community Development Department 48 hours in advance of the scheduled meeting so that accommodations can be made at 847-448-8170 (Voice) or 847-866-5095 (TDD). La ciudad de Evanston está obligada a hacer accesibles todas las reuniones

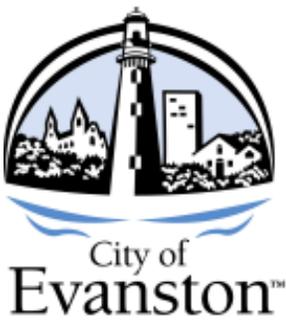


City of
Evanston™

Lorraine H. Morton Civic Center
Planning and Zoning Division
2100 Ridge Avenue
Evanston, IL 60201

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City of Evanston
Community Development Department
Planning & Zoning Division
2100 Ridge Avenue
Evanston, Illinois 60201
847-448-4311
847-448-8230

June 7, 2022

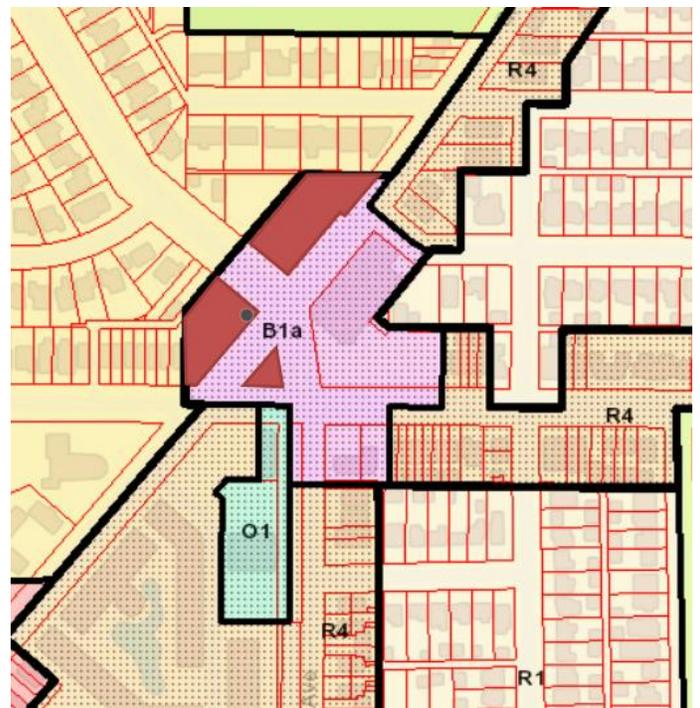
RE: Map Amendment to the Zoning Ordinance to remove the Central Street Overlay District Regulations from Certain Properties at Crawford Ave. & Gross Point Rd. | 22PLND-0038

Dear Property Owner,

The City of Evanston is considering modifying the zoning requirements at 6 properties along the western side of Gross Point Rd. as well as the center triangle property between Crawford Ave. and Gross Point Rd. These properties, which include Sarkis restaurant, Wil-Ridge Complete Auto Repair, Evanston Paper, and the Little Island Restaurant, have been identified as over-regulated by zoning. **The zoning change should not affect any business operations in any way. Rezoning DOES NOT mean a business needs to move.**

This rezoning is City initiated by the Planning & Zoning Division; therefore there is nothing you are required to do. The City proposes to remove the extra zoning requirements of the Central Street Overlay District from these properties so that they will revert to their underlying and existing B1a Business District zoning only. The Central Street Overlay District stretches from Central St. and the North Shore Canal west to Gross Point Rd. The Overlay District also extends to the north and south along Green Bay Rd. and Gross Point Rd. The properties identified for removal from the Overlay District are identified in **red**. The existing Overlay District is identified by hatched dots.

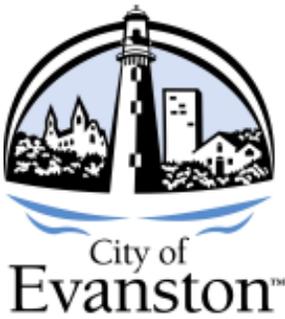
The intent of the Central Street Overlay District is a form-based code with specific design regulations that was adopted in 2008 and calls for wider, extensive parkway areas for pedestrian street furniture, wider sidewalks, and extensive landscape areas in addition to specific requirements for building design. **The Central Street Overlay District is appropriate for many properties along the pedestrian-friendly Central Street corridor, but is overly-limiting for the identified properties.**



Removal of these properties from the Central Street Overlay District means:

- Nothing **MUST** change.
- The vacant corner property (now owned by Sarkis) could install a permanent tent for outdoor seating.
- All properties could remodel existing storefronts and building facades without triggering substantial zoning variations.

Removal of these properties from the Central Street Overlay District does **NOT** mean the properties will be redeveloped with new buildings. **There is no proposal for new buildings on any of the identified properties.** Additionally, the existing underlying B1a Business District zoning will remain, and will continue to require typical zoning regulations such as a Special Use for a Type 2 Restaurant should one be requested (**there is no current or**



City of Evanston
Community Development Department
Planning & Zoning Division
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Evanston, Illinois 60201
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preliminary request in process). Removal of the Overlay District is necessary for the benefit of the existing buildings and a permanent tent at Sarkis.

Removal of these properties from the Overlay District will not negatively impact the rest of the Overlay District since the properties identified for rezoning are specifically the only parcels within the Overlay that are west of Gross Point Rd. or within the intersection. No other changes are proposed to the existing Overlay District.

Additional information is attached that lists all zoning regulations of the Central Street Overlay District. Note the extensive form-based code design requirements that are not feasible for the identified properties and existing structures. Also attached are the current regulations for the B1a Business District in which the properties are currently in and will remain in. The full Zoning Ordinance may be accessed at www.cityofevanston.org/government/city-code (Title 6 - Zoning). For additional information, contact the Planning & Zoning Division at zoning@cityofevanston.org. The public hearing with the Land Use Commission, where the proposed rezoning will be discussed (and will include a recommendation to City Council) is scheduled for a meeting on June 22, 2022, at 7pm. Property owners within 500 feet of the proposed rezoning will receive a public notice post card in the mail confirming this meeting.

Sincerely,

Melissa Klotz
Zoning Administrator

Cc: Tom Suffredin – 6th Ward Councilmember
Johanna Leonard – Community Development Director

Enclosure: Central Street Overlay District Regulations; B1a Business District Regulations

6-15-14. - oCSC CENTRAL STREET CORRIDOR OVERLAY DISTRICT.

6-15-14-1. - PURPOSE STATEMENT.

The oCSC district is intended as a primary means to implement the recommendations contained in the Central Street Master Plan (2007). The geographical scope of the plan extended the length of Central Street from Gross Point Road in the west to Ridge Avenue in the east, and includes intersecting portions of Gross Point Road, Crawford Avenue and Green Bay Road. Specifically, this overlay district seeks to:

- (A) Preserve existing character and scale.
- (B) Encourage a healthy mix of uses along the corridor; preserve independent and unique uses.
- (C) Sustain and enhance the corridor as a location for diverse, unique, small scale, pedestrian oriented retail shops, services, and restaurants.
- (D) Encourage retail uses close to transit.
- (E) Allow a wide, consistent sidewalk width.
- (F) Ensure wider, landscaped parkways as a transition between retail frontages and residential side streets.
- (G) Provide improved sightlines for motorists.
- (H) Ensure consistent building placement and create a pedestrian friendly and human scaled "street wall."
- (I) Articulate buildings and reduce the perceived height and mass of new development by using building stepbacks at upper stories.
- (J) Establish new sidewalk standards for improved sidewalk widths, sightlines, and streetscapes.
- (K) Encourage buildings with clearly defined bases, middles, and tops.
- (L) Allow the intuitive identification of storefronts through the use of appropriate store windows and fenestration for retail and mixed use buildings.

(Ord. 5-O-08)

6-15-14-2. - APPLICATION OF THE DISTRICT.

Any property that comes to be located within this district shall retain its original zoning district designation, and shall gain the additional designation of the oCSC district. The provisions of this Section 6-15-14 shall serve as a supplement to the zoning district regulations of the underlying district. Where a conflict exists between the provisions of this Section 6-15-14 and those of the underlying zoning district, the provisions of this overlay district shall control.

(Ord. 5-O-08)

6-15-14-3. - DESIGNATION OF OVERLAY DISTRICT.

The Central Street corridor overlay district shall be designated by the City Council and shown as an overlay to the underlying districts with the designation "oCSC" on the City zoning map.

(Ord. 5-O-08)

6-15-14-4. - SUBAREAS.

- (A) *Purpose:* The Central Street corridor overlay district contains seven (7) subareas that allow the district to be tailored to the needs and existing conditions of different areas along the corridor.
- (B) *Subareas Defined:* The following subareas are defined as part of the Central Street corridor overlay district:
- Subarea 1; multi-family residential A: This subarea is based on properties having a base zoning district of R4.
- Subarea 2; multi-family residential B: This subarea is based on properties having a base zoning district of R5.
- Subarea 3; office: This subarea is based on properties having a base zoning district of O1.
- Subarea 4; mixed use A: This subarea is based on neighborhood commercial properties having a base zoning district of B1a.
- Subarea 5; mixed use B: This subarea is based on commercial properties having a base zoning district of B1a.
- Subarea 6; Gross Point/Crawford mixed use: This subarea is based on properties surrounding the intersection of Gross Point Road, Crawford Avenue, and Central Street, having a base zoning district of B1a.
- Subarea 7; Green Bay commercial: This subarea is based on properties along Green Bay Road having a base zoning district of C2.
- (C) *Subarea Designation:* Subareas will be designated by the City Council and shown on the City zoning map or on a separate detail map that is indicated and referenced on the City zoning map. Subareas shall be indicated by appending the subarea number to the overlay district designation: oCSC-1 through oCSC-7.

(Ord. 5-O-08)

6-15-14-5. - ADDITIONAL USES.

- (A) Uses listed under "additional permitted uses" in Table 1 of this Section shall be permitted in the oCSC district, in the indicated subarea. These uses are in addition to those permitted in the base zoning district.
- (B) Uses listed under "additional special uses" in Table 1 of this Section may be allowed in the oCSC district in the indicated subarea subject to the general provisions set forth in Section 6-3-5 of this Title, and the special provisions contained herein.
- (C) Uses listed under "additional administrative review uses" in Table 1 of this Section may be allowed in the oCSC district in the indicated subarea subject to the general provisions set forth in Section 6-3-5-16 of this Title, and the special provisions contained herein.

TABLE 1: ADDITIONAL USES

Subarea	Additional Permitted Uses	Additional Special Uses	Additional Administrative Review Uses
1	None	None	None
2	None	None	None
3	Retail goods establishment (on ground floor only) Retail services establishment (on ground floor only)	Specialty food store Cultural facility Dormitory Performance entertainment venue	Commercial indoor recreation
4	Specialty food store	Cultural facility Performance entertainment venue	None
5	Specialty food store	Cultural facility Performance entertainment venue	None
6	Automobile service station Specialty food store	Cultural facility Performance entertainment venue	None
7	Specialty food store	Cultural facility Performance entertainment venue	Commercial indoor recreation

(Ord. 5-O-08; Ord. No. 144-O-14, § 2, 12-8-2014; Ord. No. 48-O-21, § 49, 5-10-2021)

6-15-14-6. - PROHIBITED USES.

Uses shown in Table 2 of this Section shall be prohibited in the indicated subarea. This prohibition supersedes any permitted uses identified in the base zoning district.

TABLE 2: PROHIBITED USES

Subarea	Prohibited Uses
1	
2	
3	Drive-through facility
4	Drive-through facility
5	Drive-through facility
6	Drive-through facility (except at 2628 Gross Point Road)
7	Drive-through facility

(Ord. 5-O-08; Ord. No. 69-O-12, § 2, 11-12-2012; Ord. No. 144-O-14, § 3, 12-8-2014)

6-15-14-7. - ACTIVE GROUND FLOOR USES.

In Subareas 3, 4, 5, 6 and 7, active uses shall occupy the ground floor level along the primary street frontage. "Active uses" are hereby defined in the table below along with the matter in which they are allowed in each subarea as either permitted use ("P") or administrative review use ("A") or special use ("S"). In Subareas 3, 4, 5, 6 and 7, active uses shall occupy the ground floor level along the primary street frontage. "Active uses" are hereby defined in the table below along with the matter in which they are allowed in each subarea as either permitted use ("P") or administrative review use ("A") or special use ("S").

	Allowed in:		
Uses:	B1A (Subareas 4, 5 and 6)	O1 (Subarea 3)	C2 (Subarea 7)
Animal hospital	S		P

Artist's studios and accessory dwelling units (provided the accessory dwelling unit shall not front upon any street)	P		
Automobile body repair establishment			S
Automobile and recreational vehicle sales and/or rental			S
Automobile repair service establishment			S
Automobile service station	S		P
Banquet hall	S	S	S
Brew pub	P		P
Business or vocational school		S	S
Cannabis dispensary	S	S	S
Commercial indoor recreation	A	A	A
Convenience store	S		S
Craft alcohol production facility	S		S
Cultural facility	S	S	S
Daycare center—Adult	S		
Daycare center—Domestic animal	S		S
Daycare center—Child	S	S	
Dormitory		S	
Dwelling—Multiple family	S	S	
Educational institution—Private	S		S

Educational institution—Public	S		S
Financial institution (ground level)	A	A	P
Food store establishments	P		P
Government institution	P	P	P
Hotels		P	S
Micro-distillery	S		S
Office (ground level)	A	A	P
Performance entertainment venue	S	S	S
Religious institution	S		S
Resale establishment	S		S
Restaurants—Type 1	P	P	P
Restaurants—Type 2	A		A
Retail goods establishments	P	P	P
Retail service establishments	P	P	P
Tattoo and body art establishment	S	S	S

(Ord. 5-O-08; Ord. No. 144-O-14, § 4, 12-8-2014; Ord. No. 18-O-19, § 1, 4-22-2019; Ord. No. 126-O-19, § 16, 10-28-2019; Ord. No. 31-O-20, § 23, 2-24-2020; Ord. No. 3-O-21, § 15, 1-25-2021; Ord. No. 48-O-21, § 50, 5-10-2021)

6-15-14-8. - BUILDING HEIGHT.

(A) *Maximum Building Height:* The maximum building height in the oCSC district, without bonuses, is shown under "maximum building height (the shorter of)" in Table 3, "Building Height," of this Section in both feet and number of stories. The maximum height is the shorter of the two.

TABLE 3: BUILDING HEIGHT

Subarea	Maximum Building Height (The Shorter Of)		Transitional Height Plane ¹	10% Required Stepback
	Feet	Stories		
1	35	2.5	None	None
2	45	4	R1, R2, R3, R4	None
3	52	5	R1, R2, R3, R4	3rd story and above
4	35	3	R1, R2, R3, R4	10%: 2nd story and above 15%: 3rd story and above ²
5	45	4	R1, R2, R3, R4	2nd story and above
6	45	4	R1, R2, R3, R4	3rd story and above
7	45	4	R1, R2, R3, R4	3rd story and above

Notes:

1. Applies to properties adjacent to the districts listed.
2. See Subsection (C) of this Section, regarding the location of the additional 15 percent setback.

(B) *Transitional Height Plane:* A transitional height plane shall apply in those subareas as indicated under "transitional height plane" in Table 3 of this Section, for properties adjacent to or abutting the districts listed. See Chapter 18 of this Title for details on determining the transitional height plane. This height plane shall be used in place of any transitional height plane height restricting device required by the base district zoning code (for example, the O1 district, [Section 6-15-2-9](#) of this Chapter).

(C) *Required Stepback:* As indicated under "10% required setback" in Table 3 of this Section, a setback from the required pedestrian area of ten percent (10%) of the lot width or depth, as applicable, or five (5) feet, whichever is greater, is required for upper stories. Ten percent (10%) of the lot depth or five (5) feet, whichever is greater, is required for building front stepback. Ten percent (10%) of the lot width or five (5) feet, whichever is greater, is required for building side stepbacks.

1. An additional stepback of fifteen percent (15%) of the lot depth or width from the required pedestrian area required for third story and above in subarea 4 from Hartrey Street in the west to the north leg of Prairie to the east.
2. Buildings with front or side facades of seventy-five (75) feet or more are required to meet this requirement for sixty-five percent (65%) of the second floor front or side facade. Buildings with front or side facades less than seventy-five (75) feet must meet this requirement for one hundred percent (100%) of the second floor front or side facade. The requirements must be met for one hundred percent (100%) of the front or side facade for the third story and above.
3. Stepbacks are required only for building facades that are adjacent to street rights-of-way.

(Ord. 5-O-08; Ord. No. 144-O-14, § 5, 12-8-2014)

6-15-14-9. - FLOOR AREA RATIO.

- (A) The maximum floor area ratio in the oCSC district without bonuses is shown by subarea under "maximum FAR without bonuses" in Table 4 of this Section.
- (B) The maximum floor area ratio in the oCSC district with bonuses is shown by subarea under "maximum FAR with bonuses" in Table 4 of this Section.

TABLE 4: MAXIMUM FAR

Subarea	Maximum FAR Without Bonuses	Maximum FAR With Bonuses
1	None	None
2	None	None
3	1.4	2.0
4	1.4	2.0
5	1.4	2.0
6	1.4	2.0
7	1.0	2.0

(Ord. 5-O-08)

6-15-14-10. - SITE DEVELOPMENT ALLOWANCES.

Site development allowances for any overall building height increase, FAR increase or density (number of dwelling units) increase for planned developments in the oCSC district are not permitted.

(Ord. 5-O-08; Ord. No. 144-O-14, § 6, 12-8-2014)

6-15-14-11. - DEVELOPMENT BONUSES.

Floor area ratio (FAR) bonuses are available and may be approved for developments in the subareas identified in Table 5, "Development Bonuses," of this Section:

TABLE 5: DEVELOPMENT BONUSES

Subarea	Extra Parking	Underground Parking
1		
2		
3		Available
4	Available	Available
5	Available	Available
6	Available	Available
7		Available

(A) *Bonus For Extra Parking:*

1. Bonus Formula: A floor area bonus may be approved for qualifying parking in excess of that required, in accordance with the following standards:

$$\text{Bonus FAR} = \frac{[(\text{number of qualified parking spaces in excess of requirement} \times 350 \text{ square feet}) / \text{lot area}]$$

2. Design Standards and Guidelines: A parking space is qualified if it meets the following standards:
 - (a) It is located on site, specifically identified in the development plan, and legal text indicating that the parking space shall be made available to the general public regardless of whether

they are visiting any of the on-site uses shall be recorded with the property deed.

- (b) It is in excess of the number of on-site spaces required by the Zoning Ordinance.
- (c) It is made available for use to the general public, as well as to on site users.
- (d) If parking spaces are posted as available for a limited time per user, the time period available to general public users shall not be less than the time period for on-site users.
- (e) Aboveground parking garages must be concealed from public view.
- (f) Pedestrian access to the garage must be provided from the public sidewalk.

(B) *Bonus for Underground Parking:*

1. Bonus Formula: A floor area bonus may be approved for qualifying underground parking in subareas as shown under "underground parking" in Table 5 of this Section, in accordance with the following formula:

$$\text{Bonus FAR} = \frac{[(\text{number of qualified underground parking spaces} \times 350 \text{ square feet}) / \text{lot area}]}$$

2. Standards and Guidelines: An underground parking space is qualified if it meets the following standards:
 - (a) Parking spaces must be located entirely below the lowest grade level of any adjacent street frontage.
 - (b) Parking spaces must comply with all parking dimension and access requirements.
 - (c) Vehicular access to the parking garage must be located off an alley.

(Ord. 5-O-08; Ord. No. 144-O-14, § 7, 12-8-2014)

6-15-14-12. - PEDESTRIAN AREA REQUIREMENTS.

- (A) Location: A pedestrian area shall be located between the front facade of all buildings and the curb along Central Street, Green Bay Road, Gross Point Road and Crawford Avenue and along all intersecting streets for properties occupied by nonresidential uses. Each pedestrian area shall consist of two (2) zones parallel to the curb: a sidewalk clear zone and a parkway/street furniture zone Each subarea shall have a minimum width of the Pedestrian Area as specified in Table 6, "Pedestrian Area Requirements," of this Section.

TABLE 6: PEDESTRIAN AREA REQUIREMENTS

Subarea	Minimum Width Along Central, Green Bay, Gross Point, Crawford	Minimum Width Along Side Street Frontage

	(A) Clear Zone	(B) Parkway/Street Furniture Zone	(C) Minimum Pedestrian Area Width ((A) + (B))	(D) Clear Zone	(E) Parkway/Street Furniture Zone	(F) Minimum Pedestrian Area Width ((D) + (E))
1	5 feet	9 feet	14 feet	5 feet	9 feet	14 feet
2	5 feet	9 feet	14 feet	5 feet	9 feet	14 feet
3	8 feet	6 feet	14 feet	10 feet	10 feet	20 feet
4	8 feet	6 feet	14 feet	10 feet	10 feet	20 feet
5	8 feet	6 feet	14 feet	10 feet	10 feet	20 feet
6	15 feet	15 feet	30 feet	15 feet	15 feet	30 feet
7	8 feet	6 feet	14 feet	10 feet	10 feet	20 feet

(B) Sidewalk Clear Zone Requirements:

1. The sidewalk clear zone shall be a minimum width as specified in Table 6 of this Section, shall be located immediately contiguous to the parkway/street furniture zone and shall be continuous.
2. This zone shall be hardscape, and shall be unobstructed for a minimum height of eight (8) feet.
3. Building entryway doors shall not open in a manner that causes them to swing into or in any way obstruct the sidewalk clear zone.

(C) Parkway/Street Furniture Zone Requirements: The parkway/street furniture zone shall have a minimum width as specified in Table 6 of this Section. This zone shall be located immediately adjacent to the curb and shall be continuous. This zone shall be landscaped or hardscaped, and may be used for the placement of trees, street furniture, benches, waste receptacles, fire hydrants, traffic signs, newspaper vending boxes, bus shelters, bicycle racks and similar elements in a manner that does not obstruct pedestrian access or motorist visibility.

(D) Vehicle Sightlines and Visibility: Nothing shall be erected, placed, planted or allowed to grow in such a manner as to impede visibility within visibility triangles at street intersections between the heights of two and one-half (2½) feet and eight (8) feet above grade.

(E) Utilities: Every commercially reasonable effort shall be made to place utilities underground or to the rear

of structures to allow for unobstructed use of sidewalks.

- (F) Relationship of Building to Pedestrian Area: Each building with at least one (1) facade adjacent to the pedestrian area shall have a pedestrian entrance located on the ground floor of that facade and opening directly onto the pedestrian area. Such entrance shall be unlocked and accessible during business hours.
- (G) Optional Sidewalk Setback Area: Buildings may be set back an additional distance up to ten (10) feet from the pedestrian area. The resulting area between the front facade and the sidewalk may be used for landscaping, seating, public art, street furniture, outdoor dining, outdoor display of merchandise during business hours, or other similar uses, provided they are permissible for that location otherwise.
- (H) The front yard and street side yard requirements by the underlying zoning district in Subareas 3, 4, 5, 6 and 7 along Central Street, Green Bay Road, Gross Point Road and Crawford Avenue, as applicable, is hereby replaced with the pedestrian area requirement and does not have to be satisfied.

(Ord. 5-O-08; Ord. No. 144-O-14, § 8, 12-8-2014)

6-15-14-13. - MINIMUM BICYCLE PARKING REQUIREMENTS.

- (A) All nonresidential developments which provide automobile parking facilities shall provide bicycle parking facilities (bike racks) at a ratio of at least one (1) bicycle parking space for every ten (10) automobile parking spaces.
- (B) Multi-family developments shall provide interior bicycle parking facilities for building residents at a ratio of at least one (1) bicycle parking space for every one and one-half (1.5) dwelling units. Such bicycle parking facilities must be provided near the building entrance accessible to the street. Multi-family developments shall also provide bicycle parking facilities for visitors/public at a ratio of at least one (1) bicycle parking space for every ten (10) automobile parking spaces provided and shall meet the location criteria under subsection (D) of this section.
- (C) No development, except a one- or two-family development, shall have fewer than three (3) bicycle/moped parking spaces nor be required to exceed a maximum of ten (10) such spaces.
- (D) Bicycle parking facilities for nonresidential developments and bicycle parking facilities provided for the visitors and/or public shall be located within the parkway/street furniture zone a maximum distance of fifty (50) feet from the building entrance, or shall be located at least as close as the closest automobile space.
- (E) Each bicycle parking facility shall include a metal anchor sufficient to secure the bicycle frame when used in conjunction with a user supplied lock.

(Ord. 5-O-08; Ord. No. 144-O-14, § 9, 12-8-2014)

6-15-14-14. - FENESTRATION.

- (A) Ground level nonresidential uses that face a public way shall provide a minimum linear fenestration of sixty-five percent (65%), measured along the length of the street frontage. Corner buildings shall continue such fenestration around the corner of the building for at least forty (40) feet or the length of the building, whichever is shorter.

(B) Qualifying frontage shall meet the following conditions:

1. Lower edge of lite (sill) is no higher than three (3) feet above grade.
2. Upper edge of lite (header) is no lower than ten (10) feet above grade.

(C) Fenestration shall utilize clear glass. Painted glass, reflective glass or other similarly treated or opaque windows are not eligible. Entrances with glass elements may be counted towards fenestration requirement, as long as qualifying fenestration extends above the entrance to the minimum height specified above.

(Ord. 5-O-08; Ord. No. 144-O-14, § 10, 12-8-2014)

6-15-14-15. - BUILDING FACADE ARTICULATION.

For all building facades facing public streets:

- (A) The length of facade without intervening fenestration or entryway shall not exceed twenty (20) feet.
- (B) Building floors from ground level to third story above ground level shall be delineated through the use of windows, belt courses, cornice lines or similar architectural detailing.
- (C) Facades wider than twenty-five (25) feet shall be vertically articulated to give the appearance of being composed of multiple buildings. Such articulation shall be accomplished by using projections, recesses, material changes, parapets, cornices, varying roof heights/lines, or other similar architectural features.
- (D) Prohibition of Sheet Walls: No more than twenty-five (25) feet of width of any building facade shall form a "sheet wall" from ground level to the topmost floor. A "sheet wall" is defined as a vertical unbroken plane of facade that is unarticulated in depth.

(Ord. 5-O-08; Ord. No. 144-O-14, § 11, 12-8-2014)

6-15-14-16. - ALLEY ACCESS.

New nonresidential or mixed use construction in the oCSC overlay district on zoning lots that abut a public alley narrower than eighteen (18) feet in width must provide a clear area immediately abutting and parallel to the public alley extending to a depth of eighteen (18) feet from the opposite edge of the existing alley. This area may not be used for parking and is to remain free of all obstructions, including, but not limited to, fences, posts, bollards, retaining walls, dumpsters, garbage cans, etc. Relief from this requirement based on exceptional site conditions or other practical difficulties may be granted by approval of the Zoning Administrator and the director of public works.

(Ord. 5-O-08)

6-9-5. - B1a BUSINESS DISTRICT.

6-9-5-1. - PURPOSE STATEMENT.

- (A) The B1a business district is designed to accommodate and encourage the continued viable use of neighborhood business districts and pedestrian oriented shopping areas found throughout the City primarily on arterial roadways and often adjacent to single-family residential areas.
- (B) These shopping and business areas are primarily neighborhood oriented, however, they can also accommodate specialty stores and service facilities that attract a larger market area. The established physical pattern of the district is typically buildings built to or near the front lot line and continuous bands of storefronts. The provisions of this district are intended to maintain pedestrian character and scale within the context of surrounding residential areas.
- (C) Uses in the B1a business district may include businesses catering to the daily shopping needs of neighborhood residents, specialty stores that provide retail opportunities that have broader market appeal, retail service uses and professional service uses, offices and financial institutions without drive-up facilities. No individual use in this district should exceed twenty thousand (20,000) square feet in size.

(Ord. 136-O-05)

6-9-5-2. - PERMITTED USES.

The following uses, provided they are twenty thousand (20,000) square feet or less in size, are permitted in the B1a district:

Artist studios and accessory dwelling units (provided the accessory dwelling unit shall not front upon any street).

Brew pub.

Caterer.

Cultural facility.

Dwellings (above ground floor).

Educational institution—Private.

Educational institution—Public.

Financial institution.

Food store establishment (hours between 6:00 a.m. and 12:00 midnight).

Governmental institutions.

Live-work units (subject to the general requirements of Section 6-4-13 of this Title).

Neighborhood garden.

Offices.

Religious institution.

Residential care home—Category I.

Restaurant—Type 1.

Retail goods establishment.

Retail services establishment.

(Ord. 136-O-05; Ord. No. 81-O-14, § 33, 8-11-2014; Ord. No. 105-O-18, § 9, 10-8-2018; Ord. No. 82-O-21, § 4, 9-13-2021)

6-9-5-2.5. - ADMINISTRATIVE REVIEW USES.

The following uses may be allowed in the B1a district, subject to the provisions set forth in Section 6-3-5-16, "Administrative Review Uses," of this Title:

Commercial indoor recreation.

Restaurant—Type 2.

(Ord. No. 48-O-21, § 13, 5-10-2021)

6-9-5-3. - SPECIAL USES.

The following uses may be allowed in the B1a business district, subject to the provisions set forth in Section 6-3-5, "Special Uses," of this Title:

Administrative review uses, pursuant to Section 6-3-5-16(B) "Applicable Uses".

Animal hospital.

Aquaponics.

Banquet hall.

Cannabis craft grower.

Cannabis dispensary (subject to the general requirements of Section 6-4-11 of this Title).

Cannabis infuser.

Cannabis transporter.

Commercial outdoor recreation.

Convenience store.

Craft alcohol production facility.

Daycare center—Adult.

Daycare center—Child.

Daycare center—Domestic animal.

Drive-through facility (accessory or principal).

Dwelling—Multiple-family.

Food store establishment.

Funeral services, excluding on-site cremation.

Independent living facility.

Kennel.

Membership organization.

Micro-Distillery.

Planned development.

Public utility.

Resale establishment.

Tattoo and body art establishment (subject to the general requirements of Section 6-4-12 of this Title).

Trade contractor (provided there is no outside storage).

Urban farm, rooftop.

Uses permitted pursuant to Section 6-9-5-2 of this Chapter and this Section exceeding twenty thousand (20,000) square feet.

Vocational training facility.

(Ord. 136-O-05; amd. Ord. 122-O-09; Ord. No. 78-O-10, § 5, 11-8-10; Ord. No. 71-O-10, § 6, 10-25-10; Ord. No. 67-O-11, § 6, 9-12-2011; Ord. No. 129-O-12, § 6, 1-14-2013; Ord. No. 3-O-14, § 6, 2-10-2014; Ord. No. 56-O-14, § 6, 5-27-2014; Ord. No. 57-O-14, § 6, 6-9-2014; Ord. No. 81-O-14, § 7, 8-11-2014; Ord. No. 8-O-17, § 4, 4-24-2017; Ord. No. 105-O-18, § 10, 10-8-2018; Ord. No. 126-O-19, § 11, 10-28-2019; Ord. No. 31-O-20, § 12, 2-24-2020; Ord. No. 3-O-21, § 12, 1-25-2021; Ord. No. 48-O-21, § 14, 5-10-2021)

6-9-5-4. - LOT SIZE.

The minimum lot sizes in the B1a business district are:

- (A) *Nonresidential*: There is no minimum lot size requirement for nonresidential uses in the B1a business district.
- (B) *Residential*: The minimum residential lot size in the B1a business district is nine hundred (900) square feet for each dwelling unit.

Any structure, legally established as of the effective date hereof, with a number of dwelling units that exceeds the maximum permitted under this Section, that is damaged or destroyed, by any means not within the control of the owner, to the extent of one hundred percent (100%) of the cost of replacement of such structure new, may be repaired or restored; provided however, that no repair or restoration shall be made that would create any new noncompliance not existing prior to such damage or destruction.

(Ord. 136-O-05)

6-9-5-5. - LOT WIDTH.

There is no minimum lot width requirement in the B1a business district.

(Ord. 136-O-05)

6-9-5-6. - FLOOR AREA RATIO.

The maximum floor area ratio permitted in the B1a business district is 2.0.

(Ord. 136-O-05)

6-9-5-7. - YARD REQUIREMENTS.

The minimum yard requirements for the B1a business district are as follows:

(A)	Front yard	Building on corner property or property comprising forty percent (40%) or more of the block width, excluding the public right-of-way, three (3) feet
		Building on property comprising less than forty percent (40%) of block width, excluding public right-of-way, the lesser of three (3) feet or depth on same public right-of-way of any existing street-facing facades of principal buildings on abutting properties
		Open, unenclosed parking and appurtenant areas at grade, including driveway aisles and ramps, prohibited
(B)	Side yard when abutting street designated major street in the comprehensive general plan	Building, three (3) feet
		Open, unenclosed parking and appurtenant areas at grade, including driveway aisles and ramps, prohibited
(C)	Side yard when abutting street designated collector street or distributor street in the comprehensive general plan	Building, none, if front yard abuts a designated collector street, distributor street, or major street in the comprehensive general plan

		Building, three (3) feet, if front yard abuts a street not designated as a collector street, distributor street, or major street in the comprehensive general plan
		Open, unenclosed parking and appurtenant areas at grade, including driveway aisles and ramps, prohibited
(D)	Side yard when abutting street not designated as a collector street, distributor street, or major street in the comprehensive general plan	Building, none; open, unenclosed parking and appurtenant areas at grade, including driveway aisles and ramps, prohibited
(E)	Side yard when not abutting a street but abutting residential district	Building, ten (10) feet; parking, five (5) feet
(F)	Side yard when not abutting street and not abutting residential district	Building, none; parking, five (5) feet
(G)	Rear yard when abutting residential district	Building, twenty-five (25) feet; parking, fifteen (15) feet
(H)	Rear yard when abutting nonresidential district	Building, fifteen (15) feet; parking, five (5) feet

(Ord. 136-O-05; amd. Ord. 43-O-09)

6-9-5-8. - BUILDING HEIGHT.

The maximum building height in the B1a business district is forty (40) feet or three (3) stories, whichever is less.

Any structure, legally established as of the effective date hereof, with a building height that exceeds the maximum permitted under this Section, that is damaged or destroyed, by any means not within the control of the owner, to the extent of one hundred percent (100%) of the cost of replacement of such structure new, may be repaired or restored; provided however, that no repair or restoration shall be made that would create any new noncompliance not existing prior to such damage or destruction.

(Ord. 136-O-05)



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Fri, Jun 10, 2022 at 4:21 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/10/22 5:21 PM

Name: Diane Alfille

Address of Residence: 2620 Princeton Avenue

Phone: (847) 475-4751

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I am strongly against removing the central street zoning overlay from central street north to Sarkis & including all the businesses next to our part of Central & Gross point streets.

I think it's dangerous to remove a level of zoning protection and potentially open the area up to drive throughs other unsuitable options in a busy pedestrian area. Residents of this part of extreme Northwest Evanston use this area to walk & play. Most homes now have young families with children running, walking & playing all over our neighborhood. And if the Kensington School opens, the traffic patterns will change and the volume of traffic & congestion will be dramatically greater. This will likely also force residents to exit Princeton and Central Street via Thayer to Crawford during school pick ups & drop offs, affecting traffic flows even more, so I think the zoning requirements that exist should absolutely remain in place! As a longtime resident of Princeton (I moved here in 1993!) I am strongly opposed to changing the zoning overlay for our area!!

Agenda Item (or comment

22PLND-0038 Central Street Zoning Overlay - Land Use

**on item not
on the
agenda):**

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Thu, Jun 16, 2022 at 12:02 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/16/22 1:02 AM

Name: Diane Alfille

Address of Residence: 2620 Princeton Avenue Evanston IL, 60201

Phone: (847) 475-4751

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:
I am strongly opposed to the changes to our quality of life Map Amendment 22PLND-0038 would allow. I have lived here since 1993 and do not want to see the changes this would allow:
Taller buildings
Buildings that abut the sidewalk (3 feet from street) instead of being set back to help traffic sightlines
Restaurants with drive-thrus
Precedence to change zoning for other properties in the area: CVS, Alden, Berglund, and the Arnel office buildings between Gross Point and Crawford.
Please vote no to this dramatically negative zoning change.

Agenda Item (or comment on item not on the agenda): Map Amendment 22PLND-0038

Position on Agenda Item: Opposed



[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Fri, Jun 10, 2022 at 3:52 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/10/22 4:52 PM

Name: Amy Blanton

Address of Residence: 2710 Princeton Ave

Phone: (919) 360-3086

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

As a long time resident of Princeton Ave, we strongly object to the Central Street Overlay being removed from the Gross Point/Crawford/Central intersection. Neighborhood residents are well aware that lifting this overlay would remove the regulations on drive throughs...which nearly everyone in the surrounding neighborhoods oppose. as a parent of school age children, who often take the bus to school, we believe that this area does not need LESS regulation, but potentially more. we experience car accidents often at this intersection so much so that i personally think this area needs some serious rehab. Removing the drive through restrictions and allowing some giant chain food place bring exponentially more traffic to that intersection would be devastating to the safety of everyone that crosses at that intersection.

Agenda Item (or comment on item not on the agenda):

Central Street Overlay District removal

**Position on
Agenda
Item:** Opposed

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Formstack, [11671 Lantern Road, Suite 300, Fishers, IN 46038](#)



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 8:30 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/11/22 9:30 AM

Name: Leslie Brown

Address of Residence: 3517 Central St

Phone: (847) 275-8832

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: Dear members of the committee:

I am writing in regard to 22PLND-0038 and the Central Street Overlay's zoning issue. I live and walk every day on Gross Point Rd - so many people in our neighborhood walk on Gross Point to get to the businesses there, to access the bus stop, and to walk to the park . I ask you to consider the impact of removing that overlay.

By removing the overlay, there is more chance for developers to build properties up, and drive-thru restaurants have an easier opportunity to build there. I understand Sarkis wants to put a permanent tent on its property, and I don't have an issue with that. I wish you could just grant that separately. The tent won't increase traffic that could be dangerous, but drive-thrus and larger developments will cause more traffic in an already dangerous intersection. Accidents happen regularly.

Please think about the families who live over here, the children who walk to the bus stop on Central and Gross Point, people who walk to Little Island (and the other businesses there) to retrieve their dinner and other goods, and the

animal owners who traverse those streets in order to get to the park. Taking away the overlay district for Little Island, Will-Ridge, the Paper Company, and Sarkis doesn't make any sense and is extremely short sighted for the safety of the neighborhood.

Thank you for your consideration.

Agenda Item (or comment on item not on the agenda):

22PLND-0038

Position on Agenda Item:

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Thu, Jun 16, 2022 at 9:56 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/16/22 10:56 AM

Name: John Cooper

Address of Residence: 3614 Hillside Rd

Phone: (847) 866-9100

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

First, what is the reason FOR such a change. There must have been a reason to begin with
I am not sure about safety concerns, that is complex, but it is not a very safe corner in my opinion.
Mostly, the Sarkis tent is an eyesore as is their lot. The tent is unacceptable. OK for covid, but looks like a garbage dump. If they want to make more money, let them build out like other businesses needs to do, with building codes etc. Or, if they want to put a patio out there... good, cover it, and, go by normal building codes. Again, what is the safety reason or any reason for doing this?

Agenda Item (or comment on item not on the agenda): Sarkis

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Melissa Klotz <mklotz@cityofevanston.org>

Land Use Commission Public Comment

1 message

noreply@formstack.com <noreply@formstack.com>

Wed, Jun 15, 2022 at 7:05 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/15/22 8:05 AM

Name:	Jerie Dahlman
Address of Residence:	2650 Hillside Lane
Phone:	
How would you like to make your public comment?:	In-person
Provide Written Comment Here:	
Agenda Item (or comment on item not on the agenda):	Amendment 22PLND-0038 - Zoning change
Position on Agenda Item:	Opposed

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Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 7:49 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/11/22 8:49 AM

Name: Nathan Elwell

Address of Residence: 2502 Princeton Ave

Phone: (847) 530-0249

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

As a resident of the neighborhood, I am very concerned over the removal of zoning restrictions around Central/Gross Point/Crawford. The traffic congestion is already an issue and I am very opposed to any kind that would lead to larger buildings or drive thru operations of any kind. My family and I walk our dogs through that area daily and removal of the restrictions could significantly impact the neighborhood feel of the area and lead to many other issues associated with drive thrus. While not overly pleased about the Kensington School plan, I am notnopropped to it because I know business has to be allowed but these othe mr restrictions are needed to keep our neighborhood intact. 22PLND-0038.

Agenda Item (or comment on item not on the agenda):

22PLND-0038.

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 1:01 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/11/22 2:01 PM

Name: Fernando Ferrer

Address of Residence: 3520 Hillside Rd.

Phone: (773) 550-7020

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I am against an special use for the Sarkis Lot. They have the ugliest storefront in town. And no I don't want to allow for a permanent tarp to be built or any drive threw to be allowed. I loved what they had. They had character. I love the art on the side but the tarps are horrible.

Agenda Item (or comment on item not on the agenda): 22PLND-0038

Position on Agenda Item: Opposed



[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 5:14 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/11/22 6:14 AM

Name: Kim Ferrer

Address of Residence: 3520 Hillside Rd

Phone: (847) 917-3558

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I'm opposed to removing Sarkis, Wilridge and Little Island from the central street plan. This intersection is used by many, along with the hillside community to walk to school, Lovelace, etc and it's very dangerous. In fact, my teenagers can not get anywhere in Evanston without using this intersection. I also use this intersection daily to walk, etc and feel this is the most dangerous intersection in Evanston. I know several neighbors nearly missed or hit by motorists. the central plan is in place for a reason and those reasons exist for this intersection. Also, the current zoning does NOT allow for drive thru businesses. I am NOT in favor of allowing drive thrus for this intersection for many reasons. Come spend some time trying to cross the street on foot or bike here and you'll understand - allowing this could be beyond dangerous.

Agenda Item (or comment on item not on the agenda): 22PLND-0038.

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Mon, Jun 13, 2022 at 12:12 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/13/22 1:12 PM

Name: Karen Flood

Address of Residence: 2525 Princeton Avenue

Phone: (847) 869-3503

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: Under consideration is the issue of 22PLND-0038. I oppose any action that removes the Central Street corridor restrictions. My main concern is turning an already heavily used intersection into a worse one by providing drive through access to business. Thank you, Karen Flood

Agenda Item (or comment on item not on the agenda): 2PLND-0038.

Position on Agenda Item: Opposed

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Formstack, [11671 Lantern Road, Suite 300, Fishers, IN 46038](#)



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Fri, Jun 10, 2022 at 6:41 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/10/22 7:41 PM

Name: Tony Frenzel

Address of Residence: 2665 Crawford Avenue

Phone: (312) 371-8295

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: Please do not allow excessive commercial zoning to happen. No drive-throughs! It creates extra traffic and more chances for pedestrian accidents on a busy Central/Crawford road. All residents are AGAINST this.

Agenda Item (or comment on item not on the agenda): No drive-throughs

Position on Agenda Item: Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Thu, Jun 16, 2022 at 8:55 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/16/22 9:55 PM

Name: Jordan Goldberg

Address of Residence: 3420 Park Place

Phone: (773) 505-9911

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I am opposed to the change in zoning for the west side of gross point road. There are several children, including my own, that walk in this area. I do not want to increase traffic, and I do not want there to be future plans for drive thrus which may result in pedestrian harm.

Agenda Item (or comment on item not on the agenda): Rezoning

Position on Agenda Item: Opposed



[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Fri, Jun 10, 2022 at 10:08 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/10/22 11:08 PM

Name: Wayne Heimbach

Address of Residence: 3613 CENTRAL ST

Phone: (847) 414-5160

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: Things are moving too quickly in the neighborhood and not for the better. The hearing on the agenda item (22PLND-0038) should reject it since it could bring too many consequences here and not add anything for residents. Evanston should be able to do better.

Agenda Item (or comment on item not on the agenda): 22PLND-0038

Position on Agenda Item: Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Wed, Jun 15, 2022 at 7:24 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/15/22 8:24 AM

Name: Gilbert Lutz

Address of Residence: 3425 Park Place

Phone: (419) 874-1222

How would you like to make your public comment?: Written (see below)

I live within 500 feet of this rezoning proposal and am opposed to making changes that could impact the future walkability of the intersection at Gross Point/Crawford/Central. I am a visually impaired senior citizen who uses a seeing eye dog and the intersection is already extremely difficult for pedestrians and for vehicles.

Provide Written Comment Here: Removing the overlay means that larger buildings can be built closer to the street which can reduce sight lines, and it would remove the drive through prohibition. There have already been several attempts for businesses with drive throughs in this area.

The residents of this neighborhood supported the Central Street Master Plan when it was being developed and want the same zoning vision and protection that the rest of the Central Street Corridor has.

Please don't make this significant change and set precedent for removing other parcels in the area from the overlay.

Agenda 22PLND-0038

**Item (or
comment
on item not
on the
agenda):**

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 10:34 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: **Land Use Commission Public Comment**

Submitted at 06/11/22 11:34 AM

Name: Mike Murray

Address of Residence: [3530 Hillside Rd, Evanston](#)

Phone: (650) 387-3648

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I want to give tentative opposition to changes in the central street overlay zoning. I have two concerns and one hope:

Concern 1: Spillover traffic on Hillside Rd and Hillside Lane. The city must take into consideration any development that will force extra traffic and parking onto these two streets. At the moment, sidewalks are not on both sides of the street, are not to regulation, and young children freely play with their neighborhood friends in the street. I'll be honest - I will not support any development that will force kids off the street to accommodate cars. I can send my pre-K and kindergarten girls out to play in the street now with friends, the kids should continue to have dominion over the neighborhood. This is what a neighborhood should aspire to be. If accommodating a McDonald's or bank branch forces the kids to stay inside, we are worse for it. Please come visit Hillside on a summer day and you will see groups of children freely playing as children ought to.

Concern 2: Drive-thrus - almost all development proposed in the area since we moved to this neighborhood has been drive thru based. There is a very limited amount of retail zoned land here, I am hopeful that something that

would serve the community will work and not just be a dunkin donuts or something for distracted drivers.

Hope 1: If the needle can be threaded to encourage more retail that will serve the local neighborhood and limit impact to my concerns above, I will support. If Sarkis wanted to be open for dinner and serve beer, and needs a better building, I will support it. If the CVS was razed and replaced with retail and residential above, I could even see that working!

Agenda Item (or comment on item not on the agenda):

Central Street Overlay zoning (22PLND-0038)

Position on Agenda Item:

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Wed, Jun 15, 2022 at 7:43 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/15/22 8:43 AM

Name: Jacob Noble

Address of Residence: 3445 Park Place

Phone: (312) 608-3175

How would you like to make your public comment?: Written (see below)

I oppose the proposed amendment to remove certain properties along this stretch from the Central Street Overlay district on the grounds that it will reduce walkability for surrounding neighbors. This walkability factor was a big factor in my family's decision many years ago to support the Central Street overlay many years ago.

Provide Written Comment Here: Our block of Park Place (3400 block) has many young children that walk to and from the bus stop on Central in order to get to and from school. Traffic at the Gross Point Road/Central/Crawford intersection is already extremely dangerous with many traffic accidents. Removing these properties from the Central Street Overlay increases the potential for redevelopment of certain commercial properties in a manner that will have a detrimental effect on on out children's safety when walking this already dangerous area. This is despite the fact that resining seems unnecessary as most of the properties are already home to established commercial businesses that have been open for many years.

Agenda Item (or Map Amendment 22PLND-0038

**comment
on item not
on the
agenda):**

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Thu, Jun 16, 2022 at 3:30 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/16/22 4:30 PM

Name: Jennifer Packman

Address of Residence: 2744 Crawford Ave

Phone: (847) 425-9662

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: It is my understanding that zoning changes are necessary when the character of an area changes and needs are different.

This amendment proposes to change the zoning at an intersection where the children of the neighborhood catch the bus, where everyone coming from that direction must cross with their dogs and kids to get to Lovelace Park, where parents cross with their children laden with towels and float toys every day of the summer to get to the pool, where we all cross every time we need to run to CVS to pick something up or grab a latte at Starbucks.

How has any of this changed? How does this amendment improve the safety and well-being of the people and their kids and pets who use this intersection every day?

The only change that may be happening appears to be that the church at the intersection may be replaced by a large preschool. It hardly seems like it needs to be pointed out that this will mean there are even more children using this intersection. Many, many tiny children.

Frankly, the idea of changing zoning to make an area less pedestrian friendly while at the same time considering putting a preschool into the same intersection is completely nonsensical.

Agenda Item (or comment on item not on the agenda):

Map Amendment 22PLND-0038

Position on Agenda Item:

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sun, Jun 12, 2022 at 10:34 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/12/22 11:34 AM

Name: Sigrid Pilgrim

Address of Residence: 2750 Bernard Place

Phone: (224) 420-0949

How would you like to make your public comment?: Written (see below)

I frequently go through Lovelace Park to visit CVS at Central Street, which means that I do use Gross Point Road as a pedestrian. I also see many other people walking on the sidewalks especially on their way to Sarkis. This will certainly increase once the Kensington School is opened.

Provide Written Comment Here:

I therefore urge your Commission to retain the existing Central Street Overlay protection for this area, including Sarkis, Hot Dog Island, the Wilshire Property and related areas. I also believe it is important to maintain the unique character of our neighborhood. Removing the overlay protection will open up the undesirable possibility or bringing in businesses that are incompatible with the preferences of this area's residents.

I urge you to retain the existing Central Street Overlay protection. Thank you.

Agenda Item (or comment on item not

22PLND-0038

**on the
agenda):**

**Position on
Agenda
Item:** Opposed

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Formstack, [11671 Lantern Road, Suite 300, Fishers, IN 46038](#)



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 8:29 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/11/22 9:29 AM

Name: Rachel Elwell

Address of Residence: 2502 Princeton Avenue

Phone: (847) 800-5631

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I do not want the zoning regulations changed. This is where my family and I walk our dogs and spent time outside. A change in zoning regulations would change the neighborhood significantly.

Agenda Item (or comment on item not on the agenda): 22PLND-0038

Position on Agenda Item: Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 8:39 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/11/22 9:39 AM

Name: Marlene Turvill

Address of Residence: 2535 Princeton Ave

Phone: (847) 644-4859

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I am opposed to removing businesses on the west side of Ridge Ave, immediately north of Central Street, from the Central Street Overlay zoning. Removing that additional layer of zoning would make an already overly busy and difficult set of intersections too hazardous. It would also allow for drive-through businesses which would dramatically increase the congestion and safety of that area, especially to the many people who live in that area and need to walk there to get from place to place.

Agenda Item (or comment on item not on the agenda): 22PLND-0038

Position on Agenda Item: Opposed



[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Wed, Jun 15, 2022 at 8:35 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/15/22 9:35 PM

Name: Don Westphal

Address of Residence: 2736 Bernard Pl

Phone: (847) 905-0935

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: This is a very dangerous corner already as statistics proves, changing zoning to increase business traffic will cost lives.

Agenda Item (or comment on item not on the agenda): 22PLND-0038.

Position on Agenda Item: Opposed

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Formstack, 11671 Lantern Road, Suite 300, Fishers, IN 46038



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Thu, Jun 16, 2022 at 9:29 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/16/22 10:29 AM

Name: Marc Wilson

Address of Residence: 3517 Central St.

Phone: (847) 275-8868

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I want to express my opposition to 22PLND-0017.

I don't want anymore drive-thru establishments in our neighborhood, (Starbucks is enough and already congested!) and this will make that far easier to take place. I walk all the time on the west side of Gross Point Road, as do many of us on Central and along Crawford, as we head toward Lovelace park.

Please keep it safe. If you change it, the walkability will be worse. I do believe Sarkis should be able to have a permanent tent, but don't change everything that currently works to make that happen, please.

It opens up so many more Issues if you remove the existing zoning. We already have so many accidents there.

Thank you!

Agenda Item (or 22PLND-0017

**comment
on item not
on the
agenda):**

**Position on
Agenda
Item:**

Opposed

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Sat, Jun 11, 2022 at 9:46 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/11/22 10:46 AM

Name: Pam Winkler

Address of Residence: 3614 Hillside Road

Phone: (773) 294-6313

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: This is regarding 22PLND-0038.. I live on Hillside Rd and oppose changing the Central Street Zoning overlay that affects the businesses near Central st/Crawford ave/Gross Point Rd. Changing the zoning and allowing through ways would negatively impact the quiet and easy walking that all of us residents have in this area.

Agenda Item (or comment on item not on the agenda): 22PLND-0038.

Position on Agenda Item: Opposed

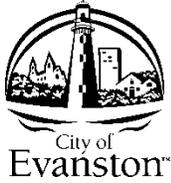


[Quoted text hidden]

3434 Central Street
Kensington School

Planned Development
22PLND-0012

LUC Recommending Body



Memorandum

To: Chair and Members of the Land Use Commission

From: Johanna Nyden, Director of Community Development
Michael Griffith, Planner

Subject: Planned Development
3434 Central Street, 22PLND-0012

Date: June 17, 2022

Request

The applicant applies for a Special Use for a Planned Development and a Special Use to demolish the existing church and other site improvements and to construct a new 2-story, 22,416 square foot building for a Daycare Center-Child, Kensington School. The following site development allowances are needed:

1. Off-street parking located within the front yard where parking is not permitted.
2. Off-street parking located within the south interior side yard where parking is not permitted.
3. Detached accessory use, refuse enclosure, located within the south interior side yard where a detached accessory use is not permitted.
4. Reduce the required transition landscape strip along the south property from 10' to 6'.
5. Eliminate the required 10-foot wide transition landscape strip along the west property line.
6. Reduce the two-way driveway aisle width from 24' to 16'.

The applicant may seek and the Land Use Commission may consider additional Site Development Allowances as may be necessary or desirable for the proposed development.

Notice

The Application has been filed in conformance with applicable procedural and public notice requirements including publication in the Evanston Review on June 2, 2022.

General Information

Applicant: Charles Marlas
Kensington School
743 McClintock Drive
Burr Ridge, IL 60527

Owner(s): Unity Church on the North Shore
3434 Central Street
Evanston, IL 60201

Existing Zoning: R2 Single-Family Residential District

Existing Land Use: Religious Institution (Unity Church on the North Shore)

Property Size: 57,942.76 sq. ft. (1.33 acres)

PIN: 10-10-200-073-0000

Surrounding Zoning and Land Uses	Zoning	Land Use
North	R2 Single-Family Residential	Dwelling - Single-family detached
South	R2 Single-Family Residential	Dwelling - Multiple-family
East	R4 General Residential District/oCS Overlay District	Dwelling - Multiple-family and Office
West	R2 Single-Family Residential District	Dwelling - Single-family detached, Public utility (water tower)

Analysis

Project Description

The applicant is proposing to demolish the existing church and other site improvements and to construct a 2-story, 22,416 square foot building for a Daycare Center-Child, Kensington School, playground areas, with 34 on-site parking spaces.

Kensington School is a private nursery, preschool, and kindergarten providing full-day programming. The proposed daycare center will accommodate 160-165 students and have approximately 23 staff. The facility will operate between 6:30 a.m. - 6:30 p.m.

A Daycare Center-Child is a special use in the R2 district.



3434 Central Street - rendering of proposed building, looking towards the southwest

Site Layout

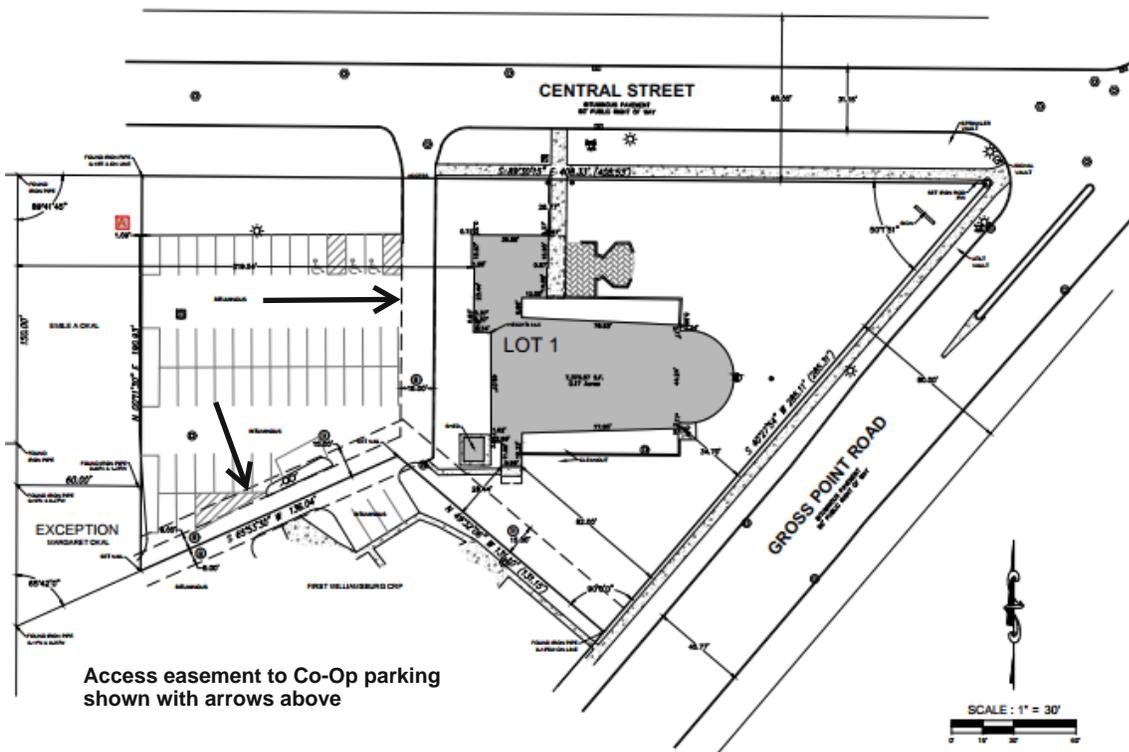
The site is located at the southwest corner of the intersection of Central Street and Gross Point Road. Residential uses surround the site generally, with detached single-family dwellings to the north and west and multi-family dwellings to the south and east. To the east and northeast of the site are office and commercial uses, and to the southwest is a City water tower.

Current improvements on the property include the existing church, Unity Church on the North Shore, a shed, and a parking lot accommodating 46 spaces. There is one full access curb cut off Central Street currently providing access to the church parking lot and to the multi-family dwellings to the south, 2538-2548 Gross Point Road, First Williamsburg Co-Op. The ALTA/NSPS Land Title Survey for the subject property notes a 16-foot wide easement extending south off Central Street through the existing church parking lot to the common property line with the Co-Op then extending to the southwest along the common property line providing access to the Co-Op's parking spaces. The Co-Op's parking, located at the rear of their property, does not have direct access to a public street.

The site is marked on the image below with an orange line.



3434 Central Street - aerial shot noting site boundary



Access easement to Co-Op parking shown with arrows above

3434 Central Street - land title survey, excerpt

The proposed site plan places the building approximately in the center of the property with playground areas on the west side and the parking lot on the east side of the site. The applicant provided and staff reviewed several site plan layouts and the one proposed was determined to be the best site plan in terms of the parking lot and playground locations, and on-site circulation. The parking lot is proposed to have a full access driveway off Gross Point Road and a right-out only exit onto Central Street. A total of 34 parking spaces are provided, including 2 accessible spaces. The parking lot provides spaces for caretakers to drop-off and pick-up their children and for staff parking. The number of parking spaces provided complies with the zoning code.

Bike parking is shown at the northeast corner of the building, near the main entrance into the building. The bike parking area appears to be able to accommodate cargo and trailers pulled behind bikes. The specific details can be reviewed by the Design and Project Review Committee (DAPR) at the time of a building permit review.

A refuse enclosure is located to the south of the building and screened by a 6' tall masonry enclosure. The materials will include red face brick, gray limestone, and a white steel gate.

A 6' tall solid white vinyl plank fence is shown enclosing the areas south and west of the building.

The site plan notes a monument sign near the northeast corner of the site. Signage requires a separate permit from a building permit.

Locating the full access driveway off Gross Point Road is the City's preference given the site is in close proximity to the three intersections at Central Street/Gross Point Road, Central Street/Crawford Avenue, and Gross Point Road/Crawford Avenue which have limited vehicle stacking area between the intersections. The eastbound traffic on Central Street at Gross Point Road has been provided limited green time due to the low traffic volume and to maintain proper traffic flows on the other major legs of these intersections. A full access off Gross Point Road is also intended to avoid northbound vehicles on Gross Point Road from having to make a left turn at Central Street and thereby minimize vehicles stacking for the left turn; the driveway is located far enough away from the intersection to allow vehicles to maneuver around vehicles making a left turn into the parking lot.

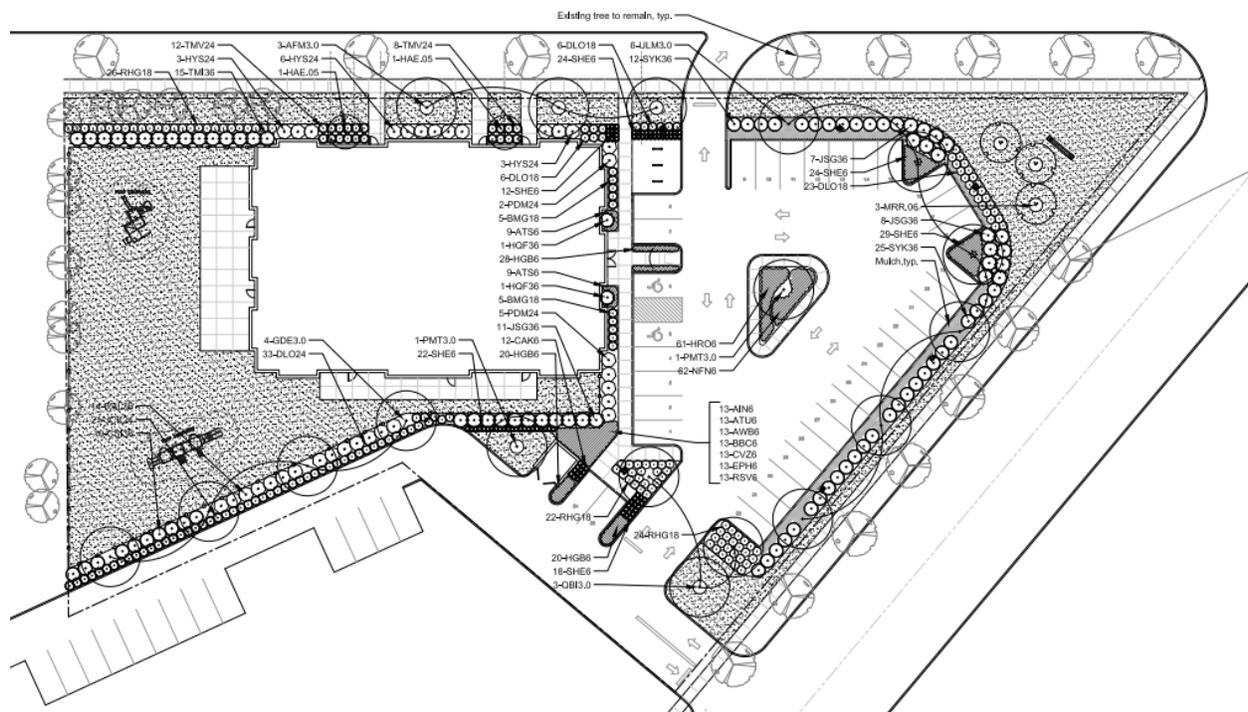
The proposed site plan maintains access to the First Williamsburg Co-Op residents via the proposed curb cuts, driveways, including a 16-foot wide driveway extending along the site's south property line to the Co-Op's parking spaces (driveway straddles the property line). The south driveway width maintains the existing width and stays within the current easement. However, since the subject property is being redeveloped, current code requires a 24-foot wide drive aisle for 2-way traffic, this is a noted site development allowance. The existing ingress/egress easement section off Central

should be aimed down toward the ground, dimmed evenings, and classroom lights turned off when not in use. Exterior lighting details can be reviewed by DAPR at the time of a building permit review.

The plan shows approximately 20 existing trees on the property, though several trees appear to straddle or may be over the property line along the west side of the site. Eight trees are shown to be removed, the remaining trees are clustered at the northwest and west sides of the site.

The landscape plan provides landscaping along the north, east, and south sides of the site, along the east side of the building and within the parking lot island, incorporating a mix of shade trees, ornamental trees, shrubs, and perennials. A 10-foot wide landscape strip is required along the west and south sides of the property. The plan provides landscaping along the south side of the site; however, the landscaped area is 6-foot wide where 10 feet is required. The plan does not delineate a landscape strip along the west side, however, there are existing trees and a 6-foot tall solid white vinyl plank fence is proposed.

Because the existing trees along the west property line straddle the property line and are to remain, access to these trees needs to be provided for tree maintenance by both property owners.



Staff encourages the applicant to work with Bird Friendly Evanston on bird friendly measures. Finalized bird friendly measures can be reviewed by DAPR at the time of a building permit review.

The plans do not show mechanical equipment; however, at the April 12, 2022, DAPR meeting, the applicant stated mechanical equipment will be located on the roof and will be hidden from view by the roof parapet.

Planned Development

The applicant is requesting Special Use approval for a Planned Development to construct the 22,416 square foot building on a zoning lot exceeding 30,000 square feet (the element triggering a Planned Development). The following table identifies how the project meets or does not meet bulk and other requirements in the R2 District:

3434 Central Street		
Zoning Requirements	Standard Min. / Max. Permitted	Proposed
Building lot coverage	40%	19%
Impervious surface coverage	55%	52%
Building setbacks:		
Front yard setback (southeast)	27 feet	27+ feet
Street side yard setback (north)	15 feet	15 feet
Interior side yard setback (south)	15 feet	15+ feet
Rear yard setback (west)	30 feet	30+ feet
Parking setbacks:		
Front setback (southeast)	Open parking prohibited	Open parking
Street side setback (north)	Open parking prohibited	No parking
Interior side setback (south)	Open parking	Open parking

	prohibited	
Rear yard setback (west)	5 feet	No parking
Landscape strip	10-foot wide along west and south property lines	No landscape strip along west property line 6-foot wide landscape strip along south property line
Detached accessory use location	Not to be located within a side yard abutting a street, or interior side yard between principal structure and side lot line, or located between the building line and principal structure	Located in interior side yard between principal structure and side lot line
Building height:	35 feet not to exceed 2.5 stories	28 feet to roof, 32 feet to parapet, 2 stories
Off-street parking, total Accessible spaces	34 2	34 2
Driveway aisle width	2-way traffic: 24 feet	2-way traffic: 16 feet

The following site development allowances are required for plan approval:

3434 Central Street Site Development Allowances	Regulation	Proposed
Front setback (southeast)	Open parking prohibited	Open parking
Interior side setback (south)	Open parking prohibited	Open parking
Landscape strip	10-foot wide along west and south property lines	No landscape strip along west property line

		6-foot wide landscape strip along south property line
Detached accessory use location	Not to be located within a side yard abutting a street, or interior side yard between principal structure and side lot line, or located between the building line and principal structure	Located in interior side yard between principal structure and side lot line
Driveway aisle width	2-way traffic: 24 feet	2-way traffic: 16 feet

Locating the parking within the front and south interior side yards was done to move vehicle maneuvering activities away from adjacent residences as much as is feasible, locating the parking within the rear yard locates it next to the single-family residence to the west. One alternative site plan reviewed by staff had the parking lot located on the west side of the site with the main driveway along the west property line, adjacent to the single-family residence, and the playground areas on the east side of the site near the street intersection.

While the required 10-foot wide transition landscape strip is not provided along the south and west property lines, the plan provides both landscaping and a 6-foot tall solid fence along the south property line. Along the west property line, a 6-foot tall solid fence is proposed along with existing trees to remain. As proposed, the plan provides screening intended by the Zoning Ordinance, however, the applicant should explain why the required 10-foot wide landscape strip cannot be provided along both the south and west property lines.

The refuse enclosure is logically located to move it away from adjacent residences as much as is feasible and still provide adequate access for refuse pick-up. The refuse containers will be screened by a masonry wall, metal gate, and landscaping.

The proposed 2-way 16-foot wide drive aisle adjacent to the Co-Op's parking spaces maintains the existing condition and stays within the existing easement limits. The applicant should explain why the drive aisle cannot be widened to 24-feet.

Exterior Building Materials

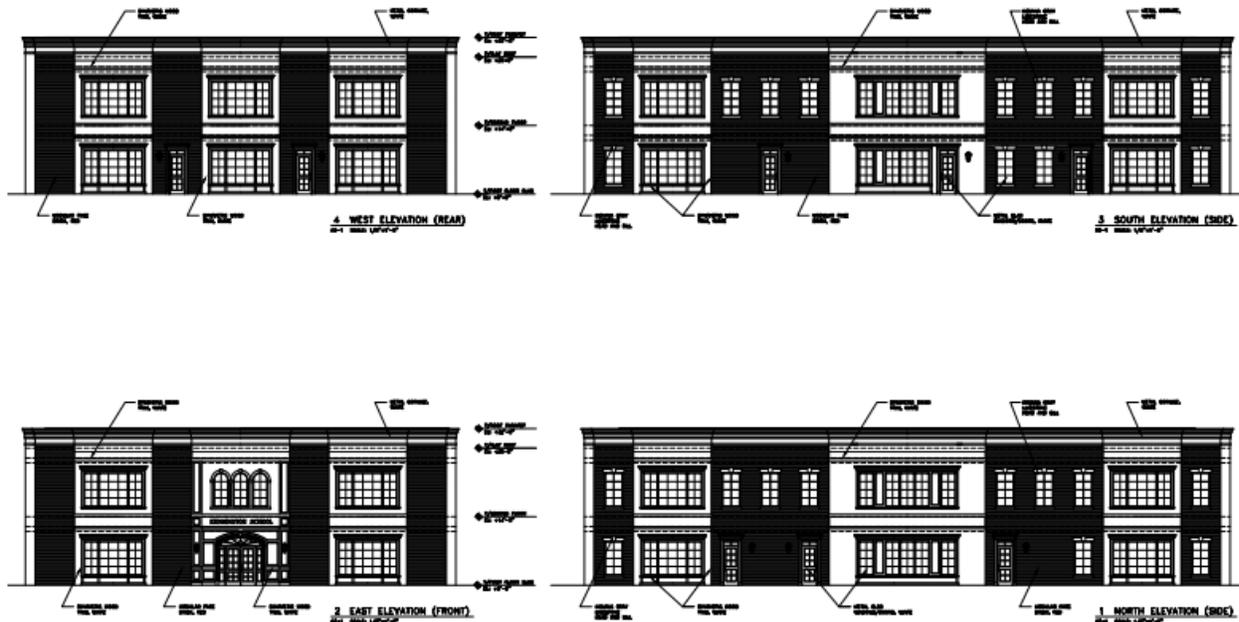
The proposed exterior building materials include:

- Modular face brick, red
- Synthetic wood trim, white
- Metal cornice, white

- Indiana limestone window head and sill, gray
- Metal clad windows and doors, white

The materials for the refuse area enclosure include:

- Modular face brick, red
- Limestone, gray
- Steel gate, white



3434 Central Street - elevations

Traffic and Circulation

The applicant submitted a Traffic Impact Study prepared by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA), dated February 10, 2022. The traffic study used traffic counts from 2022 taking into account the ongoing COVID-19 pandemic, and projected traffic volumes to year 2028 taking into account ambient area growth factors not attributable to any particular development as well as traffic volume generated by the proposed daycare center.

Central Street is an east-west road classified as a minor arterial road that generally has one lane and one bike lane in each direction. Central Street west of Gross Point Road is classified as a local road with one lane in each direction. On-street parking is generally permitted on both sides of Central Street east of Crawford Avenue but is restricted to the south side of the street west of Gross Point Road. Central Street is under the jurisdiction of the City of Evanston.

Gross Point Road is a northeast-to-southwest road classified as a minor arterial road that has two lanes in each direction southwest of Crawford Avenue and one lane and one bike lane in each direction northeast of Crawford Avenue. On-street parking is generally prohibited on both sides of the road. Gross Point Road is under the jurisdiction of IDOT.

Crawford Road is generally a north-south road classified as a minor arterial road that generally has two lanes in each direction divided by a median. On-street parking is generally permitted on both sides of the road northwest of Gross Point Road and prohibited on both sides of the road south of Gross Point Road. Crawford Avenue is under the jurisdiction of the City of Evanston.

Wellington Court is generally a north-south private local road with one lane in each direction. On-street parking is prohibited on both sides of the road.

Pace Bus Route 421 runs along Wilmette Avenue and has a local bus stop approximately 0.6 miles from the site. Pace Bus Routes 208 and 422 run along Golf Road and have a local bus stop approximately 0.7 miles from the site.

Public sidewalks are generally provided on both sides of roads within the vicinity of the site except there is not a sidewalk on the north side of Central Street west of Gross Point Road. Standard style crosswalks and pedestrian traffic signals are located at all three nearby signalized intersections in the Central Street/Gross Point Road/Crawford Avenue triangle.

Bike lanes are provided on both sides of Central Street east of Crawford Avenue and on Gross Point Road northeast of Crawford Avenue. Crawford Avenue northwest of Central Street is a designated bike route. A Divvy bike station accommodating 11 bikes is located at the intersection of Central Street and Lincolnwood Drive, approximately 0.7 miles from the site.

The traffic study looked at crash data over the past five years, 2016 to 2020, for the nearby intersections. The review indicates there were 10 crashes at the intersection of Gross Point Road and Wellington Court over the five year period but there were no fatal crashes at any of the intersections within the five year period.

The proposed daycare center will have approximately 165 students and 23 staff, operating between 6:30 a.m. and 6:30 p.m. Most students will be dropped by between 6:30 a.m. and 8:30 a.m. and picked up between 4:00 p.m. and 6:00 p.m. Caregivers will be required to park their vehicles and walk the students to and from the school, per the applicant's operation details provided.

The traffic study indicates the area intersections have sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed daycare center and

does not suggest roadway improvements and/or traffic control modifications are necessary.

To address traffic concerns, staff recommends the following:

- After operations begin, if traffic added by the daycare center becomes an issue on Central Street, the right-only turn onto Central Street will be restricted to outside peak traffic hours. If this is unsuccessful in resolving traffic issues, the owner will be required to engage an IDOT pre-qualified SCAT consultant to complete the analysis to retime the traffic signals.

Special Use - Daycare Center-Child

Special uses are those uses that, because of their potential adverse impact upon the immediate neighborhood and the City, as a whole, require a greater degree of scrutiny and review of site characteristics and impacts to determine their suitability in a given location. As such, the determination of special uses as appropriate shall be contingent upon their meeting a set of specific standards and the weighing, in each case, of the public need and benefit against the local impact, giving effect to the proposals of the applicant for ameliorating adverse impacts through special site planning and development techniques and contributions to the provisions of public improvements, sites, right of way and services, Zoning Code Section 6-3-5-1.

The proposed daycare center is a listed special use in the R2 district and is intended to serve the nearby and broader community. The proposed site plan locates the building away from the immediate residence to the west, locates parking away from adjacent residences when compared against other potential parking locations on the site, and the proposed curb cut locations and configuration maintains access to the Co-Op residences, aims to direct traffic onto adjacent roadways in a manner to minimize impacts to the surrounding neighborhood and provide safe circulation.

Compliance with the Zoning Ordinance

The R2 Single-Family Residential District is intended to provide for small lot single-family development at a relatively low density and to preserve the present physical character of that area while providing for infill development.

Both the current and proposed uses are non-residential and both are allowable special uses in the R2 district. The proposed development does not significantly change the physical character of the area.

Through the special use review process, the R2 district allows a variety of non-residential uses that serve and are compatible with residential uses, to identify potential impacts and to develop a plan to mitigate those impacts.

Compliance with the Comprehensive Plan

The proposed Planned Development is consistent with the 2000 Comprehensive General Plan. The guiding principle of the Plan is to encourage new development that improves the economy, convenience and attractiveness of Evanston while simultaneously working to maintain a high quality of life within the community where new developments should be integrated within existing neighborhoods to promote walking and the use of mass transit.

The site is within an identified corridor along Gross Point Road where opportunities for commercial and mixed-use redevelopment revitalization are probable, sensitive transit oriented development is encouraged.

The proposed development is consistent with the following Comprehensive Plan objectives:

- Preserve neighborhood character while supporting redevelopment efforts that add to neighborhood desirability. The neighborhood character will not change significantly since the proposed development characteristics are similar to existing site conditions. The proposed daycare center fills a need for child daycare in proximity to where people live.
- Minimize the adverse effects of traffic and parking congestion or incompatible hours of operation of businesses proposing expansion or relocation to sites adjacent to residential areas. The site plan is laid locating parking and curb cut locations/configuration to minimize impacts onto adjacent residents. Staff recommends that after operations begin, if traffic added by the daycare center becomes an issue on Central Street, the right-only turn onto Central Street will be restricted to outside peak traffic hours. If this is unsuccessful in resolving traffic issues, the owner will be required to engage an IDOT pre-qualified SCAT consultant to complete the analysis to retime the traffic signals.
- Seek creative means to increase Evanston's property tax base to maintain the provision of quality services while relieving some of the tax burden placed on homeowners. Currently, the property is tax exempt. The proposed development returns the parcel back onto the tax roll.

Compliance with the Design Guidelines for Planned Developments

The proposed building is consistent with the Design Guidelines for Planned Developments.

The proposed building is sited in a logical position on the property complying with required setbacks and to minimize impacts onto adjacent residences. The parking is located away from adjacent residences as much as is feasible, the playground area is located away from the adjacent roadways. The building massing is broken up by several vertical planes on all facades, window, fenestration, and other architectural details. The

scale of the 2-story building is compatible with the surrounding area of mostly 2-story structures and 3-story multi-family residential buildings to the east.

DAPR Committee Review

The Design and Project Review Committee reviewed the proposed Planned Development on April 12, 2022. The Committee recommended approval of the proposed development subject to the following conditions:

- After operations begin, if traffic added by the daycare center becomes an issue on Central Street, the right-only turn onto Central Street will be restricted to outside peak traffic hours. If this is unsuccessful in resolving traffic issues, the owner will be required to engage an IDOT pre-qualified SCAT consultant to complete the analysis to retime the traffic signals.
- Staff is not allowed to park in the neighborhood.
- Provide access to maintain trees and plantings along the west property line.
- Provide a drawing indicating in/out access for emergency vehicles. *Item has been addressed by the applicant.*

Standards

The proposed development must satisfy the standards for Special Use in Zoning Code Section 6-3-5-10, the Standard for Planned Development in Zoning Code Section 6-3-6-9 and standards and guidelines established for Planned Developments in the R2 District, Zoning Code Section 6-8-1-10.

Staff finds that the proposed development meets all standards for approval.

Standards for Special Use (Section 6-3-5-10)

- A Planned Development is listed as an allowed special use in the R2 District.
- A Daycare Center-Child is listed as an allowed special use in the R2 District.
- As noted above, the proposal is in keeping with the purposes and policies of the adopted Comprehensive Plan and the Zoning Ordinance.
- The proposed development will not cause a negative cumulative effect when considered in conjunction with other special uses on the immediate area and the City as a whole with the recommended traffic circulation configuration.
- The proposed development will not interfere with or diminish the value of property in the neighborhood.
- The proposed development will be adequately served by public infrastructure already available. The street and sidewalk network, as well as water, sewer, electricity and gas infrastructure already exist. The plan extends the public sidewalk west along Central Street to the west property line..
- The proposed development will not cause undue traffic congestion. The applicant has submitted a traffic study indicating there will be minimal effect on the level of service on existing surrounding roadways.

- There are no historical and architectural resources or environmental features present on the site.

Standards and Guidelines for Planned Developments in the R2 District (Sections 6-3-6-9 and 6-8-1-10)

Each planned development shall be compatible with surrounding development and not be of such a nature in height, bulk, or scale as to exercise any influence contrary to the purpose and intent of the Zoning Ordinance.

Each planned development shall be compatible with and implement the adopted land use or urban design plan specific to the area, the Zoning Ordinance, and any other pertinent City planning and development policies in terms of: land use, land use intensity, housing, preservation, environmental, traffic impact and parking, impact on schools, public services and facilities, essential character of the neighborhood, neighborhood planning, and conservation of the taxable value of land.

- The proposed daycare center includes preschool and kindergarten activities which are typical uses in residential areas,
- The proposed land use intensity is compatible with surrounding uses and the existing land use of the site,
- Traffic circulation has been configured to minimize impacts onto adjacent roads and the neighborhood, off-street parking is provided,
- The proposed development is not changing the essential character of the neighborhood.
- The parcel is being returned to the tax base.

Public Benefits

Public benefits are intended to address impacts development has on the community. The applicant provides the following to address how the proposed development provides public benefits per Zoning Code Section 6-3-6-3:

- Child care is a necessity for working families, made more clear during the COVID-19 pandemic, allowing families to manage work demands while raising children.
- Daycare center will support over 165 Evanston's working families.
- Public sidewalk extended along Central Street.
- Primary curb cut off Gross Point Road reduces traffic on Central Street west of Gross Point Road. The curb cut will be shared by the Co-Op residents to the south.

Recommendation

Based on the analysis above and the DAPR Committee recommendation, staff recommends the Land Use Commission make a positive recommendation of the

Special Use for the proposed Planned Development at 3434 Central Street to the City Council subject to the following conditions:

1. Provide an ingress and egress easement to the First Williamsburg Co-Op to provide access to their parking, note the easement on the plan, and record the easement with the Cook County Recorder of Deeds.
2. A permit from IDOT is required for any work within the Gross Point Road right-of-way.
3. Provide a revised photometric plan with light levels measured to the property line noting a maximum of 0 lumens at the property line, plan needs to be legible.
4. Provide access to maintain trees and plantings along the west property line.
5. After operations begin, if traffic added by the daycare center becomes an issue on Central Street, the right-only turn onto Central Street will be restricted to outside peak traffic hours. If this is unsuccessful in resolving traffic issues, the owner will be required to engage an IDOT pre-qualified SCAT consultant to complete the analysis to retime the traffic signals.
6. Staff is not allowed to park in the neighborhood.
7. Make reasonable efforts to hire local contractors to do work to construct the child care center.
8. Make reasonable efforts to hire Evanston residents to work at this child care center.

Attachments

3434 Central Street - Kensington School plan, revised March 28, 2022

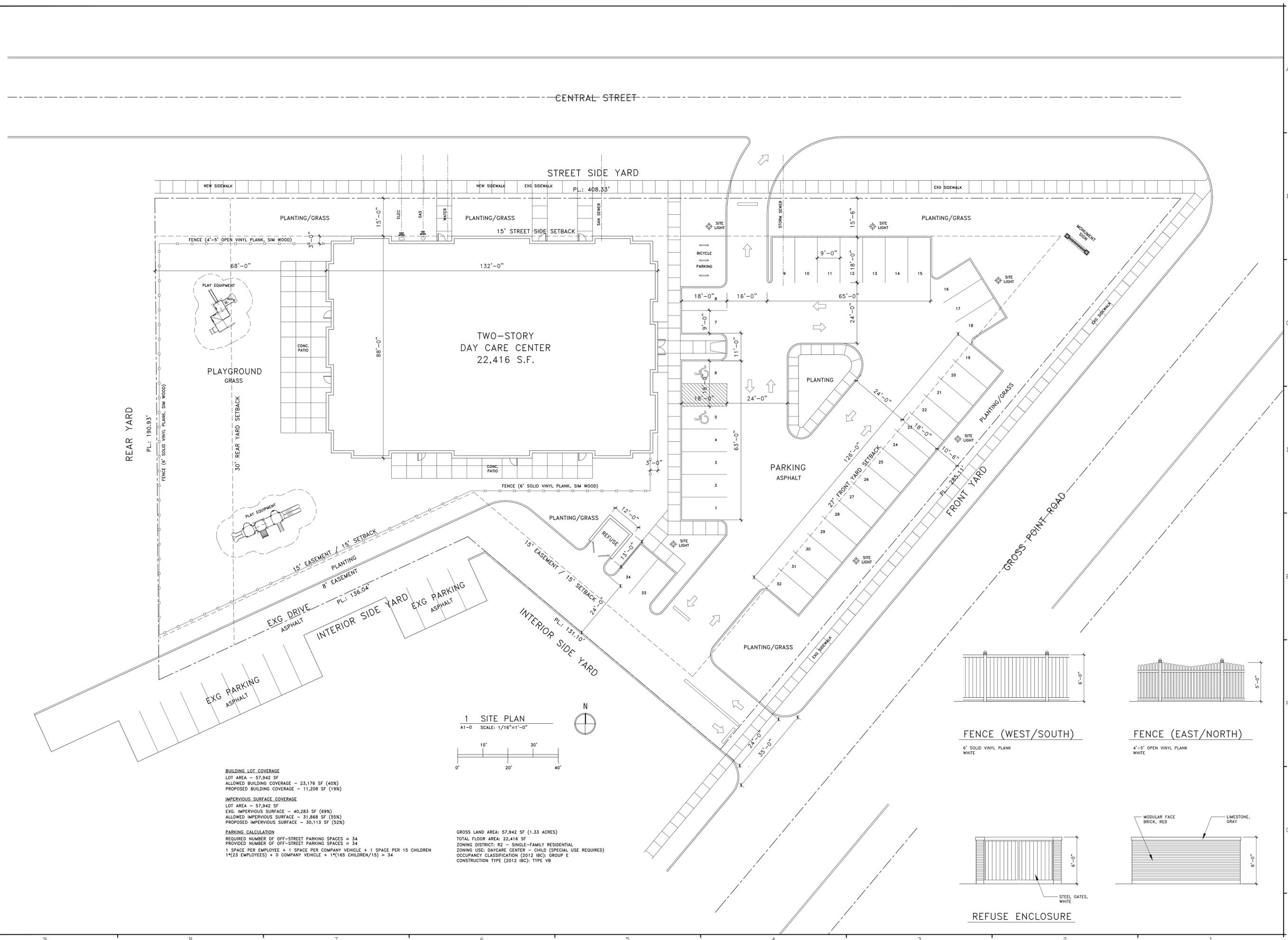
Market Analysis and Site Efficacy Study, dated February 8, 2022

Traffic Impact Study, dated February 10, 2022

Approved minutes from April 12, 2022, DAPR meeting

Public comments received

[Link to Planned Development Application](#)



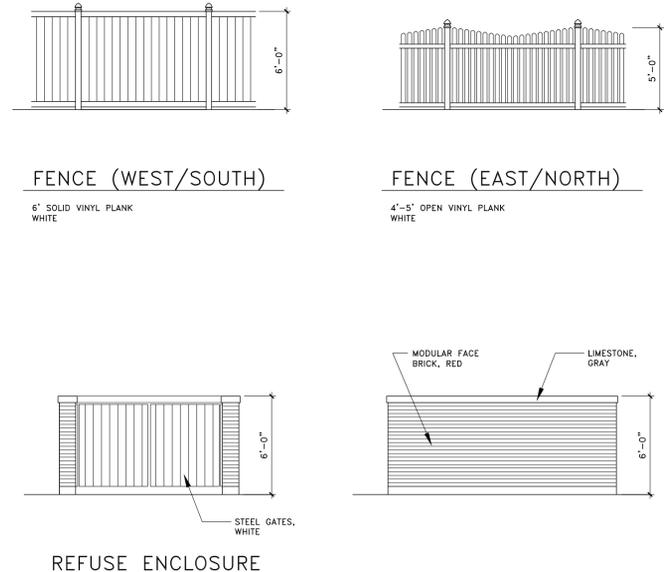
BUILDING LOT COVERAGE
 LOT AREA - 57,942 SF
 ALLOWED BUILDING COVERAGE - 23,176 SF (40%)
 PROPOSED BUILDING COVERAGE - 11,208 SF (19%)

IMPERVIOUS SURFACE COVERAGE
 LOT AREA - 57,942 SF
 EXG. IMPERVIOUS SURFACE - 40,283 SF (69%)
 ALLOWED IMPERVIOUS SURFACE - 31,868 SF (55%)
 PROPOSED IMPERVIOUS SURFACE - 30,113 SF (52%)

PARKING CALCULATION
 REQUIRED NUMBER OF OFF-STREET PARKING SPACES = 34
 PROVIDED NUMBER OF OFF-STREET PARKING SPACES = 34
 1 SPACE PER EMPLOYEE + 1 SPACE PER COMPANY VEHICLE + 1 SPACE PER 15 CHILDREN
 1*(23 EMPLOYEES) + 0 COMPANY VEHICLE + 1*(15 CHILDREN/15) = 34

1 SITE PLAN
 A1-0 SCALE: 1/16"=1'-0"

GROSS LAND AREA: 57,942 SF (1.33 ACRES)
 TOTAL FLOOR AREA: 22,416 SF
 ZONING DISTRICT: R2 - SINGLE-FAMILY RESIDENTIAL
 ZONING USE: DAYCARE CENTER - CHILD (SPECIAL USE REQUIRED)
 OCCUPANCY CLASSIFICATION (2012 IBC): GROUP E
 CONSTRUCTION TYPE (2012 IBC): TYPE VB



DATE	DESCRIPTION
03-28-2022	RESPONSE TO STAFF COMMENTS
02-04-2022	PLANNED DEVELOPMENT APPL
10-02-2021	FOR ZONING ANALYSIS REVIEW

DATE	DESCRIPTION
21-001	
10-1-21	

DRAWING NO. **A1-0**



22 E. Chicago Avenue
Suite 200A
Naperville, IL 60540
T 630.961.1787

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PREPARED FOR
Kensington School

3434 Central Street
Evanston, IL 60201

PROJECT

Kensington School of Evanston

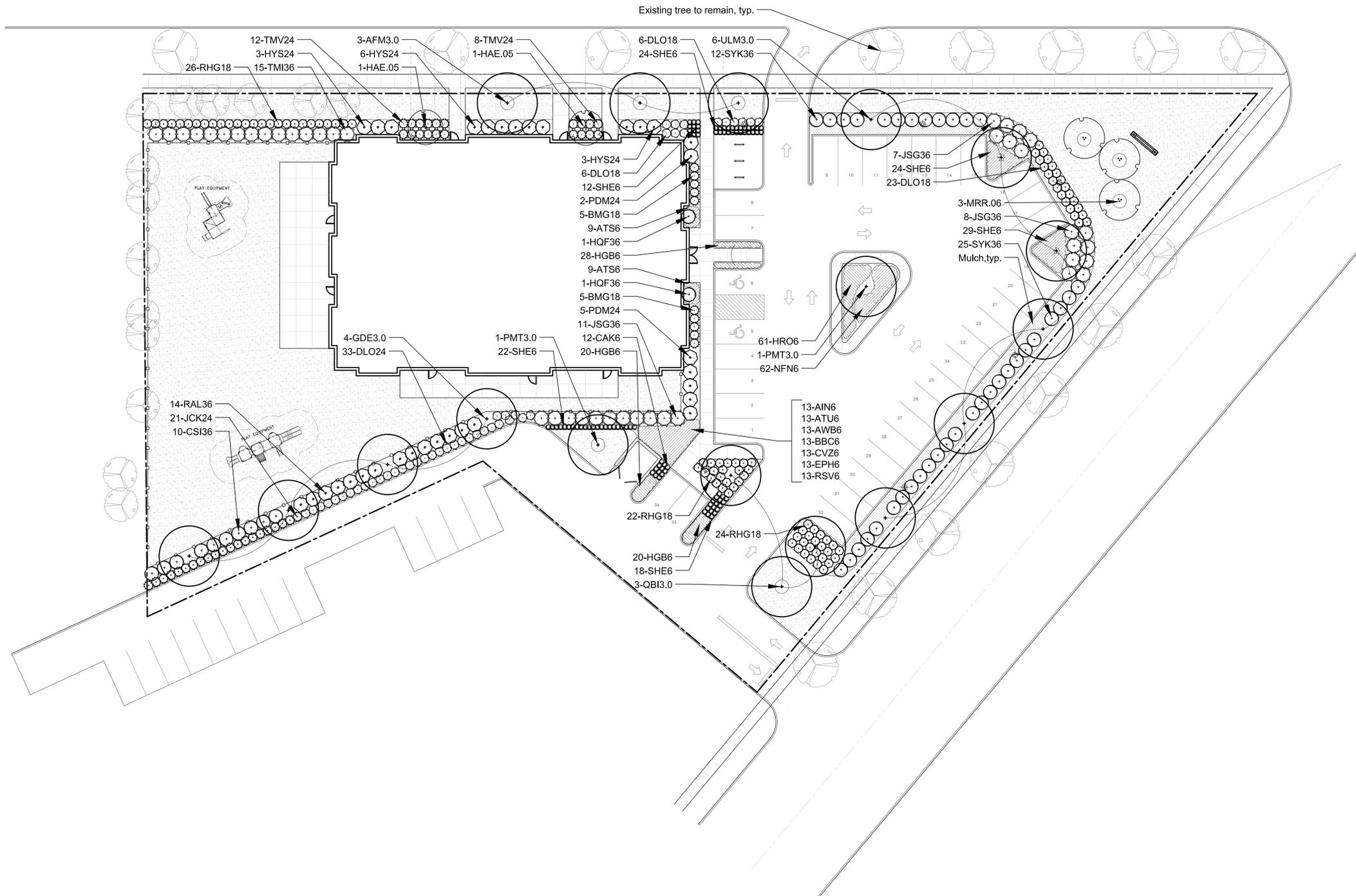
3434 Central Street
Evanston, IL 60201

PLANTING NOTES

- Sod limit line is approximate. Sod to limits of grading and disturbance. Contractor responsible for restoration of any unauthorized disruption outside of designated construction area.
- Contractor responsible for erosion control in all sodded areas.
- Tree mulch rings in turf areas are 5 foot diameter, typical. Contractor shall provide a mulch ring around all existing trees within the limit of work.
- Bedlines are to be spade cut to a minimum depth of 3 inches unless otherwise shown on the plans. Curved bedlines are to be smooth and not segmented.
- Do not locate plants within 10' of utility structures, or within 5' horizontally of underground utility lines unless otherwise shown on the plans.
- Plants and other materials are quantified and summarized for the convenience of the Owner and jurisdictional agencies only. Confirm and install sufficient quantities to complete the work as drawn and specified.
- Refer to specifications for additional conditions, standards and notes.

PLANTING LEGEND

-  Shade Tree, typical. See detail for installation.
-  Ornamental Tree, typical. See detail for installation.
-  Shrub, typical. See detail for installation.
-  Perennial, Groundcover and Annual, typical. See detail for installation.
-  Sod



ISSUED
February 4, 2022
REVISIONS

No	Date	Issue
1	3/28/2022	City Comments

CHECKED BY _____ DRAWN BY _____

SHEET TITLE
Planting Plan

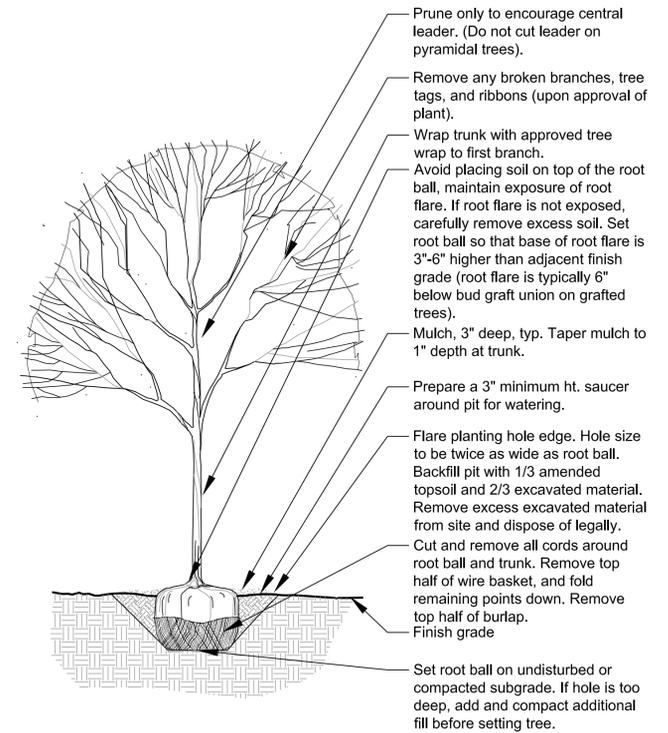
SCALE IN FEET
1" = 20'
0' 10' 20' 60'

NORTH SHEET NUMBER
L1.0

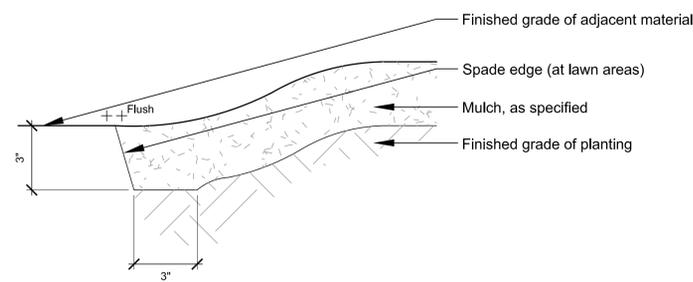
©2022 Hitchcock Design Group

Plant Material List

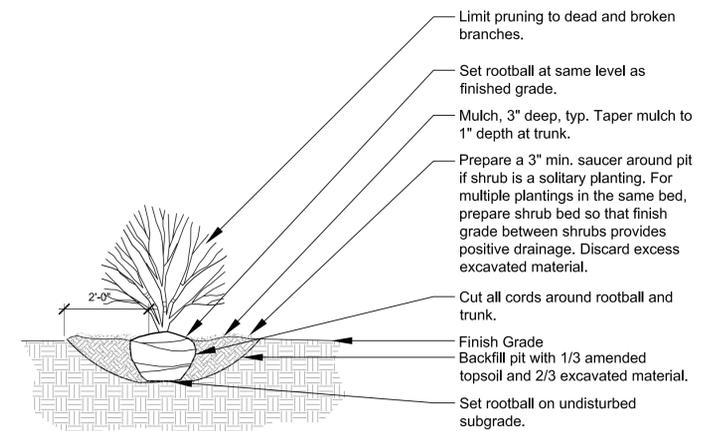
Code	Botanical Name	Common Name	Size	Qty
Shade Trees				
AFM3.0	Acer x freemanii 'Marmo'	Marmo Freeman Maple	3" C	3
GDE3.0	Gymnocladus dioicus 'Espresso-JFS'	Espresso Kentucky Coffeetree	3" C	4
PMT3.0	Platanus x acerifolia 'Morton Circle'	Exclamation! London Planetree	3" C	2
QBI3.0	Quercus bicolor	Swamp White Oak	3" C	3
ULM3.0	Ulmus japonica x wilsoniana 'Morton'	Accolade™ Elm	3" C	6
Intermediate Trees				
HAE.05	Hamamelis vernalis 'Autumn Embers'	Autumn Embers Witchhazel	5' HT	2
MRR.06	Malus 'Royal Raindrops'	Royal Raindrops Crabapple	6' HT	3
Deciduous Shrubs				
CSI36	Cornus sericea 'Isanti'	Dwarf Redtwig Dogwood	36" HT	10
DLO18	Diervilla lonicera	Dwarf Bush-honeysuckle	18" HT	68
HQF36	Hydrangea paniculata 'Quick fire'	Quick Fire Hydrangea	36" HT	2
HYS24	Hydrangea paniculata 'Vanilla Strawberry'	Vanilla Strawberry Panicked Hydrangea	24" HT	12
PDM24	Physocarpus opulifolius 'Donna May'	Little Devil Eastern Ninebark	24" HT	7
RHG18	Rhus aromatica 'Gro-Low'	Gro-Low Fragrant Sumac	18" HT	72
RAL36	Ribes alpinum	Alpine Currant	36" HT	14
SYK36	Syringa patula 'Miss Kim'	Miss Kim Manchurian Lilac	36" HT	37
Evergreen Shrubs				
JCK24	Juniperus chinensis 'Kallay Compact'	Kallay Compact Chinese Juniper	24" HT	21
JSG36	Juniperus chinensis 'Sea Green'	Sea Green Chinese Juniper	36" HT	26
TMV24	Taxus x media 'Everlow'	Everlow Anglojap Yew	24" HT	20
TM36	Taxus x media 'Hicksii'	Hicks Anglojap Yew	36" HT	15
Broadleaf Evergreens				
BMG18	Buxus x 'Glencoe'	Chicagoland Green Boxwood	18" HT	10
Perennials				
ATS6	Allium tanguticum 'Summer Beauty'	Summer Beauty Ornamental Chive	1 GAL	18
AIN6	Asclepias incarnata	Swamp Milkweed	1 GAL	13
ATU6	Asclepias tuberosa	Butterfly Milkweed	1 GAL	13
AWB6	Aster 'Woods Blue'	Woods Blue Aster	1 GAL	13
BBC6	Buddleia 'Blue Chip'	Lo & Behold Dwarf Butterfly Bush	1 GAL	13
CAK6	Calamagrostis x acutiflora 'Karl Foerster'	Karl Foerster Feather Reed Grass	1 GAL	12
CVZ6	Coreopsis verticillata 'Zagreb'	Zagreb Threadleaf Coreopsis	1 GAL	13
EPH6	Echinacea purpurea 'Kim's Knee High'	Kim's Knee High Purple Coneflower	1 GAL	13
HGB6	Hemerocallis 'Going Bananas'	Going Bananas Daylily	1 GAL	68
HRO6	Hemerocallis 'Rosy Returns'	Rosy Returns Daylily	1 GAL	61
NFN6	Nepeta x faassenii 'Novanepjun'	Junior Walker Catmint	1 GAL	62
RSV6	Rudbeckia fulgida var. speciosa 'Viette's Little Suzy'	Viette's Little Suzy Black-eyed Susan	1 GAL	13
SHE6	Sporobolus heterolepis	Prairie Dropseed	1 GAL	129



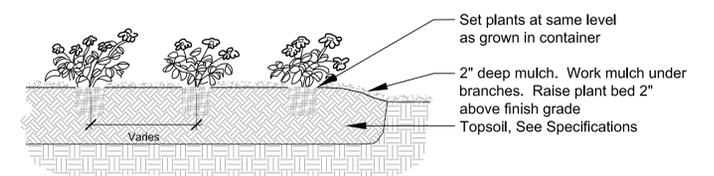
1 Deciduous Tree Planting
Scale: 1/2" = 1'-0"



4 Planting Bed Edge
Scale: 3" = 1'-0"



2 Shrub Planting
Scale: 1/2" = 1'-0"



3 Perennial and Groundcover Planting
Scale: 1/2" = 1'-0"



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PREPARED FOR
Kensington School

3434 Central Street
Evanston, IL 60201

PROJECT

Kensington School of Evanston

3434 Central Street
Evanston, IL 60201

ISSUED

February 4, 2022

REVISIONS

No	Date	Issue
1	3/28/2022	City Comments

CHECKED BY _____ DRAWN BY _____

SHEET TITLE

Planting Details

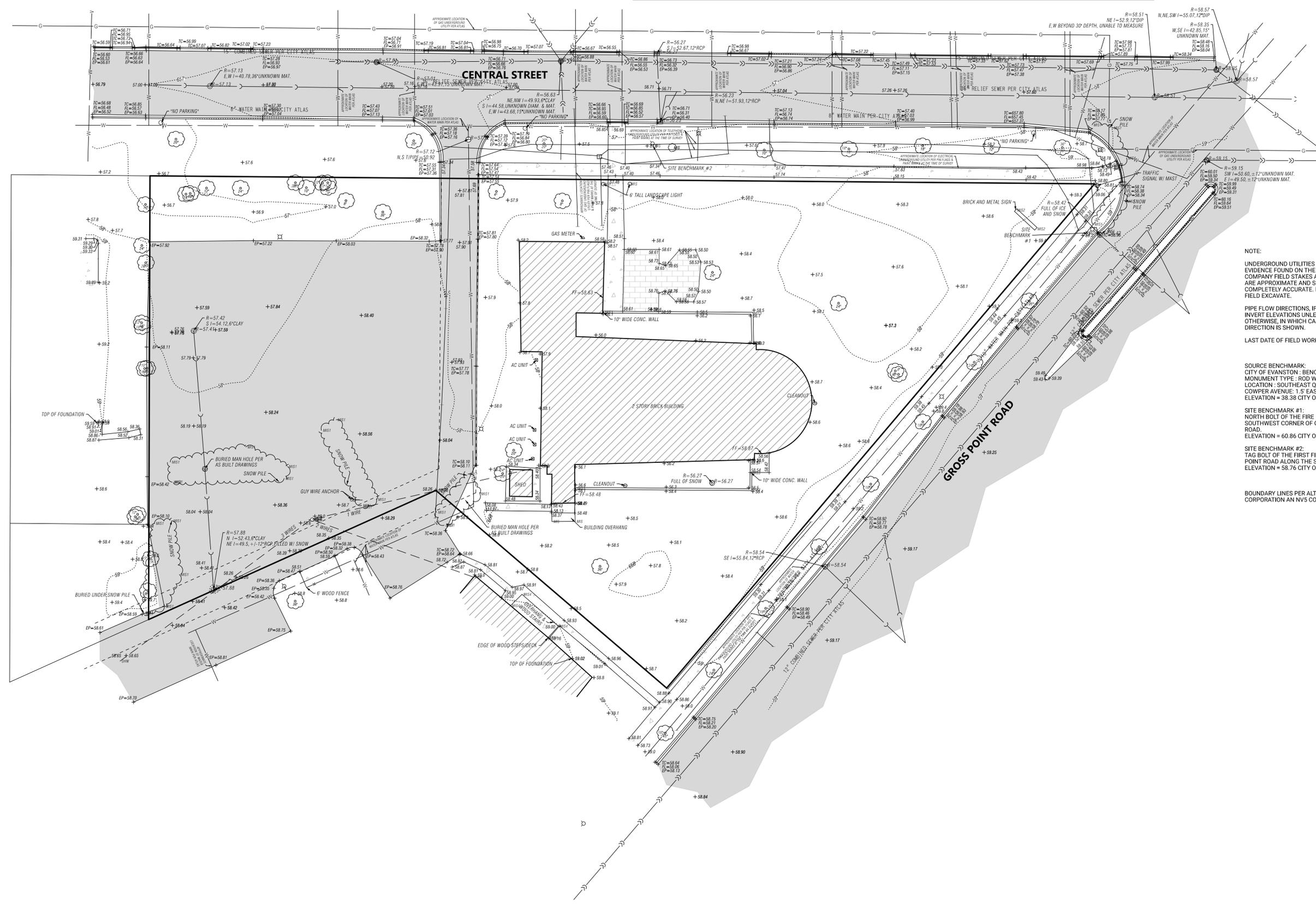
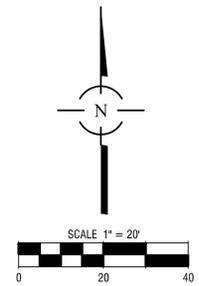
SCALE IN FEET
as noted

NORTH SHEET NUMBER



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LEGEND									
	STORM SEWER		AIR CONDITIONING UNIT		GAS MANHOLE		MANHOLE		TREE WITH SIZE
	SANITARY SEWER		HAND HOLE		SANITARY MANHOLE		FLAG POLE		TREE WITH SIZE
	COMBINED SEWER		TRAFFIC SIGNAL BOX		GREASE TRAP		SATELLITE DISH		TREE WITH SIZE
	WATER MAIN		ELECTRIC METER		CLEANOUT		UNIDENTIFIED PEDESTAL		TREE WITH SIZE
	GAS MAIN		LIGHT POLE		DOWNSPOUT		UNIDENTIFIED MANHOLE		TREE WITH SIZE
	UNDERGROUND TELEPHONE LINE		GUY WIRE ANCHOR		STORM MANHOLE		STONE		TREE WITH SIZE
	UNDERGROUND ELECTRIC LINE		LANDSCAPE LIGHT/FLOOD LIGHT		UTILITY POLE		FLASHED END SECTION		TREE WITH SIZE
	OVERHEAD ELECTRIC LINE		ELECTRIC PEDESTAL		MANHOLE		MEET		TREE WITH SIZE
	CATV		TELEPHONE PEDESTAL		MANHOLE		AUXILIARY VALVE		TREE WITH SIZE
	OVERHEAD WATER/UTILITY POLES		TELEPHONE MANHOLE		MANHOLE		BURIED MANHOLE		TREE WITH SIZE
	FIBER OPTIC LINE		CABLE PEDESTAL		MANHOLE		FIRE HYDRANT		TREE WITH SIZE
	RAILROAD		GAS METER		MANHOLE		WATER METER		TREE WITH SIZE
	FENCE		GAS VALVE		MANHOLE		WELL		TREE WITH SIZE
	GUARDRAIL		VACU AND VALVE		MANHOLE		TEST PIT		TREE WITH SIZE
	EDGE OF WATER				MANHOLE		MANHOLE		TREE WITH SIZE
	WETLAND LIMITS				MANHOLE		MANHOLE		TREE WITH SIZE



NOTE:
UNDERGROUND UTILITIES ARE SHOWN BY USING PHYSICAL EVIDENCE FOUND ON THE SURFACE AND/OR FROM UTILITY COMPANY FIELD STAKES AND, THEREFORE, THEIR LOCATIONS ARE APPROXIMATE AND SUSPECTED AND MAY NOT BE COMPLETELY ACCURATE. FOR MORE ACCURATE LOCATION, FIELD EXCAVATE.

PIPE FLOW DIRECTIONS, IF SHOWN, ARE BASED ON FIELD INVERT ELEVATIONS UNLESS EXISTING PLANS INDICATE OTHERWISE. IN WHICH CASE THE EXISTING PLAN FLOW DIRECTION IS SHOWN.

LAST DATE OF FIELD WORK: FEBRUARY 12, 2022

SOURCE BENCHMARK:
CITY OF EVANSTON: BENCHMARK #35
MONUMENT TYPE: ROD WITH CAP
LOCATION: SOUTHEAST QUADRANT; CENTRAL STREET & COPPER AVENUE: 1 S EAST & 1 S SOUTH OF SIDE ELEVATION = 58.38 CITY OF EVANSTON DATUM

SITE BENCHMARK #1:
NORTH BOLT OF THE FIRE HYDRANT LOCATED AT THE SOUTHWEST CORNER OF CENTRAL STREET AND GROSS POINT ROAD.
ELEVATION = 60.86 CITY OF EVANSTON DATUM

SITE BENCHMARK #2:
TAG BOLT OF THE FIRST FIRE HYDRANT WEST OF THE GROSS POINT ROAD ALONG THE SOUTH SIDE OF CENTRAL STREET.
ELEVATION = 58.76 CITY OF EVANSTON DATUM

BOUNDARY LINES PER ALTA SURVEY BY BOCK & CLARK CORPORATION AN NVS COMPANY, DATED OCTOBER 11, 2021.

NO.	DATE	REMARKS

NO.	DATE	RESPONSE TO STAFF COMMENTS	REMARKS
1	03/28/22		

PRELIMINARY EXISTING CONDITIONS
KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS

CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

9575 W. Higgins Road, Suite 700,
Rosemont, Illinois 60018
Phone: (847) 696-4060 Fax: (847) 696-4065

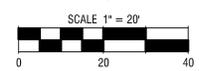
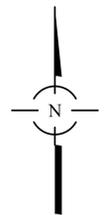
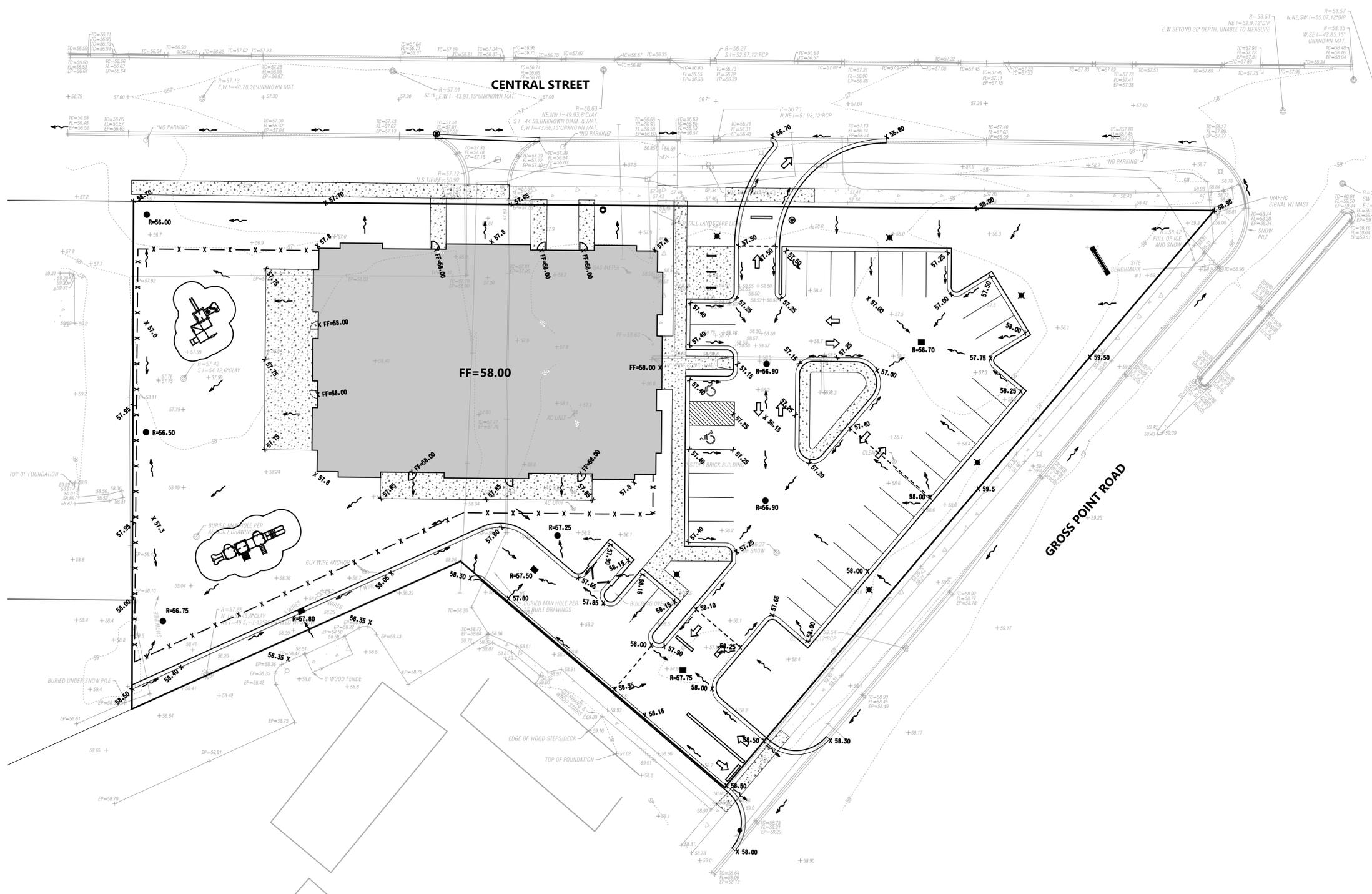


FILENAME:
11641P-EC

DATE:
02/04/22

JOB NO.
11641

SHEET
P-EC
2 OF 5



- LEGEND**
- F/F FINISHED FLOOR
 - LOCAL DRAINAGE
 - OVERLAND FLOW ROUTE
 - LOCAL DRAINAGE DIVIDE

- NOTES:**
1. ALL CURB AND GUTTER IS REVERSE PITCH UNLESS NOTED OTHERWISE.
 2. ALL SPOT GRADES ALONG CURB LINE ARE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.

NO.	DATE	REMARKS

NO.	DATE	REMARKS
1	03/28/22	RESPONSE TO STAFF COMMENTS

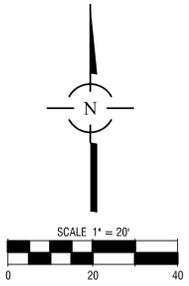
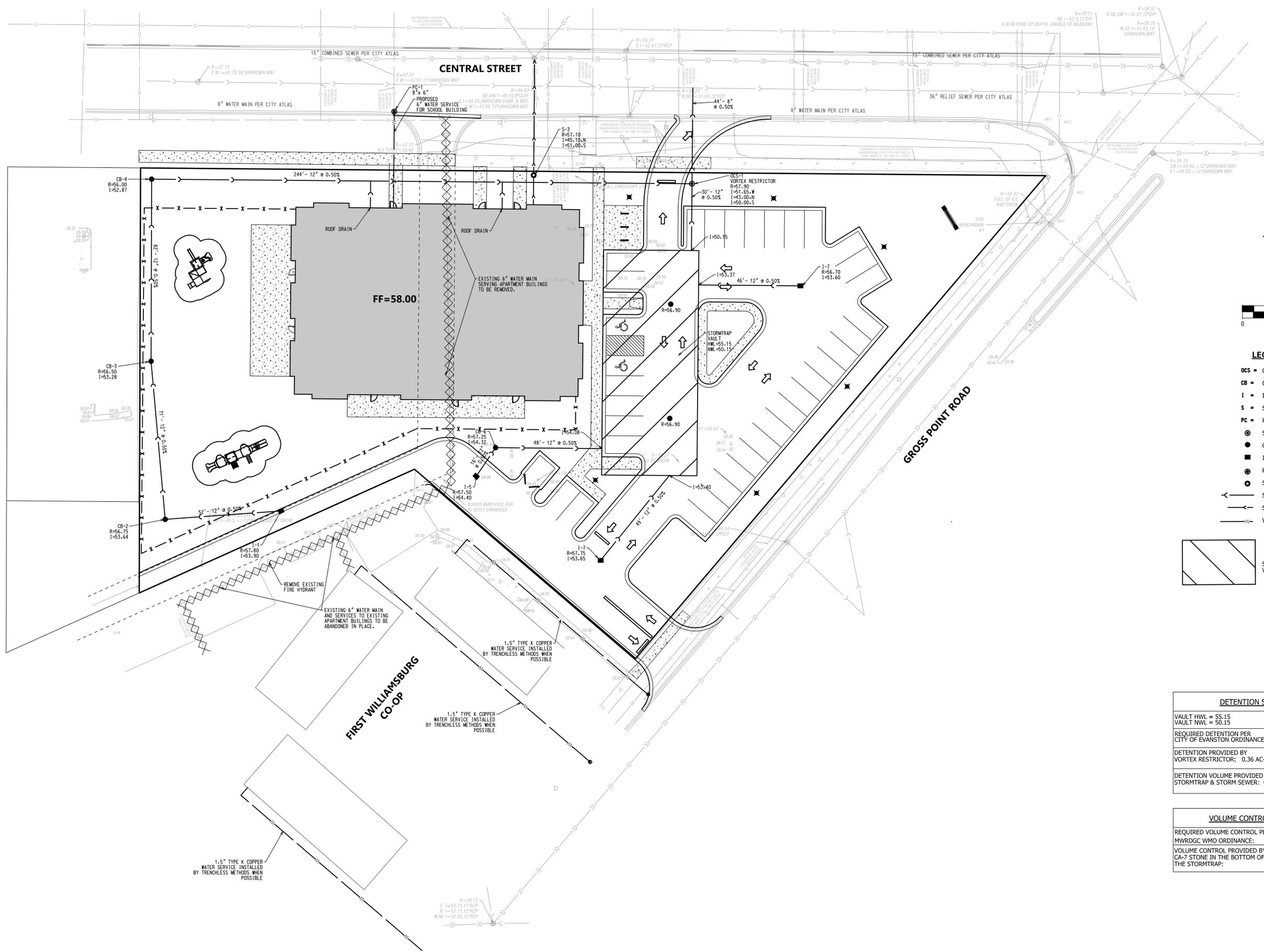
PRELIMINARY GRADING PLAN
KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS

CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

9575 W. Higgins Road, Suite 700,
 Rosemont, Illinois 60018
 Phone: (847) 696-0600 Fax: (847) 696-4065



FILENAME: 11641P-GR
DATE: 02/04/22
JOB NO. 11641
SHEET P-GR 4 OF 5



- LEGEND**
- OC = OUTLET CONTROL STRUCTURE
 - CB = CATCH BASIN
 - I = INLET
 - S = SANITARY MANHOLE
 - PC = PRESSURE CONNECTION
 - SM = STORM MANHOLE
 - CDW = CATCH BASIN/DRY WELL
 - IN = INLET
 - PC = PRESSURE CONNECTION
 - SM = SANITARY MANHOLE
 - SS = STORM SEWER
 - SW = SANITARY SEWER
 - WM = WATER MAIN
- STORMTRAP DETENTION VAULT WITH CA-7 STONE BOTTOM

DETENTION SUMMARY	
VAULT HWL =	55.15
VAULT NWL =	50.15
REQUIRED DETENTION PER CITY OF EVANSTON ORDINANCE:	0.36 AC-FT
DETENTION PROVIDED BY VORTEX RESTRICTOR:	0.36 AC-FT
DETENTION VOLUME PROVIDED BY STORMTRAP & STORM SEWER:	0.38 AC-FT

VOLUME CONTROL SUMMARY	
REQUIRED VOLUME CONTROL PER MWRDGC WMO ORDINANCE:	0.06 AC-FT
VOLUME CONTROL PROVIDED BY CA-7 STONE IN THE BOTTOM OF THE STORMTRAP:	0.07 AC-FT

NO.	DATE	REMARKS

NO.	DATE	RESPONSE TO STAFF COMMENTS	REMARKS
1	03/28/22		

PRELIMINARY UTILITY PLAN
KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS

CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

9575 W. Higgins Road, Suite 700,
 Rosemont, Illinois 60018
 Phone: (847) 696-4060 Fax: (847) 696-4065



FILENAME:
11641 P-UT

DATE:
02/04/22

JOB NO.
11641

SHEET
P-UT
5 OF 5



Kensington School of Evanston
Market Analysis & Site Efficacy Study
February 8, 2022

**Kensington School
Administration Office**
743 McClintock Drive
Burr Ridge, IL

Kensington School
LaGrange, IL

**Kensington School
Of Hinsdale**
Hinsdale, IL

**Kensington School
Lincoln Park**
Chicago, IL

**Kensington School
of Glenview**
Glenview, IL

**Kensington School
of the Highlands**
LaGrange, IL

**Kensington School
of Western Springs**
Western Springs, IL

**Kensington School
of St. Charles**
St. Charles, IL

**Kensington School
of Geneva**
Geneva, IL

**Kensington School
of Naperville**
Naperville, IL

**Kensington School
of Wheaton**
Wheaton, IL

**Kensington School
of South Naperville**
Naperville, IL

**Kensington School
of Elmhurst**
Elmhurst, IL

**Kensington School
Arlington Heights**
Arlington Heights, IL

**Kensington School
Of Evanston**
Evanston, IL

Kensington School proposes to develop a new child care center and preschool at 3434 Central Street. As Kensington School has been providing early childhood education to the Chicago area for over 53 years, it has a strong understanding of what demographic, real estate and market landscape conditions are prerequisites for project realization.

Evanston is a city in Cook County, Illinois. It is approximately 12 miles north of the City of Chicago with an approximate population of 75,000. For the purposes of this analysis, one, two- and three-mile radii will be targeted as the optimal trade area for Kensington School although in various communities which it operates, some students come from as far as five miles.

As shown on the enclosed table, one, two and three-mile 2020 population of 0-4 years of age are 857, 3403 and 7,270 respectively which meet or exceed comparable metrics at Kensington School's current locations. Income average of \$172,279 at one mile and \$136,524 at three miles also meet or exceed existing locations.

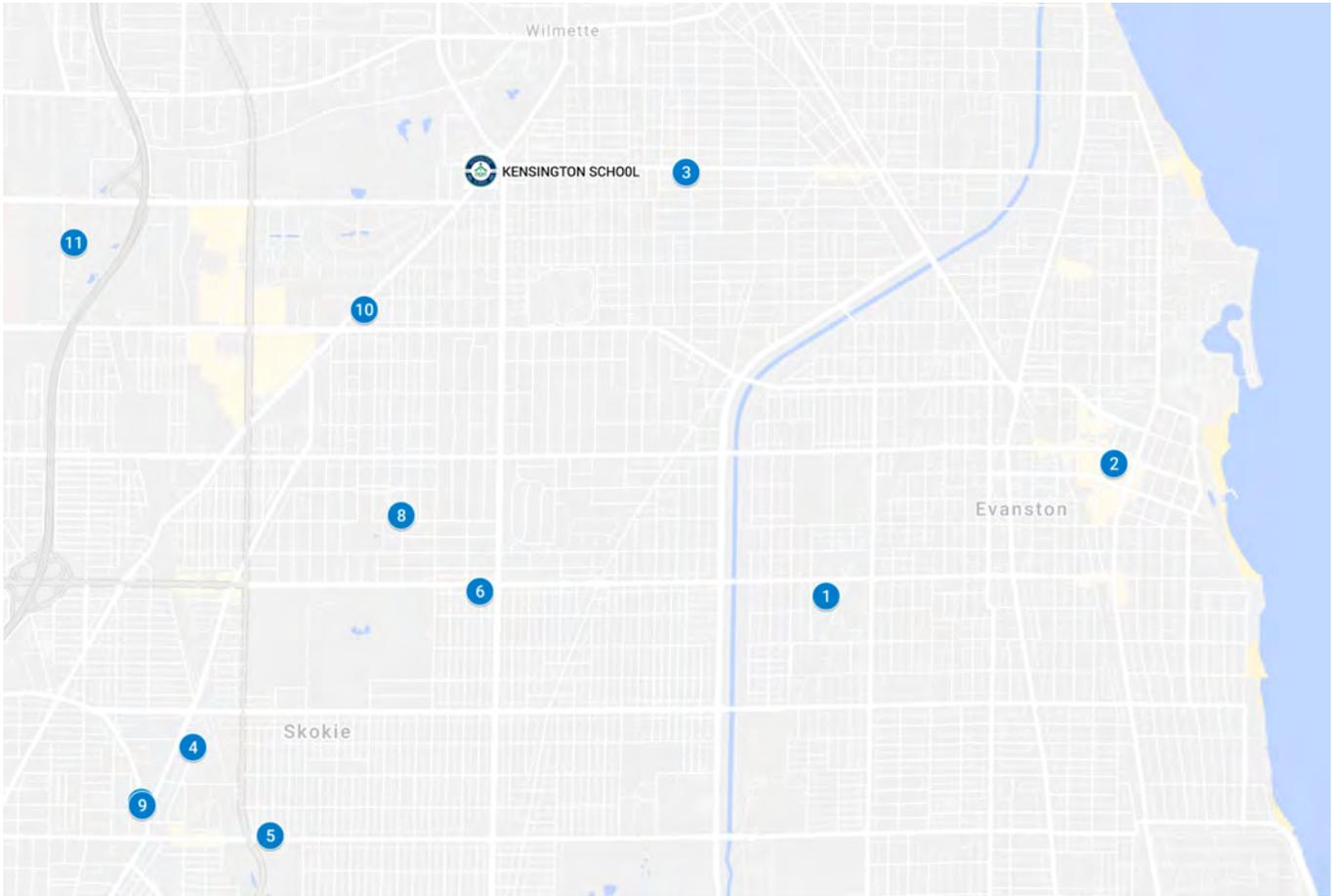
The site selected is located at 3434 Central Street. Currently, the building operating as a Unity Church, resides on the property neighboring existing R1 single family homes. It is placed on a rather well trafficked corner at Gross Point Rd. and Central Street.

The national market landscape for Child Care is a highly fragmented one with many providers having solely one location, many of which operate out of homes and churches. For the purposes of this study, Child Care Centers operating out of entities that are for profit and therefore contribute to local tax rolls are included. Home, church and non-for-profit Centers have been omitted.

As shown in the Market Study Map, there are currently only four for-profit Child Care Centers operating in Evanston. All are within the three-mile trade area of Kensington Schools proposed location. Also, within this three-mile area are additional child care centers in Skokie which are also included in this study. The total number of day spaces combined in this market area is only 1323 children. Ages served range from six weeks to as old as 12 years old for some centers that offer before and after school programs to elementary school aged children. Kensington School offers programming to only infants through five years old. With 7,270 children under four years old in this three-mile area, only 1323 available day spaces available and almost all of these spaces utilized, there is clearly a substantial need for quality center-based child care in Evanston. Approximately 81% of children in this area do not have a child care option available should they need it.

It's still too early to tell what impact the pandemic had on the child care market in Evanston, what has become clear is how vital child care is to working families whether they work from home or are working in person. While many child care center locations across the state have struggled for one reason or another, all twelve Kensington School locations are enrolled to near licensed capacity. This superlative performance in these neighboring markets is a strong indication as to how resilient its program is and how well received Kensington School will be in Evanston.

EVANSTON AREA CHILD CARE CENTERS
3-MILE RADIUS OF KENSINGTON SCHOOL OF EVANSTON



School Name	Street	City	Zip	Age Range	Capacity
1 KINDERCARE LEARNING CENTER	2014 DEMPSTER ST	EVANSTON	60202	6W TO 12Y	130
2 BRIGHT HORIZONS AT EVANSTON	1629 ORRINGTON AVE	EVANSTON	60201	6W TO 12Y	307
3 LITTLE GREEN TREE HOUSE	2814 CENTRAL ST	EVANSTON	60201	6W TO 6Y	151
4 SWIFT CHILD CARE	8260 ELMWOOD AVE	SKOKIE	60077	6W TO 12Y	147
5 MOSAIC MONTESSORI	4700 OAKTON ST	SKOKIE	60076	6W TO 6Y	46
6 MOSAIC EARLY CHILDHOOD INFANT CENTER	4035 DEMPSTER ST	SKOKIE	60076	6W TO 24M	12
7 SWIFT CHILD CARE	8106 LINCOLN AVE	SKOKIE	60077	6W TO 35M	26
8 CRADLES CRIBS & CRAYONS	9000 KILDARE AVENUE	SKOKIE	60076	6W TO 6Y	75
9 SWIFT CHILD CARE	8064 LINCOLN AVE	SKOKIE	60077	6W TO 6Y	79
10 THE GODDARD SCHOOL	9651 GROSS POINT RD	SKOKIE	60076	6W TO 6Y	188
11 BRIGHT HORIZONS AT SKOKIE	9877 WOODS DR	SKOKIE	60077	6W TO 6Y	162

Total Capacity in 3 Miles 1323

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Building Type: **General Retail**
 Secondary: -
 GLA: **60,000 SF**
 Year Built: **1950**

Total Available: **0 SF**
 % Leased: **100%**
 Rent/SF/Yr: -



Radius	1 Mile		2 Mile		3 Mile	
Population						
2026 Projection	17,661		70,580		146,801	
2021 Estimate	17,978		71,787		149,107	
2010 Census	18,655		74,171		153,016	
Growth 2021 - 2026	-1.76%		-1.68%		-1.55%	
Growth 2010 - 2021	-3.63%		-3.21%		-2.55%	
2021 Population by Age						
	17,978		71,787		149,107	
Age 0 - 4	743	4.13%	3,194	4.45%	7,623	5.11%
Age 5 - 9	940	5.23%	3,775	5.26%	7,821	5.25%
Age 10 - 14	1,185	6.59%	4,687	6.53%	8,802	5.90%
Age 15 - 19	1,227	6.83%	4,990	6.95%	9,623	6.45%
Age 20 - 24	1,097	6.10%	4,686	6.53%	10,576	7.09%
Age 25 - 29	1,030	5.73%	4,682	6.52%	12,409	8.32%
Age 30 - 34	737	4.10%	3,511	4.89%	9,650	6.47%
Age 35 - 39	712	3.96%	3,177	4.43%	7,819	5.24%
Age 40 - 44	856	4.76%	3,497	4.87%	7,424	4.98%
Age 45 - 49	1,103	6.14%	4,360	6.07%	8,534	5.72%
Age 50 - 54	1,247	6.94%	4,922	6.86%	9,399	6.30%
Age 55 - 59	1,432	7.97%	5,583	7.78%	10,602	7.11%
Age 60 - 64	1,468	8.17%	5,607	7.81%	10,611	7.12%
Age 65 - 69	1,245	6.93%	4,682	6.52%	8,837	5.93%
Age 70 - 74	960	5.34%	3,626	5.05%	6,848	4.59%
Age 75 - 79	656	3.65%	2,526	3.52%	4,766	3.20%
Age 80 - 84	482	2.68%	1,820	2.54%	3,406	2.28%
Age 85+	859	4.78%	2,460	3.43%	4,358	2.92%
Age 65+	4,202	23.37%	15,114	21.05%	28,215	18.92%
Median Age	47.10		44.60		40.20	
Average Age	43.80		42.40		40.80	

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile	2 Mile	3 Mile
2021 Population By Race	17,978	71,787	149,107
White	14,402 80.11%	52,475 73.10%	105,377 70.67%
Black	394 2.19%	4,991 6.95%	10,451 7.01%
Am. Indian & Alaskan	27 0.15%	196 0.27%	448 0.30%
Asian	2,725 15.16%	12,121 16.88%	28,465 19.09%
Hawaiian & Pacific Island	5 0.03%	27 0.04%	46 0.03%
Other	425 2.36%	1,976 2.75%	4,320 2.90%
Population by Hispanic Origin	17,978	71,787	149,107
Non-Hispanic Origin	17,060 94.89%	65,965 91.89%	135,817 91.09%
Hispanic Origin	918 5.11%	5,821 8.11%	13,290 8.91%
2021 Median Age, Male	44.60	41.70	37.80
2021 Average Age, Male	42.00	40.80	39.40
2021 Median Age, Female	49.20	46.70	42.50
2021 Average Age, Female	45.50	43.80	42.10
2021 Population by Occupation Classification	14,861	59,132	122,938
Civilian Employed	8,568 57.65%	33,905 57.34%	68,280 55.54%
Civilian Unemployed	448 3.01%	2,242 3.79%	4,493 3.65%
Civilian Non-Labor Force	5,845 39.33%	22,985 38.87%	50,147 40.79%
Armed Forces	0 0.00%	0 0.00%	18 0.01%
Households by Marital Status			
Married	4,071	15,627	29,623
Married No Children	2,110	8,196	15,920
Married w/Children	1,961	7,431	13,703
2021 Population by Education	13,292	52,599	109,287
Some High School, No Diploma	397 2.99%	2,665 5.07%	6,023 5.51%
High School Grad (Incl Equivalency)	1,176 8.85%	6,255 11.89%	12,681 11.60%
Some College, No Degree	1,787 13.44%	8,041 15.29%	16,971 15.53%
Associate Degree	506 3.81%	2,144 4.08%	4,625 4.23%
Bachelor Degree	4,220 31.75%	15,776 29.99%	33,420 30.58%
Advanced Degree	5,206 39.17%	17,718 33.69%	35,567 32.54%

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile		2 Mile		3 Mile	
2021 Population by Occupation	17,909		70,427		140,522	
Real Estate & Finance	961	5.37%	3,564	5.06%	6,929	4.93%
Professional & Management	7,682	42.89%	26,648	37.84%	51,267	36.48%
Public Administration	146	0.82%	756	1.07%	1,277	0.91%
Education & Health	2,409	13.45%	9,918	14.08%	21,018	14.96%
Services	787	4.39%	3,906	5.55%	8,086	5.75%
Information	205	1.14%	645	0.92%	1,415	1.01%
Sales	1,428	7.97%	6,483	9.21%	13,164	9.37%
Transportation	1,498	8.36%	5,710	8.11%	10,606	7.55%
Retail	692	3.86%	3,195	4.54%	6,020	4.28%
Wholesale	251	1.40%	743	1.05%	1,746	1.24%
Manufacturing	616	3.44%	2,480	3.52%	5,057	3.60%
Production	377	2.11%	2,266	3.22%	5,042	3.59%
Construction	169	0.94%	881	1.25%	2,101	1.50%
Utilities	272	1.52%	1,405	1.99%	2,946	2.10%
Agriculture & Mining	1	0.01%	26	0.04%	81	0.06%
Farming, Fishing, Forestry	1	0.01%	12	0.02%	21	0.01%
Other Services	414	2.31%	1,789	2.54%	3,746	2.67%
2021 Worker Travel Time to Job	7,545		30,793		62,767	
<30 Minutes	3,468	45.96%	14,724	47.82%	32,079	51.11%
30-60 Minutes	2,810	37.24%	11,506	37.37%	22,465	35.79%
60+ Minutes	1,267	16.79%	4,563	14.82%	8,223	13.10%
2010 Households by HH Size	6,997		27,214		56,205	
1-Person Households	1,832	26.18%	6,560	24.11%	15,325	27.27%
2-Person Households	2,182	31.18%	8,471	31.13%	17,315	30.81%
3-Person Households	1,109	15.85%	4,340	15.95%	8,713	15.50%
4-Person Households	1,162	16.61%	4,504	16.55%	8,460	15.05%
5-Person Households	505	7.22%	2,213	8.13%	4,139	7.36%
6-Person Households	149	2.13%	720	2.65%	1,431	2.55%
7 or more Person Households	58	0.83%	406	1.49%	822	1.46%
2021 Average Household Size	2.60		2.70		2.60	
Households						
2026 Projection	6,597		25,781		53,813	
2021 Estimate	6,719		26,249		54,691	
2010 Census	6,997		27,215		56,204	
Growth 2021 - 2026	-1.82%		-1.78%		-1.61%	
Growth 2010 - 2021	-3.97%		-3.55%		-2.69%	

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile	2 Mile	3 Mile
2021 Households by HH Income	6,720	26,249	54,689
<\$25,000	544 8.10%	2,332 8.88%	7,073 12.93%
\$25,000 - \$50,000	729 10.85%	3,422 13.04%	7,593 13.88%
\$50,000 - \$75,000	824 12.26%	3,355 12.78%	6,821 12.47%
\$75,000 - \$100,000	550 8.18%	2,507 9.55%	4,971 9.09%
\$100,000 - \$125,000	550 8.18%	2,442 9.30%	5,390 9.86%
\$125,000 - \$150,000	543 8.08%	2,120 8.08%	4,084 7.47%
\$150,000 - \$200,000	840 12.50%	2,703 10.30%	5,351 9.78%
\$200,000+	2,140 31.85%	7,368 28.07%	13,406 24.51%
2021 Avg Household Income	\$160,740	\$148,872	\$136,963
2021 Med Household Income	\$132,504	\$115,443	\$104,112
2021 Occupied Housing	6,719	26,249	54,691
Owner Occupied	5,442 80.99%	20,193 76.93%	38,584 70.55%
Renter Occupied	1,277 19.01%	6,056 23.07%	16,107 29.45%
2010 Housing Units	7,375	29,032	60,646
1 Unit	5,674 76.94%	20,086 69.19%	36,451 60.10%
2 - 4 Units	178 2.41%	2,498 8.60%	6,293 10.38%
5 - 19 Units	557 7.55%	2,104 7.25%	5,573 9.19%
20+ Units	966 13.10%	4,344 14.96%	12,329 20.33%
2021 Housing Value	5,443	20,193	38,585
<\$100,000	46 0.85%	170 0.84%	564 1.46%
\$100,000 - \$200,000	315 5.79%	1,315 6.51%	2,668 6.91%
\$200,000 - \$300,000	343 6.30%	2,371 11.74%	5,914 15.33%
\$300,000 - \$400,000	838 15.40%	3,519 17.43%	7,783 20.17%
\$400,000 - \$500,000	716 13.15%	2,671 13.23%	4,508 11.68%
\$500,000 - \$1,000,000	2,404 44.17%	7,003 34.68%	11,324 29.35%
\$1,000,000+	781 14.35%	3,144 15.57%	5,824 15.09%
2021 Median Home Value	\$596,402	\$503,606	\$452,428
2021 Housing Units by Yr Built	7,375	29,049	60,746
Built 2010+	41 0.56%	345 1.19%	1,225 2.02%
Built 2000 - 2010	134 1.82%	1,438 4.95%	3,685 6.07%
Built 1990 - 1999	265 3.59%	890 3.06%	2,049 3.37%
Built 1980 - 1989	260 3.53%	693 2.39%	1,468 2.42%
Built 1970 - 1979	527 7.15%	2,351 8.09%	4,714 7.76%
Built 1960 - 1969	1,403 19.02%	4,891 16.84%	9,366 15.42%
Built 1950 - 1959	2,051 27.81%	8,223 28.31%	16,226 26.71%
Built <1949	2,694 36.53%	10,218 35.18%	22,013 36.24%
2021 Median Year Built	1954	1954	1954

Demographic Summary Report

3434 Central St, Evanston, IL 60201

Building Type: **General Retail** Total Available: **0 SF**
 Secondary: - % Leased: **100%**
 GLA: **60,000 SF** Rent/SF/Yr: -
 Year Built: **1950**



Radius	1 Mile	2 Mile	3 Mile
Population			
2026 Projection	17,661	70,580	146,801
2021 Estimate	17,978	71,787	149,107
2010 Census	18,655	74,171	153,016
Growth 2021 - 2026	-1.76%	-1.68%	-1.55%
Growth 2010 - 2021	-3.63%	-3.21%	-2.55%
2021 Population by Hispanic Origin	919	5,821	13,288
2021 Population	17,978	71,787	149,107
White	14,402 80.11%	52,475 73.10%	105,377 70.67%
Black	394 2.19%	4,991 6.95%	10,451 7.01%
Am. Indian & Alaskan	27 0.15%	196 0.27%	448 0.30%
Asian	2,725 15.16%	12,121 16.88%	28,465 19.09%
Hawaiian & Pacific Island	5 0.03%	27 0.04%	46 0.03%
Other	425 2.36%	1,976 2.75%	4,320 2.90%
U.S. Armed Forces	0	0	17
Households			
2026 Projection	6,597	25,781	53,813
2021 Estimate	6,719	26,249	54,691
2010 Census	6,997	27,215	56,204
Growth 2021 - 2026	-1.82%	-1.78%	-1.61%
Growth 2010 - 2021	-3.97%	-3.55%	-2.69%
Owner Occupied	5,442 80.99%	20,193 76.93%	38,584 70.55%
Renter Occupied	1,277 19.01%	6,056 23.07%	16,107 29.45%
2021 Households by HH Income	6,720	26,249	54,689
Income: <\$25,000	544 8.10%	2,332 8.88%	7,073 12.93%
Income: \$25,000 - \$50,000	729 10.85%	3,422 13.04%	7,593 13.88%
Income: \$50,000 - \$75,000	824 12.26%	3,355 12.78%	6,821 12.47%
Income: \$75,000 - \$100,000	550 8.18%	2,507 9.55%	4,971 9.09%
Income: \$100,000 - \$125,000	550 8.18%	2,442 9.30%	5,390 9.86%
Income: \$125,000 - \$150,000	543 8.08%	2,120 8.08%	4,084 7.47%
Income: \$150,000 - \$200,000	840 12.50%	2,703 10.30%	5,351 9.78%
Income: \$200,000+	2,140 31.85%	7,368 28.07%	13,406 24.51%
2021 Avg Household Income	\$160,740	\$148,872	\$136,963
2021 Med Household Income	\$132,504	\$115,443	\$104,112

Traffic Impact Study

Proposed Kensington School

Evanston, Illinois



Prepared For:



Kenig, Lindgren, O'Hara, Aboona, Inc.

February 10, 2022

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O’Hara, Aboona, Inc. (KLOA, Inc.) for a proposed Kensington School to be located in Evanston, Illinois. The site, which currently contains the Unity Church on the North Shore, is located in the southwest quadrant of the intersection of Gross Point Road with Central Street. As proposed, the site will be redeveloped with a two-story, approximate 20,000 square-foot Kensington School and 34 parking spaces. The Kensington School will provide a full-day preschool program with a maximum of 165 students and approximately 23 staff members and will operate between 6:30 A.M. and 6:30 P.M. Access to the development is proposed to be provided via a full access drive located on Gross Point Road and an outbound right-turn only access drive on Central Street. The development will also provide access, via an easement, to 13 parking spaces serving the three multi-family buildings located directly southeast of the site.

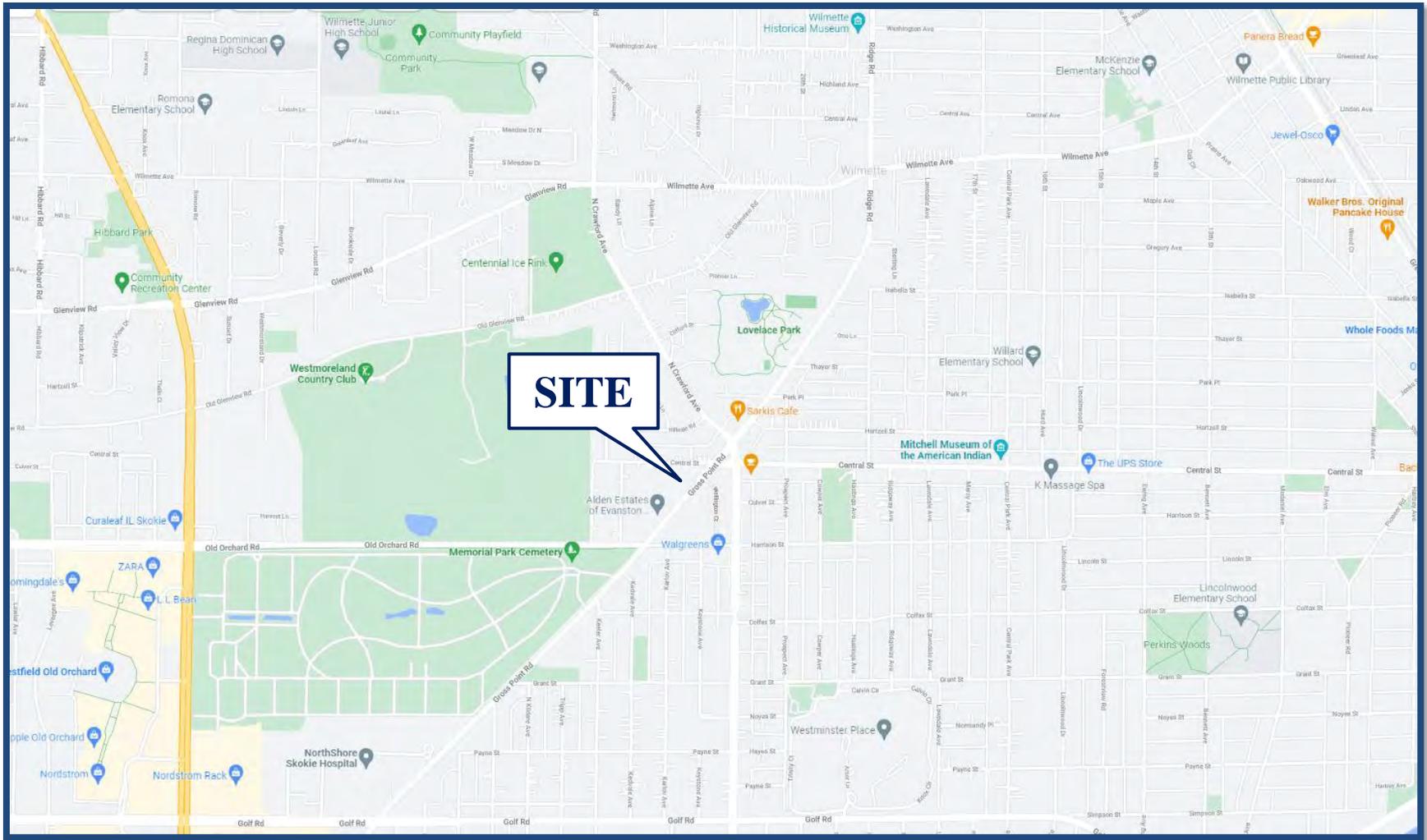
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed school will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate the traffic generated by the proposed school. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed school
- Directional distribution of the school traffic
- Vehicle trip generation for the school
- Future traffic conditions including access to the school
- Traffic analyses for the weekday morning and weekday evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following conditions:

1. Year 2022 Base Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area adjusted to account for the ongoing Covid 19 pandemic.
2. Year 2028 No-Build Conditions – Analyzes the capacity of the existing roadway system assuming Year 2022 base traffic volumes increased by an ambient area growth factor not attributable to any particular development.
3. Year 2028 Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the Year 2022 base traffic volumes, background development traffic growth, and the traffic estimated to be generated by the proposed school.



Site Location

Figure 1



Aerial View of Site

Figure 2

2. Existing Conditions

Existing traffic and roadway conditions were documented based on field visits and traffic counts conducted by KLOA, Inc. The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses, and peak hour traffic flows along area roadways.

Site Location

The site, which currently contains the Unity Church on the North Shore, is bounded by Central Street on the north, Gross Point Road on the east, multi-family buildings on the south, and a City of Evanston water tower and single-family homes on the west. Land uses within the vicinity of the site include single-family homes to the north and west and multi-family buildings to the south and east. Commercial developments are located east of the site along Central Street and at the Gross Point Road, Crawford Avenue, and Central Street intersections.

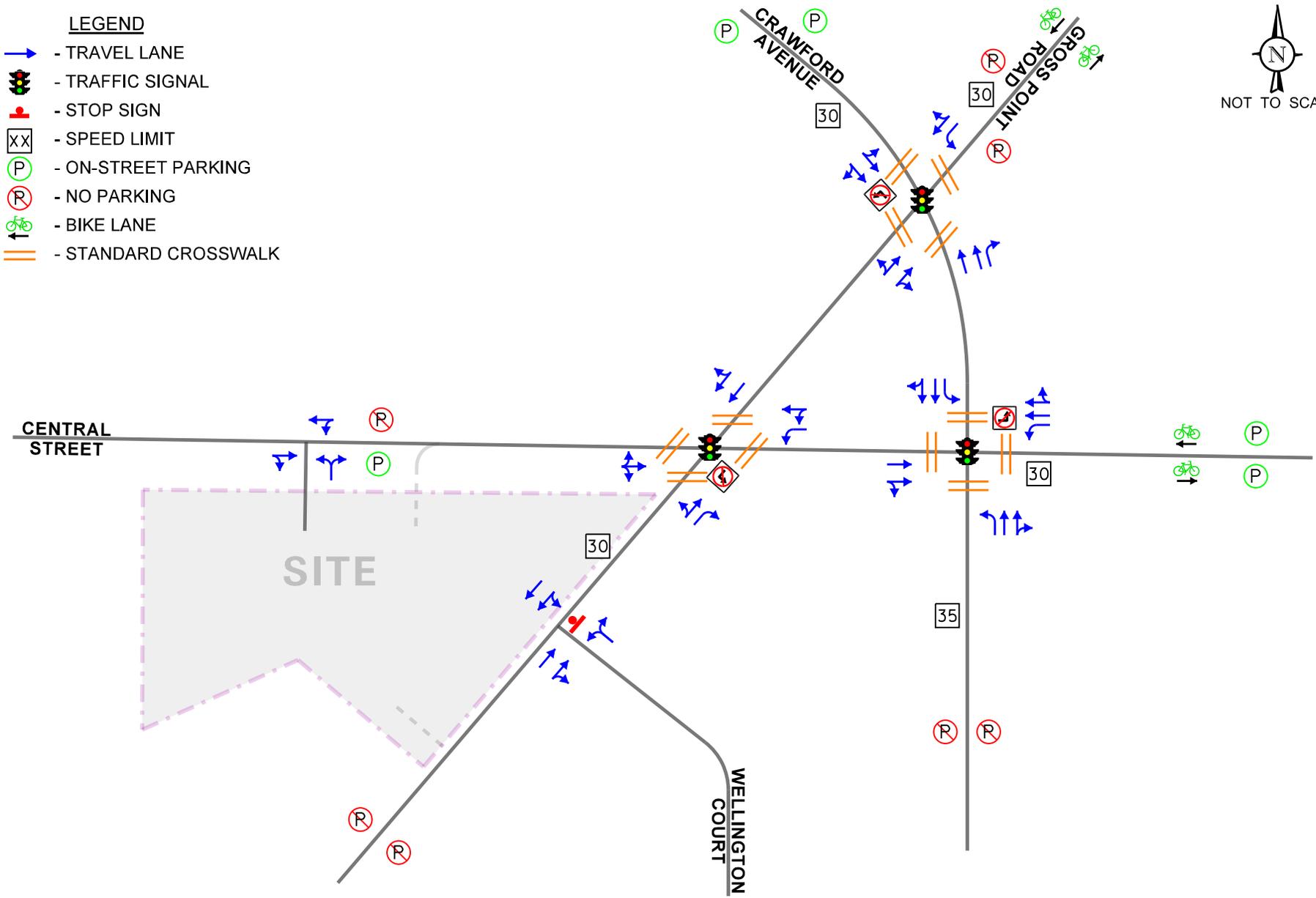
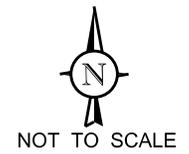
Existing Roadway System Characteristics

The characteristics of the existing roadways that surround the site are illustrated in **Figure 3** and described below.

Gross Point Road is a northeast-to-southwest, minor arterial road that has two lanes in each direction southwest of Crawford Avenue and one lane and one bike lane in each direction northeast of Crawford Avenue. Parking is generally prohibited on both sides of the road. At its signalized intersection with Central Street, Gross Point Road has a shared left-turn/through lane and a separate right-turn lane on the northeast-bound approach and a through lane and a shared through/right-turn lane on the southwest-bound approach. At its signalized intersection with Crawford Avenue, Gross Point Road has a shared left-turn/through lane and a shared through/right-turn lane on the northeast-bound approach and a separate left-turn lane and a shared through/right-turn lane on the southwest-bound approach. Gross Point Road is under the jurisdiction of the Illinois Department of Transportation (IDOT) southwest of Crawford Avenue and the City of Evanston northeast of Crawford Avenue, has an Annual Average Daily Traffic (AADT) volume of 11,100 vehicles (IDOT 2018), and has a posted speed limit of 30 mph.

Central Street is an east-west road. East of Gross Point Road, Central Street is classified as a minor arterial road that generally has one lane and one bike lane in each direction. West of Gross Point Road, Central Street is classified as a local road that has one lane in each direction. Parking is generally permitted on both sides of the road east of Crawford Avenue and permitted on the south side of the road west of Gross Point Road. At its signalized intersection with Crawford Avenue, Central Street has a separate left-turn lane, a through lane, and a shared through/right-turn lane on the westbound approach and a through lane and a shared through/right-turn lane on the eastbound approach. At its signalized intersection with Gross Point Road, Central Street has a separate left-turn lane and a shared left-turn/through lane on the westbound approach and a shared left-turn/through/right-turn lane on the eastbound approach. Central Street is under the jurisdiction of the City of Evanston, has an AADT volume of 12,500 vehicles (IDOT 2018) east of Crawford Avenue, and has a posted speed limit of 25 mph.

- LEGEND**
-  - TRAVEL LANE
 -  - TRAFFIC SIGNAL
 -  - STOP SIGN
 -  - SPEED LIMIT
 -  - ON-STREET PARKING
 -  - NO PARKING
 -  - BIKE LANE
 -  - STANDARD CROSSWALK



Kensington School
Evanston, Illinois

Existing Roadway Characteristics

Crawford Avenue is generally a north-south road. South of Gross Point Road, Crawford Avenue is classified as a minor arterial road that generally has two lanes in each direction divided by a median. Northwest of Gross Point Road, Crawford Avenue is classified as a major collector road and has one lane in each direction. Parking is generally permitted on both sides of the road northwest of Gross Point Road and prohibited on both sides of the road south of Gross Point Road. At its signalized intersection with Gross Point Road, Crawford Avenue has two through lanes and a separate right-turn lane on the northwest-bound approach and a shared left-turn/through lane and a shared through/right-turn lane on the southeast-bound approach. At its signalized intersection with Central Street, Crawford Avenue has a separate left-turn lane, a through lane, and a shared through/right-turn lane on both approaches. South of Gross Point Road, Crawford Avenue is under the jurisdiction of IDOT, has an AADT volume of 9,350 vehicles (IDOT 2018), and has a posted speed limit of 25 mph. Northwest of Gross Point Road, Crawford Avenue is under the jurisdiction of the City of Evanston, has an AADT volume of 6,000 vehicles (IDOT 2018), and has a posted speed limit of 30 mph.

Wellington Court is generally a north-south, local road that has one lane in each direction. Parking is prohibited on both sides of the road. At its intersection with Gross Point Road, Wellington Court has a shared left-turn/right-turn lane that is under stop sign control.

Public Transportation

The following summarizes the bus routes serving the area:

- *Pace Bus Route 421*, which runs along Wilmette Avenue, has a local bus stop approximately 0.6 miles from the site.
- *Pace Bus Routes 208 and 422*, which run along Golf Road, have a local bus stop approximately 0.7 miles from the site.

Bike Accommodations

Bike lanes are provided on both sides of (1) Central Street east of Crawford Avenue and (2) Gross Point Road northeast of Crawford Avenue. Further, Crawford Avenue northwest of Central Street is a designated a bike route. In addition, a Divvy bike station that can accommodate 11 bikes is located at the intersection of Central Street with Lincolnwood Drive.

Pedestrian Accommodations

Sidewalks are generally provided on both sides of the roads in the vicinity of the site except on the north side of Central Street west of Gross Point Road. Standard style crosswalks and pedestrian traffic signals are located at all three signalized intersections in the Gross Point Road/Crawford Avenue/Central Street triangle.

Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period vehicle, pedestrian, and bicycle counts at the following intersections:

- Gross Point Road with Crawford Avenue
- Gross Point Road with Central Street
- Gross Point Road with Wellington Court
- Central Street with Crawford Avenue
- Central Street with the existing church parking lot

The traffic counts were conducted during the weekday morning (7:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods on Tuesday, January 25, 2022. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:45 A.M. to 8:45 A.M. and the evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M.

To determine if the ongoing Covid 19 pandemic had any impact on the area traffic volumes, the 2022 traffic counts were compared to previous 2018 traffic counts conducted by IDOT. Based on the results of the comparison, the weekday morning and evening traffic volumes were increased by 20 percent. **Figure 4** illustrates the Year 2022 base peak hour vehicle traffic volumes and **Figure 5** illustrates the existing pedestrian and bicycle traffic volumes. Copies of the traffic counts are located in the Appendix.

Crash Data Analysis

KLOA, Inc. obtained crash data¹ for the past five years (2016 to 2020) for the following intersections:

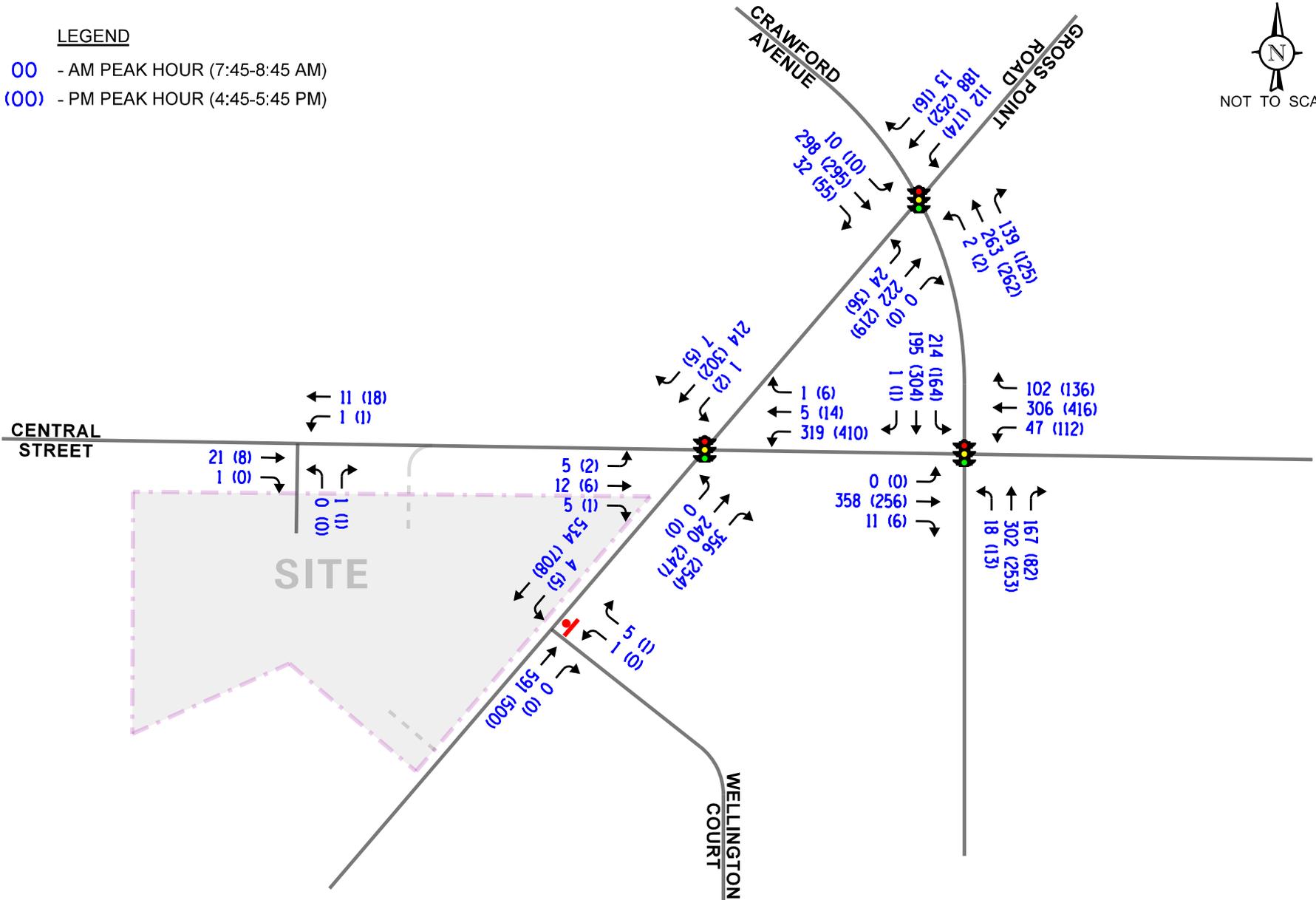
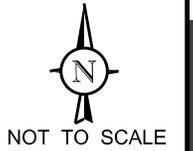
- Gross Point Road with Crawford Avenue
- Gross Point Road with Central Street
- Central Street with Crawford Avenue
- Gross Point Road with Wellington Court

Tables 1 through **3** summarize the results of the crash data for the intersections of Gross Point Road with Crawford Avenue, Gross Point Road with Central Street, and Central Street with Crawford Avenue. A review of the crash data indicated that a total of 10 crashes occurred at the intersection of Gross Point Road with Wellington Court during the five-year period. The review also indicated that no fatal crashes occurred at any of the intersections in the five-year period.

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.

LEGEND

- 00 - AM PEAK HOUR (7:45-8:45 AM)
- (00) - PM PEAK HOUR (4:45-5:45 PM)



LEGEND

- 00 - AM PEAK HOUR (7:45-8:45 AM)
- (00) - PM PEAK HOUR (4:45-5:45 PM)
- 00 (00)  - PEDESTRIAN VOLUME
- 00 (00)  - BICYCLE VOLUME

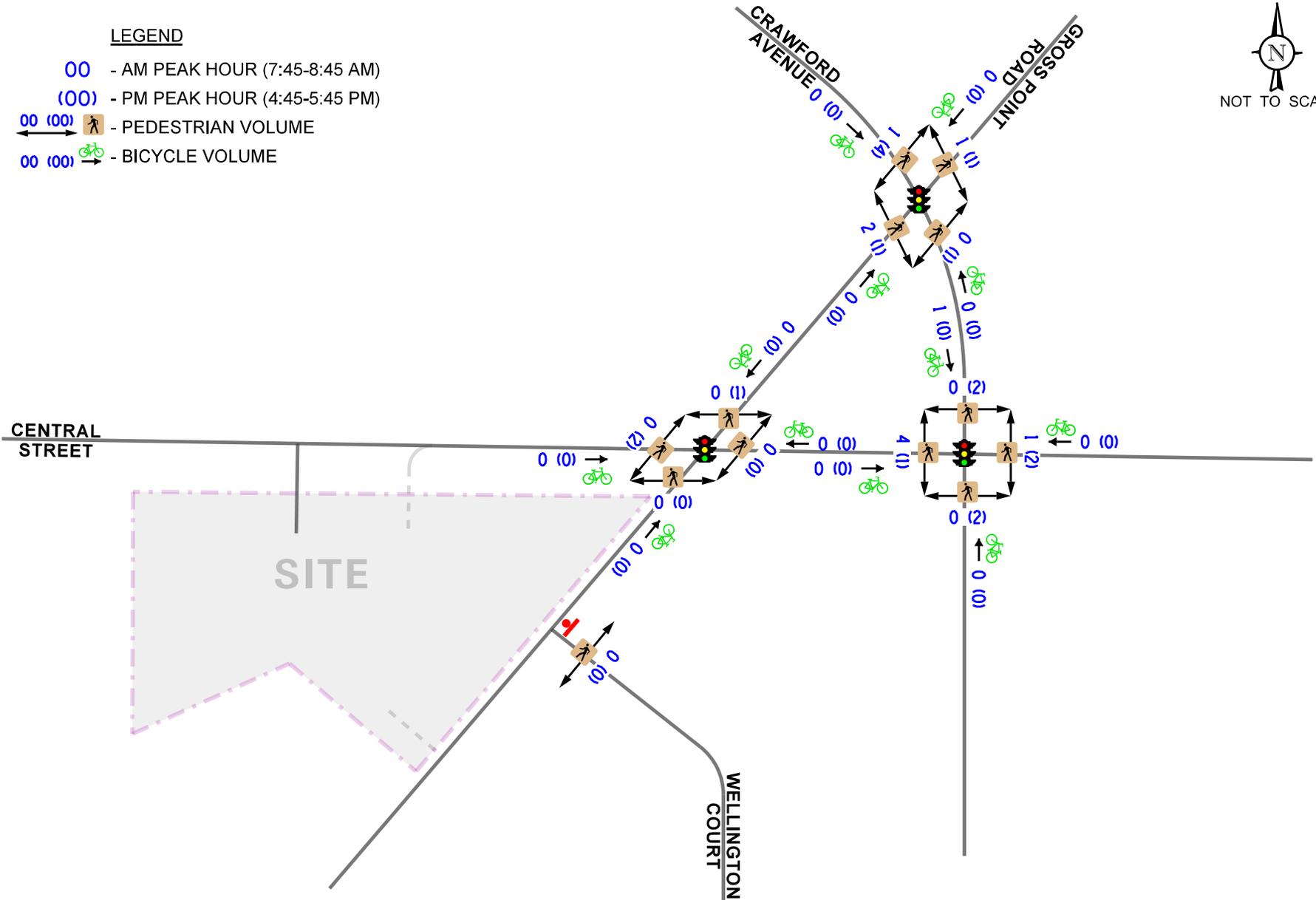
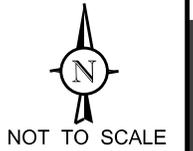


Table 1
GROSS POINT ROAD WITH CRAWFORD AVENUE – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2016	0	0	0	0	1	4	0	5
2017	1	0	0	0	0	0	0	1
2018	2	0	0	0	0	1	1	4
2019	0	0	1	0	0	0	0	1
2020	<u>2</u>	<u>0</u>	<u>1</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3</u>
Total	5	0	2	0	1	5	1	14
Average	1.0	0.0	<1.0	0.0	<1.0	1.0	<1.0	2.8

Table 2
GROSS POINT ROAD WITH CENTRAL STREET – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2016	0	0	1	1	1	0	0	3
2017	2	0	0	0	0	1	0	3
2018	0	0	0	0	1	1	0	2
2019	0	0	0	0	0	0	0	0
2020	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>1</u>	<u>0</u>	<u>1</u>
Total	2	0	1	1	2	3	0	9
Average	<1.0	0.0	<1.0	<1.0	<1.0	<1.0	0.0	1.8

Table 3
CENTRAL STREET WITH CRAWFORD AVENUE – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2016	5	0	0	0	0	0	0	5
2017	6	0	0	0	0	2	1	9
2018	3	0	0	0	0	1	0	4
2019	1	0	0	0	0	0	0	1
2020	<u>2</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>2</u>
Total	17	0	0	0	0	3	1	21
Average	3.4	0.0	0.0	0.0	0.0	<1.0	<1.0	4.2

3. Traffic Characteristics of the Proposed School

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed school, including the directional distribution and volumes of traffic that it will generate.

Proposed School Plan

As proposed, the site is to be redeveloped with a two-story, approximate 20,000 square-foot Kensington School. As proposed, the school will provide a full-day preschool program in 10 classrooms with a maximum enrollment of approximately 165 students and approximately 23 staff members. The school will operate between 6:30 A.M. and 6:30 P.M. According to the operator, most students will be dropped off between 6:30 and 8:30 A.M. and picked up between 4:00 and 6:00 P.M. All caregivers will be required to park and walk their students to and from the school. The school will have a total of 34 parking spaces.

Site Access

Access to the school is to be provided via the following two access drives:

- A full movement access drive located on the west side of Gross Point Road approximately 345 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control. Inbound left-turn movements will be accommodated via the inside northeast-bound lane along Gross Point Road.
- An outbound right-turn only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one outbound lane that will be under stop sign control. Inbound movements and the outbound left-turn movement will be restricted by channelizing the access drive and via appropriate signage and pavement markings.

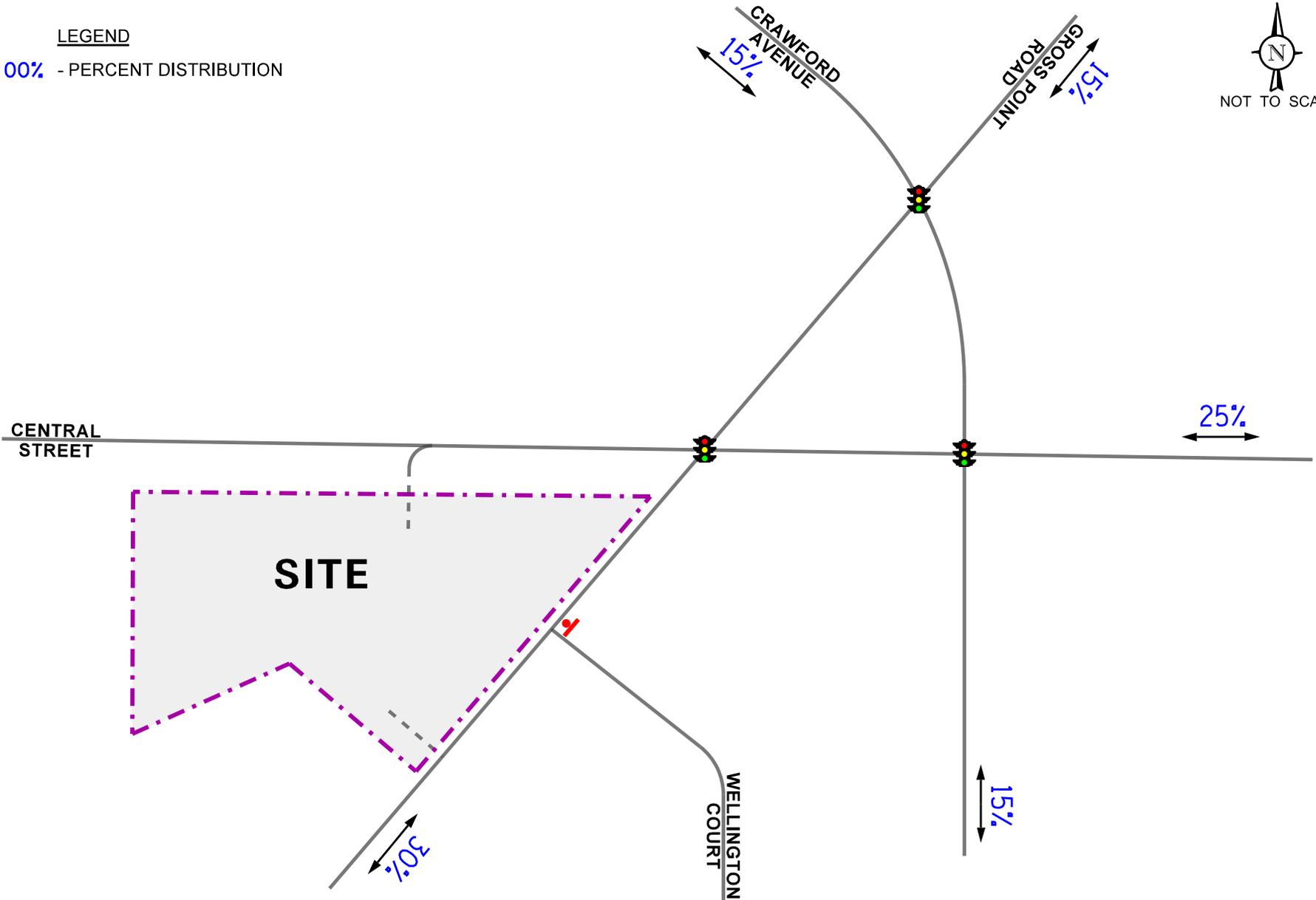
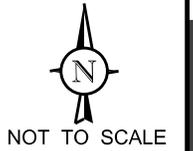
The access drives will also provide access, via an easement, to the 13 parking spaces serving the three multi-family buildings located directly southeast of the site. A copy of the preliminary site plan depicting the proposed school is included in the Appendix.

Directional Distribution

The directions from which parents and employees of the proposed school will approach and depart the site were estimated based on existing travel patterns and the operation of the roadway system. **Figure 6** illustrates the directional distribution of the school-generated traffic.

LEGEND

00% - PERCENT DISTRIBUTION



Kensington School
Evanston, Illinois

Estimated Directional Distribution



Job No: 21-353

Figure: 6

Estimated Site Traffic Generation

The traffic to be generated by the proposed school was estimated based on surveys of an existing Kensington School located in Elmhurst, Illinois. According to the operator, the Elmhurst school has nine classrooms and a similar operation to that of the proposed Evanston school. The surveys were performed on Wednesday, December 9, 2015 during the morning (6:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods. Given that the proposed school will provide 10 classrooms, the surveyed volumes were increased by 10 percent. **Table 4** tabulates the vehicle trips anticipated to be generated by the proposed school during the weekday morning and evening peak hours.

Table 4
ESTIMATED SCHOOL-GENERATED TRAFFIC VOLUMES

Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	In	Out	Total	In	Out	Total
Proposed Kensington School	59	59	118	58	65	123

Further, the school is typically closed after 6:30 P.M. on weeknights and on weekends. As such, other than during the morning and evening peak periods, the school generates a very limited volume of traffic during weekdays and little, if any, traffic on weekday evenings or weekends.

4. Projected Traffic Conditions

The total projected traffic volumes include the base traffic volumes, increase in background traffic due to ambient growth, and the traffic estimated to be generated by the proposed school.

School Traffic Assignment

The estimated weekday morning and evening peak hour traffic volumes that will be generated by the proposed school were assigned to the roadway system in accordance with the previously described directional distribution (Figure 6). The traffic assignment for the school is illustrated in **Figure 7**.

Background Traffic Conditions

The Year 2022 base traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on 2050 ADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP), the existing traffic volumes in the area are projected to increase by an average compound annual growth rate of 0.46 percent per year. As such, all of the traffic volumes were increased by a total of 3.0 percent (from 2022 to 2028) to represent Year 2028 conditions (buildout of the facility plus five years). A copy of the CMAP projections letter is included in the Appendix.

Access to the 13 parking spaces serving the three multi-family buildings directly southwest of the site is provided via the existing church parking lot. With the redevelopment of the site, access to the parking spaces is proposed to be provided via the two proposed access drives. As such, the existing traffic using these parking spaces was redistributed to the proposed access drives.

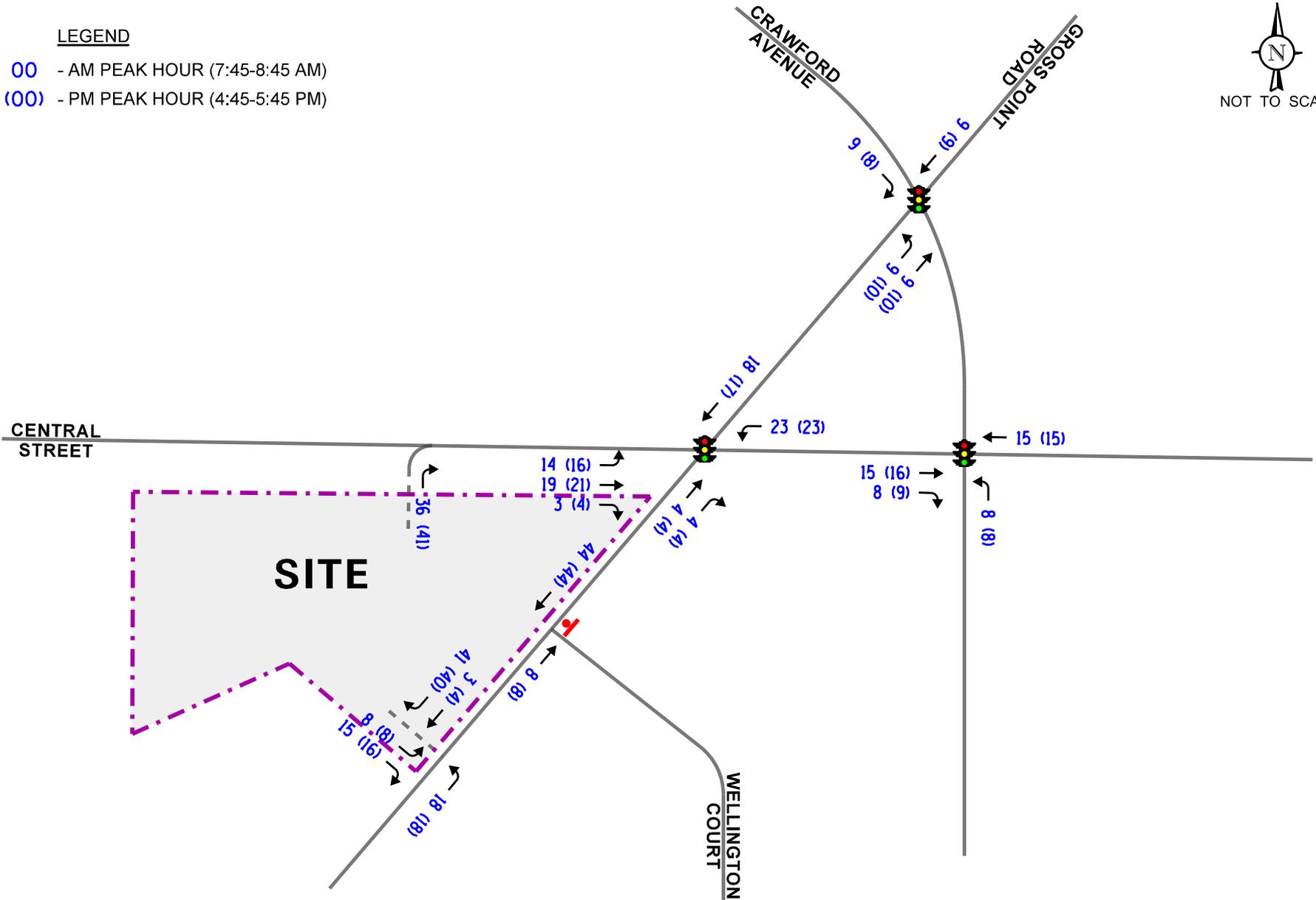
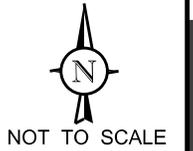
Figure 8 illustrates the 2028 no-build traffic volumes.

Total Projected Traffic Volumes

The school-generated traffic was added to the Year 2022 base traffic volumes accounting for background growth to determine the Year 2028 projected traffic volumes, shown in **Figure 9**.

LEGEND

- 00** - AM PEAK HOUR (7:45-8:45 AM)
- (00)** - PM PEAK HOUR (4:45-5:45 PM)



Kensington School
Evanston, Illinois

Estimated Site-Generated
Traffic Volumes

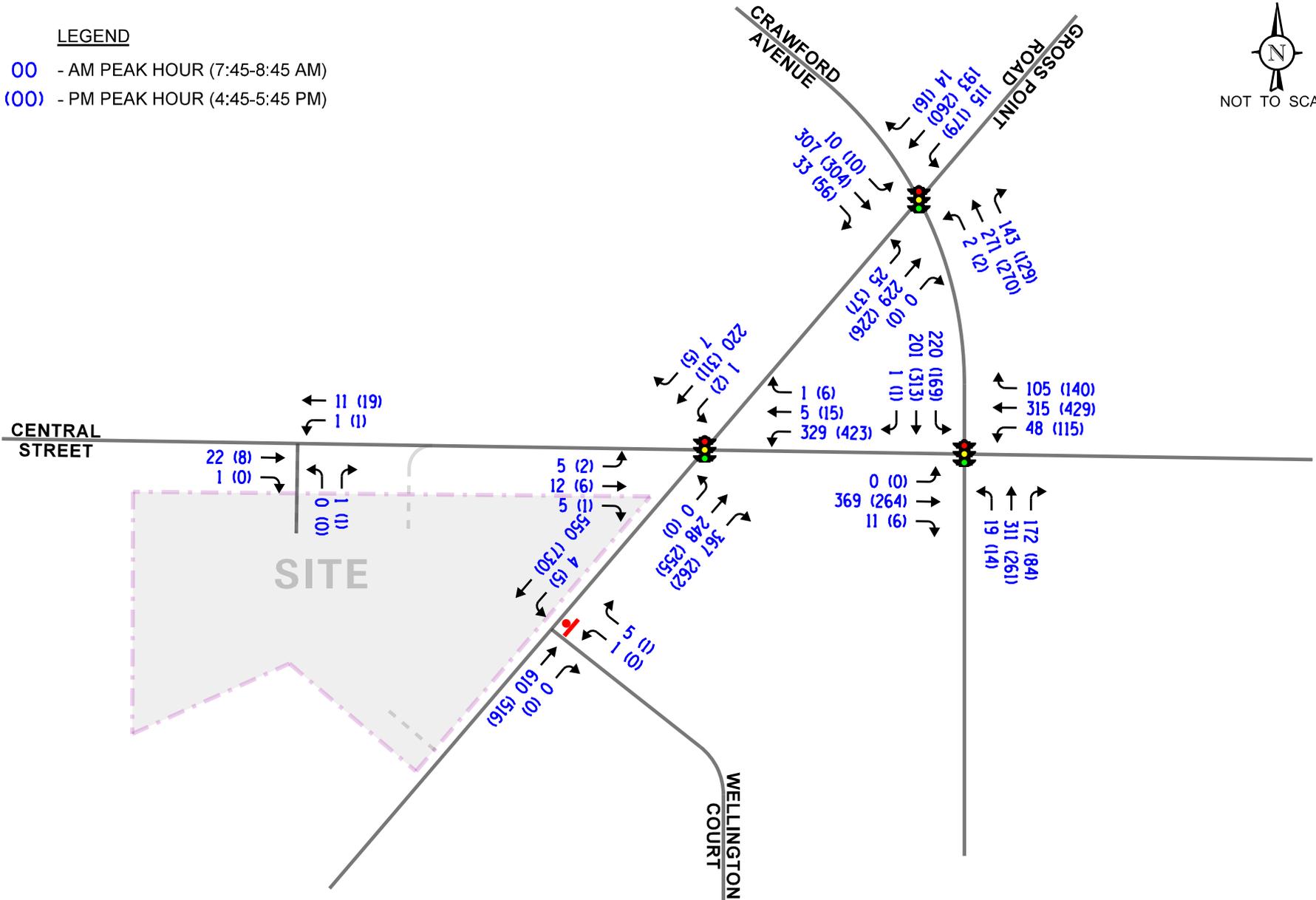
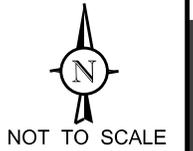


Job No: 21-353

Figure: 7

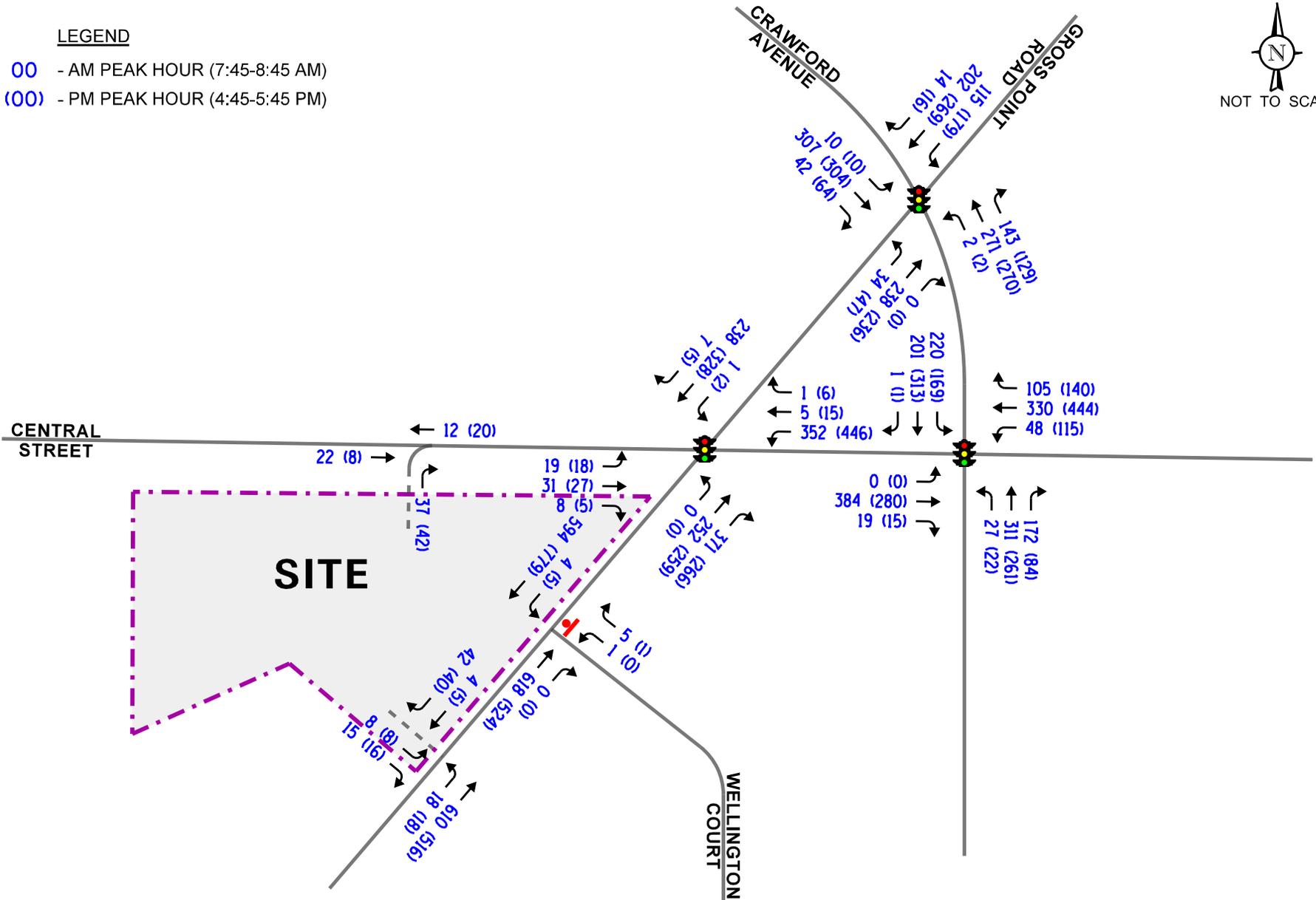
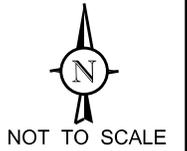
LEGEND

- 00** - AM PEAK HOUR (7:45-8:45 AM)
- (00)** - PM PEAK HOUR (4:45-5:45 PM)



LEGEND

- 00** - AM PEAK HOUR (7:45-8:45 AM)
- (00)** - PM PEAK HOUR (4:45-5:45 PM)



5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning and weekday evening peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning and weekday evening peak hours for the base, no-build, and Year 2028 total projected conditions.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and analyzed using Synchro/SimTraffic 11 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the Year 2022 base, Year 2028 no-build, and Year 2028 total projected conditions are presented in **Tables 5** through **10**. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 5

CAPACITY ANALYSIS RESULTS – GROSS POINT ROAD WITH CENTRAL STREET – SIGNALIZED

	Peak Hour	Eastbound	Westbound		Northeast-bound		Southwest-bound	Overall
		L/T/R	L	L/T	L/T	R	T/R	
Year 2022 Base Conditions	Weekday Morning	D 45.4	A 8.5	A 8.4	D 48.7	A 8.0	B 15.3	B 18.6
			A – 8.4		C – 24.4			
	Weekday Evening	D 48.1	B 15.2	B 15.2	D 51.1	A 7.6	B 19.2	C 22.0
			B – 15.2		C – 29.0			
Year 2028 No-Build Conditions	Weekday Morning	D 45.4	A 9.0	A 9.0	D 48.6	A 8.0	B 15.2	B 18.7
			A – 9.0		C – 24.4			
	Weekday Evening	D 48.1	B 16.4	B 16.4	D 49.9	A 7.3	B 18.5	C 21.9
			B – 16.4		C – 28.3			
Year 2028 Projected Conditions	Weekday Morning	D 52.0	B 10.9	B 10.9	D 48.0	A 7.9	B 18.3	C 20.6
			B – 10.9		C – 24.1			
	Weekday Evening	D 51.8	C 20.8	C 20.8	D 49.8	A 7.2	C 22.2	C 25.1
			C – 20.8		C – 28.2			

Letter denotes Level of Service L – Left Turn R – Right Turn
 Delay is measured in seconds. T – Through

Table 6

CAPACITY ANALYSIS RESULTS – CENTRAL STREET WITH CRAWFORD AVENUE – SIGNALIZED

	Peak Hour	Eastbound	Westbound		Northbound		Southbound		Overall
		T/R	L	T/R	L	T/R	L	T/R	
Year 2022 Base Conditions	Weekday Morning	D 53.7	C 28.8	C 31.9	A 9.2	B 15.1	B 10.0	A 9.9	C 26.5
			C – 31.6		B – 14.9		A – 10.0		
	Weekday Evening	D 53.6	C 34.0	D 36.9	A 8.0	B 13.8	A 7.8	A 10.0	C 27.1
			D – 36.4		B – 13.6		A – 9.2		
Year 2028 No-Build Conditions	Weekday Morning	D 53.5	C 28.6	C 31.9	A 9.4	B 15.7	B 10.5	B 10.0	C 26.6
			C – 31.5		B – 15.5		B – 10.3		
	Weekday Evening	D 53.3	C 34.0	D 37.1	A 8.1	B 14.1	A 7.9	B 10.1	C 27.2
			D – 36.6		B – 13.9		A – 9.3		
Year 2028 Projected Conditions	Weekday Morning	D 53.0	C 28.0	C 31.9	A 9.8	B 16.3	B 10.9	B 11.1	C 27.1
			C – 31.5		B – 15.9		B – 11.0		
	Weekday Evening	D 53.4	C 33.6	D 36.8	A 8.4	B 14.6	A 8.3	B 10.5	C 27.7
			D – 36.2		B – 14.2		A – 9.8		

Letter denotes Level of Service L – Left Turn R – Right Turn
 Delay is measured in seconds. T – Through

Table 7

CAPACITY ANALYSIS RESULTS – GROSS POINT ROAD WITH CRAWFORD AVENUE – SIGNALIZED

	Peak Hour	Southeast-bound	Northwest-bound		Northeast-bound	Southwest-bound		Overall
		L/T/R	T	R	L/T/R	L	T/R	
Year 2022 Base Conditions	Weekday Morning	A 9.3	A 4.5	A 1.1	C 31.6	C 32.5	D 35.3	B 17.7
			A – 3.3			C – 34.3		
	Weekday Evening	A 10.0	B 10.2	A 3.1	C 24.8	D 36.2	D 37.2	C 20.3
			A – 7.9			D – 36.8		
Year 2028 No-Build Conditions	Weekday Morning	A 9.6	A 4.5	A 1.1	C 31.3	C 32.5	D 35.2	B 17.7
			A – 3.3			C – 34.2		
	Weekday Evening	B 10.3	B 10.4	A 3.1	C 24.8	D 36.4	D 37.2	C 20.4
			A – 8.0			D – 36.9		
Year 2028 Projected Conditions	Weekday Morning	A 10.0	A 4.8	A 1.1	D 52.0	C 31.8	C 34.7	C 22.0
			A – 3.5			C – 33.7		
	Weekday Evening	B 10.8	B 10.9	A 3.1	D 47.5	D 35.3	D 36.2	C 24.6
			A – 8.4			D – 35.9		
Letter denotes Level of Service L – Left Turn R – Right Turn Delay is measured in seconds. T – Through								

Table 8
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2022 BASE CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	13.7	B	11.7
• Southwest-Bound Left Turn	A	8.8	A	8.5
LOS = Level of Service Delay is measured in seconds.				

Table 9
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2028 NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	13.9	B	11.9
• Southwest-Bound Left Turn	A	8.9	A	8.6
LOS = Level of Service Delay is measured in seconds.				

Table 11
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2028 TOTAL CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	14.1	B	11.9
• Southwest-Bound Left Turn	A	8.9	A	8.6
Gross Point Road with Access Drive				
• Eastbound Approach	C	17.8	C	19.4
• Northeast-Bound Left Turn	A	8.9	A	9.6
Central Street with Access Drive				
• Outbound Right Turn	A	8.6	A	8.5
LOS = Level of Service Delay is measured in seconds.				

Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

Gross Point Road with Central Street

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are operating at LOS D or better. Assuming the Year 2028 no-build conditions, the intersection is projected to continue to operate at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2028 total conditions, the intersection is projected to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Central Street with Crawford Avenue

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS C during the weekday morning and evening peak hours. Assuming the Year 2028 no-build conditions, the intersection is projected to continue to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2028 total conditions, the intersection is projected to continue to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Point Road with Crawford Avenue

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are operating at LOS D or better. Assuming the Year 2028 no-build conditions, the intersection is projected to continue to operate at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour and all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2028 total conditions, the intersection is projected to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at a LOS D or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Point Road with Wellington Court

The results of the capacity analysis indicate that the critical movements at this intersection currently operate at LOS B or better during the weekday morning and evening peak hours. Assuming the Year 2028 no-build conditions, the critical movements at this intersection are projected to continue to operate at LOS B or better during the weekday morning and evening peak hours. Assuming the Year 2028 total conditions, the critical movements at this intersection are projected to continue to operate at LOS B or better during the weekday morning and evening peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Pointe Road with Access Drive

Access to and from the development is proposed via a full-movement access drive to be located on the west side of Gross Point Road approximately 345 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control. Inbound left-turn movements will be accommodated via the inside northeast-bound lane along Gross Point Road. Assuming the Year 2028 total conditions, the critical movements at this intersection are projected to operate at LOS C or better during the weekday morning and evening peak hours. As such, the access drive will provide efficient and orderly access to and from the development with limited impact on Gross Point Road.

Central Street with Outbound Right-Turn Access Drive

Access from the development is proposed via an outbound right-turn only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one outbound lane that will be under stop sign control. Inbound movements and the outbound left-turn movement will be restricted by channelizing the access drive and via appropriate signage and pavement markings. Assuming the Year 2028 total conditions, the outbound right-turn movement at this intersection is projected to operate at LOS A. As such, the access drive will provide efficient and orderly access from the development with limited impact on Central Street.

6. Conclusion

Based on existing conditions and the traffic capacity analyses for the full buildout of the school, the findings and recommendations of this study are outlined below:

- The proposed Kensington School primarily generates traffic from 6:30 A.M. to 8:30 A.M. and from 4:00 P.M. to 6:00 P.M. Further, the school is typically closed after 6:30 P.M. on weeknights and on weekends. As such, other than during the morning and evening peak periods, the Kensington School generates a limited volume of traffic during weekdays and little, if any, traffic on weekday evenings or weekends.
- Access to the school is to be provided via the following two access drives:
 - A full-movement access drive located on the west side of Gross Point Road approximately 345 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control. Inbound left-turn movements will be accommodated via the inside northeast-bound lane along Gross Point Road.
 - An outbound right-turn only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one outbound lane that will be under stop sign control. Inbound movements and the outbound left-turn movement will be restricted by channelizing the access drive and via appropriate signage and pavement markings. This access drive will replace the full access drive on Central Street serving the church.
- The access drives will also provide access, via an easement, to the 13 parking spaces serving the three multi-family buildings located directly southeast of the site.
- The access drives will provide efficient and orderly access with limited impact on the existing roadway system
- The results of the capacity analyses show that the roadway system has sufficient reserve capacity to accommodate the traffic to be generated by the proposed school and no geometric improvements and/or traffic control modifications are required.

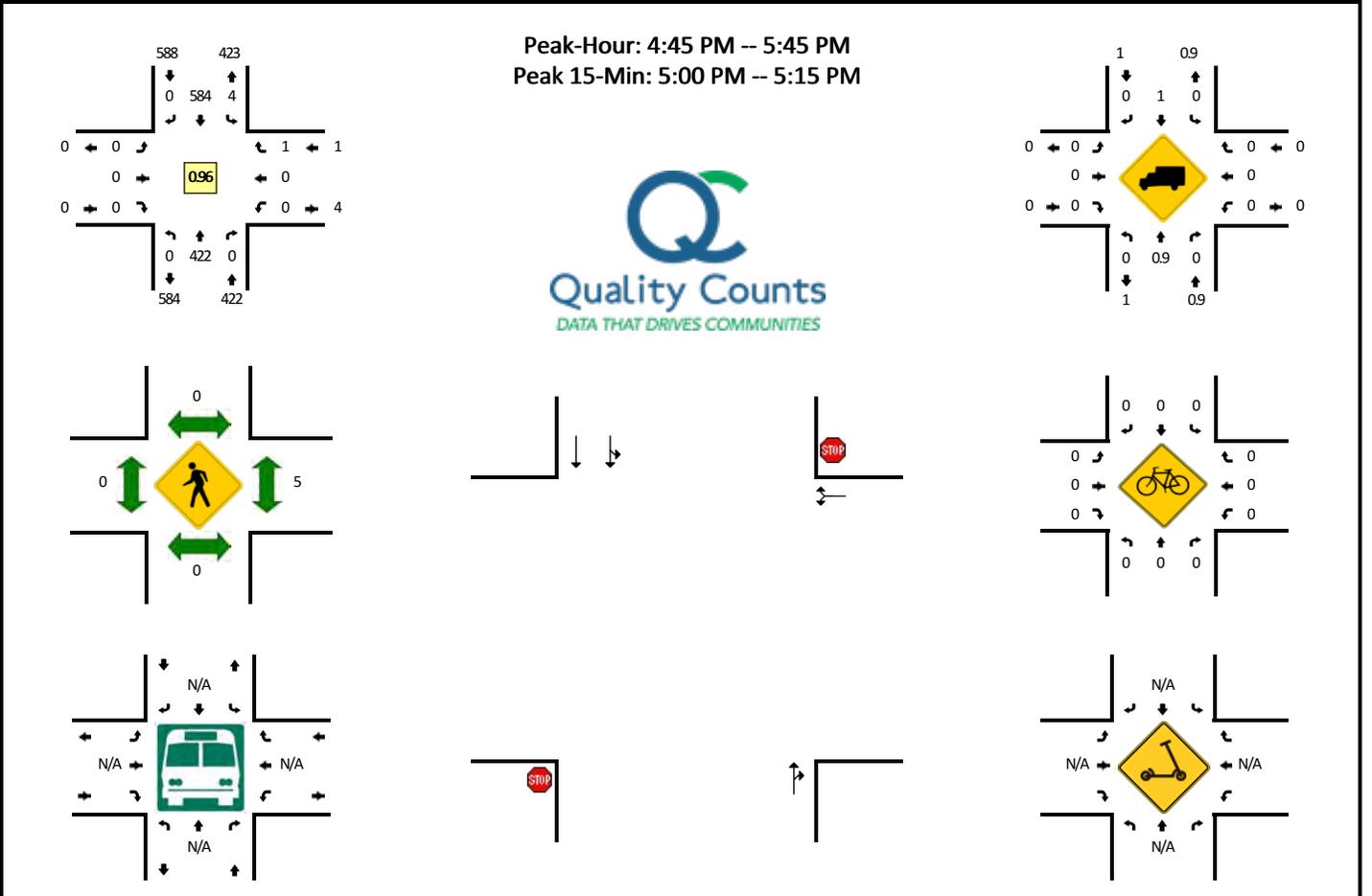
Appendix

Traffic Count Summary Sheets
Site Plan
CMAP Projections Letter
Level of Service Table
Capacity Analysis Summary Sheets

Traffic Count Summary Sheets

LOCATION: Gross Point Rd -- Wellington Ct
CITY/STATE: Evanston, IL

QC JOB #: 15687808
DATE: Tue, Jan 25 2022

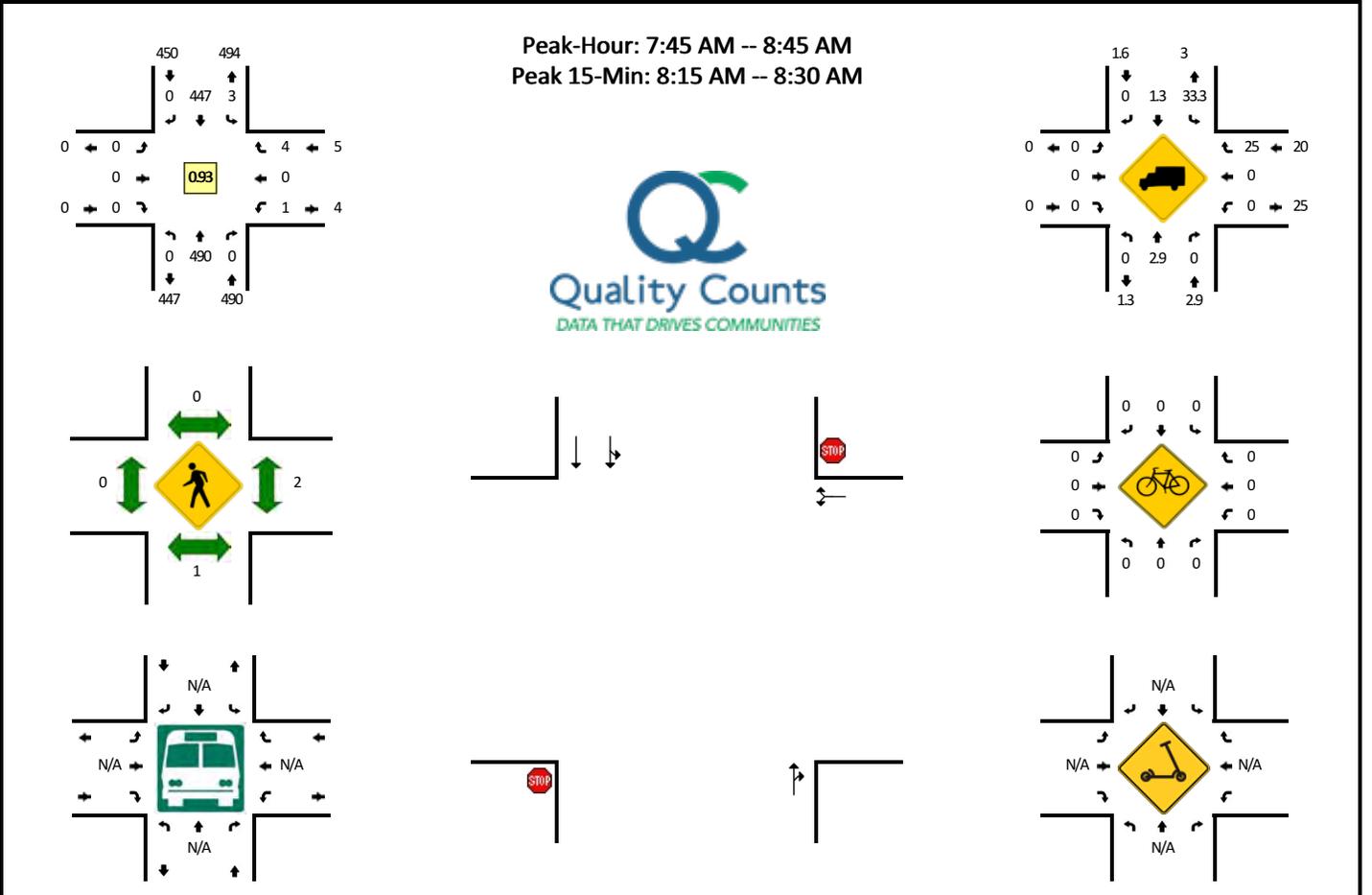


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Wellington Ct (Eastbound)				Wellington Ct (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	0	93	1	0	0	131	0	0	0	0	0	0	0	0	1	0	226	
4:15 PM	0	90	0	0	0	145	0	0	0	0	0	0	0	0	2	0	237	
4:30 PM	0	99	0	0	0	129	0	0	0	0	0	0	0	0	2	0	230	
4:45 PM	0	109	0	0	1	125	0	0	0	0	0	0	0	0	1	0	236	929
5:00 PM	0	104	0	0	1	159	0	0	0	0	0	0	0	0	0	0	264	967
5:15 PM	0	95	0	0	0	164	0	0	0	0	0	0	0	0	0	0	259	989
5:30 PM	0	114	0	0	2	136	0	0	0	0	0	0	0	0	0	0	252	1011
5:45 PM	0	99	0	0	0	122	0	0	0	0	0	0	0	0	1	0	222	997
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	416	0	0	4	636	0	0	0	0	0	0	0	0	0	0	1056	
Heavy Trucks	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	4	
Buses																		
Pedestrians		0				0				0				4			4	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		

Comments:

LOCATION: Gross Point Rd -- Wellington Ct
CITY/STATE: Evanston, IL

QC JOB #: 15687807
DATE: Tue, Jan 25 2022

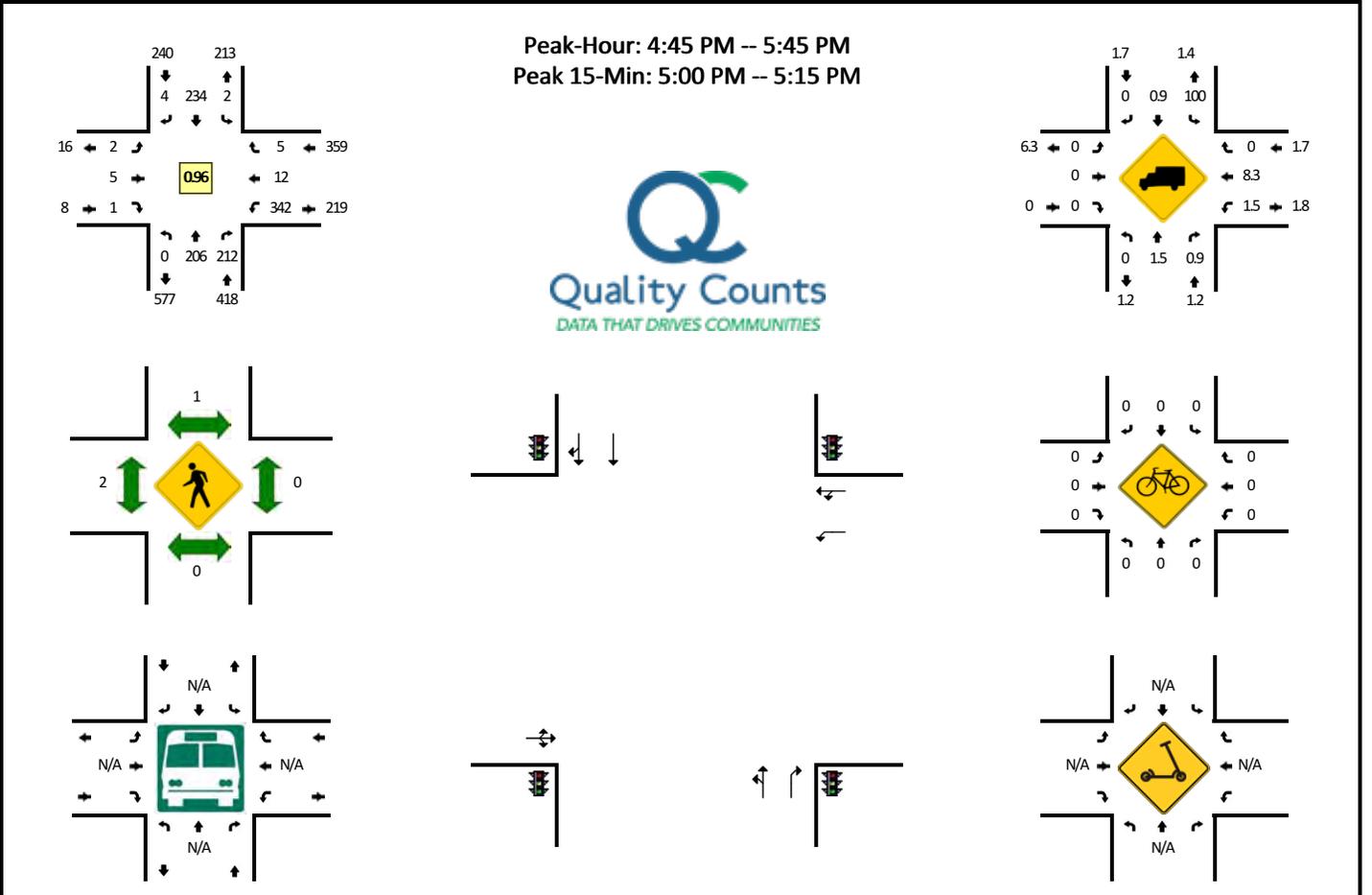


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Wellington Ct (Eastbound)				Wellington Ct (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	0	85	0	0	0	67	0	0	0	0	0	0	0	0	0	0	152	
7:15 AM	0	115	0	0	0	65	0	0	0	0	0	0	0	0	0	0	180	
7:30 AM	0	99	0	0	1	94	0	0	0	0	0	0	0	0	0	0	194	
7:45 AM	0	106	0	0	0	132	0	0	0	0	0	0	0	0	0	0	238	764
8:00 AM	0	122	0	0	0	92	0	0	0	0	0	0	0	1	0	0	215	827
8:15 AM	0	138	0	0	2	110	0	0	0	0	0	0	0	2	1	0	253	900
8:30 AM	0	124	0	0	1	113	0	0	0	0	0	0	0	1	0	0	239	945
8:45 AM	0	123	0	0	0	98	0	0	0	0	0	0	1	0	1	0	223	930
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	552	0	0	8	440	0	0	0	0	0	0	0	0	8	4	1012	
Heavy Trucks	0	8	0	0	4	4	0	0	0	0	0	0	0	0	0	0	16	
Buses																		
Pedestrians		0				0					0			8			8	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		

Comments:

LOCATION: Gross Point Rd -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687806
DATE: Tue, Jan 25 2022

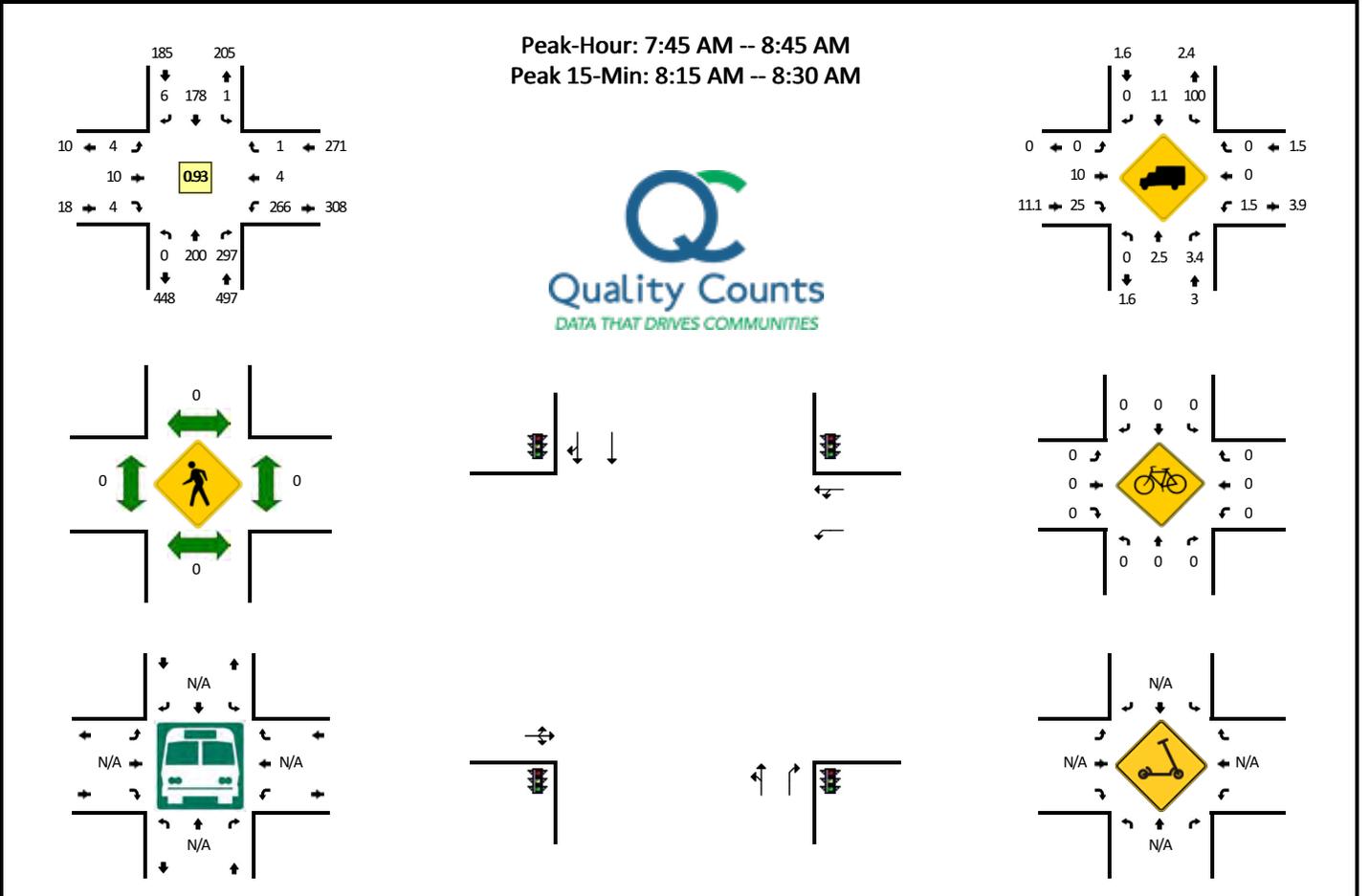


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
4:00 PM	1	44	52	0	1	43	0	0	0	1	1	0	86	1	1	0	231		
4:15 PM	1	29	62	0	0	53	2	0	2	1	0	0	89	2	2	0	243		
4:30 PM	0	43	59	0	0	45	0	0	0	3	1	0	84	2	1	0	238		
4:45 PM	0	54	56	0	1	49	1	0	1	0	0	0	76	7	3	0	248	960	
5:00 PM	0	61	44	0	1	63	1	0	0	1	0	0	93	2	0	0	266	995	
5:15 PM	0	41	51	0	0	71	0	0	1	1	0	0	88	1	1	0	255	1007	
5:30 PM	0	50	61	0	0	51	2	0	0	3	1	0	85	2	1	0	256	1025	
5:45 PM	0	56	49	0	0	44	2	0	0	2	0	0	79	0	0	0	232	1009	
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total		
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
All Vehicles	0	244	176	0	4	252	4	0	0	4	0	0	372	8	0	0	1064		
Heavy Trucks	0	0	0		4	0	0		0	0	0		4	4	0		12		
Buses																			
Pedestrians		0				0				8				0				8	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0		
Scoters																			

Comments:

LOCATION: Gross Point Rd -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687805
DATE: Tue, Jan 25 2022

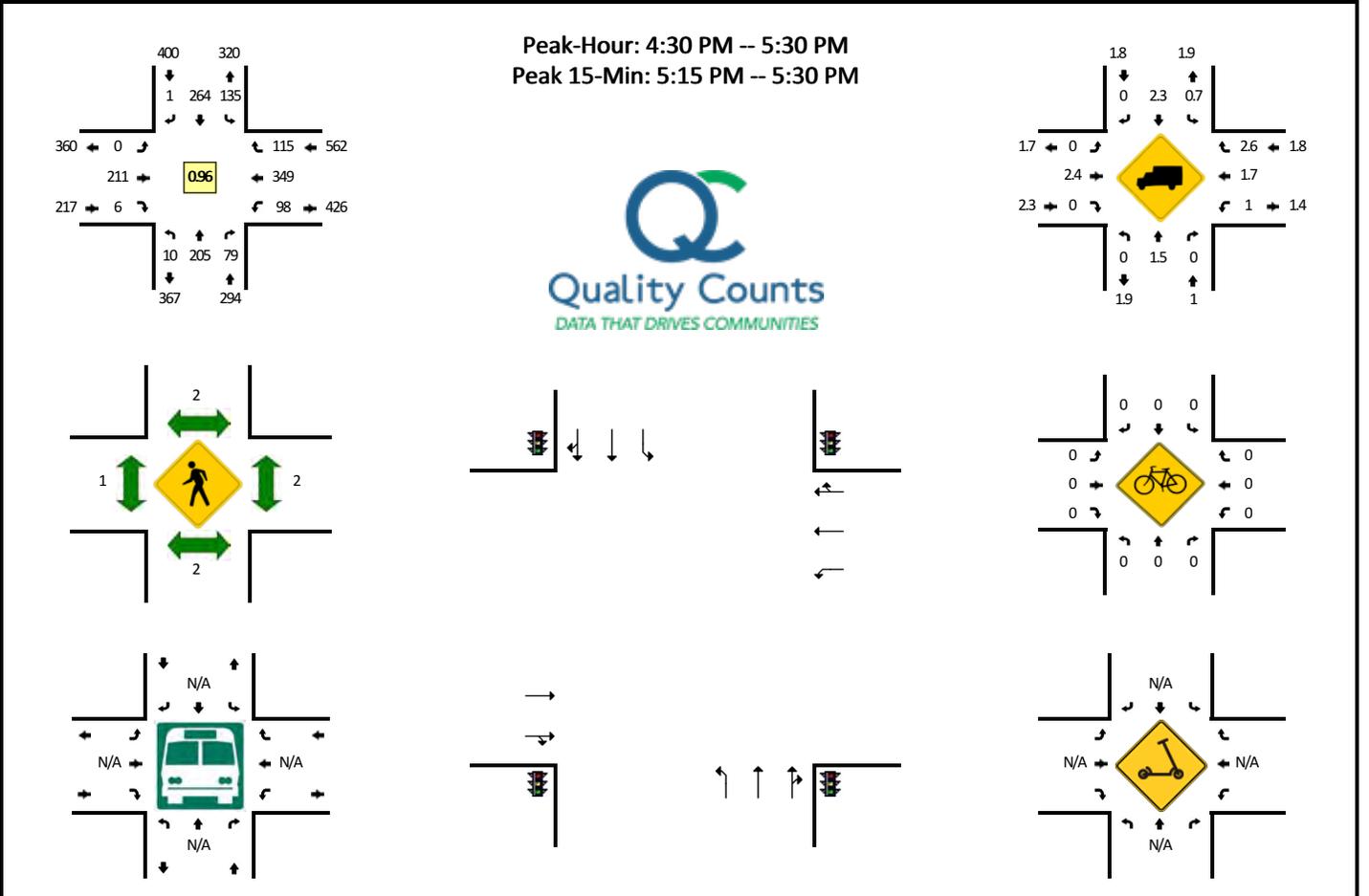


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	2	29	55	0	0	20	1	0	0	1	0	0	46	0	0	0	154	
7:15 AM	0	50	63	0	0	19	0	0	0	0	1	0	45	0	0	0	178	
7:30 AM	0	42	57	0	1	31	0	0	0	3	2	0	61	1	0	0	198	
7:45 AM	0	47	61	0	1	46	2	0	1	3	1	0	84	1	0	0	247	777
8:00 AM	0	46	75	0	0	33	1	0	1	3	0	0	58	1	1	0	219	842
8:15 AM	0	59	83	0	0	46	3	0	1	3	0	0	66	1	0	0	262	926
8:30 AM	0	48	78	0	0	53	0	0	1	1	3	0	58	1	0	0	243	971
8:45 AM	0	59	66	0	0	39	2	0	1	3	1	0	58	3	0	0	232	956
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	236	332	0	0	184	12	0	4	12	0	0	264	4	0	0	1048	
Heavy Trucks	0	0	8		0	8	0		0	0	0		0	0	0		16	
Buses																	0	
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																	0	

Comments:

LOCATION: N Crawford Ave -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687804
DATE: Tue, Jan 25 2022

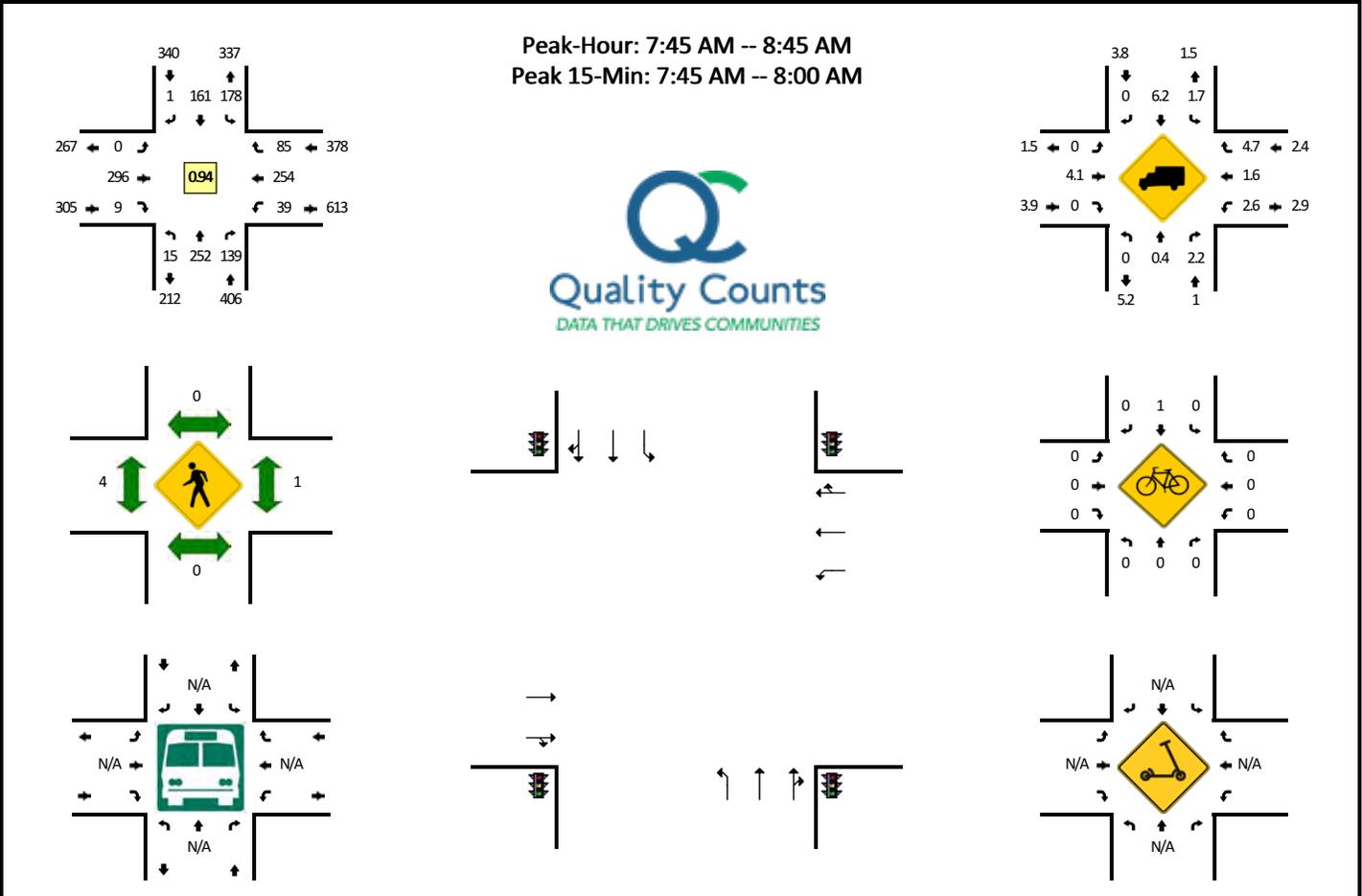


15-Min Count Period Beginning At	N Crawford Ave (Northbound)				N Crawford Ave (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	5	49	27	1	31	75	0	0	0	48	5	0	31	78	34	0	384	
4:15 PM	4	45	23	0	33	62	0	0	0	61	2	0	17	87	18	0	352	
4:30 PM	3	51	30	0	28	62	0	0	0	60	1	0	23	83	33	1	375	
4:45 PM	4	54	16	0	39	65	1	0	0	51	2	0	24	81	19	0	356	1467
5:00 PM	1	46	11	0	21	67	0	0	0	47	3	0	26	99	36	0	357	1440
5:15 PM	2	54	22	0	47	70	0	0	0	53	0	0	24	86	27	0	385	1473
5:30 PM	4	54	19	1	30	51	0	0	0	63	0	0	18	78	31	0	349	1447
5:45 PM	1	67	23	0	28	60	0	0	0	52	2	0	19	78	23	0	353	1444
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	8	216	88	0	188	280	0	0	0	212	0	0	96	344	108	0	1540	
Heavy Trucks	0	4	0		0	4	0		0	0	0		4	4	0		16	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

Comments:

LOCATION: N Crawford Ave -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687803
DATE: Tue, Jan 25 2022

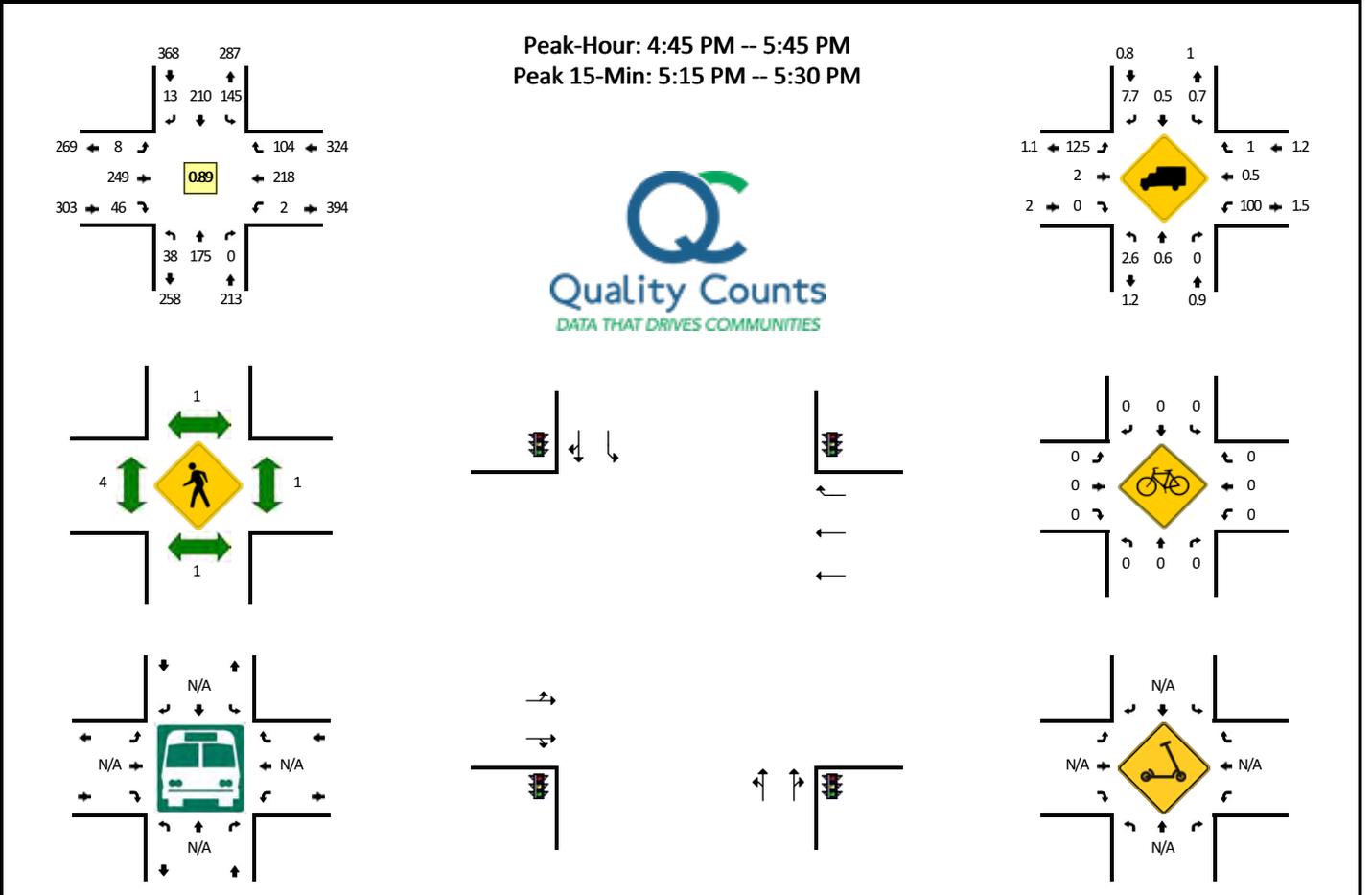


15-Min Count Period Beginning At	N Crawford Ave (Northbound)				N Crawford Ave (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	0	37	10	0	14	12	0	0	0	52	1	0	11	46	17	0	200	
7:15 AM	0	64	16	0	33	29	0	0	0	64	0	0	6	47	25	0	284	
7:30 AM	3	77	27	0	33	40	0	0	0	62	1	0	12	61	27	0	343	
7:45 AM	3	84	29	1	48	38	0	0	0	65	0	0	10	78	24	0	380	1207
8:00 AM	3	55	34	0	52	42	0	0	0	72	4	0	10	57	17	0	346	1353
8:15 AM	4	60	37	1	40	31	0	0	0	81	3	0	11	62	22	0	352	1421
8:30 AM	2	53	39	1	38	50	1	0	0	78	2	0	8	57	22	0	351	1429
8:45 AM	4	56	39	0	43	47	0	0	0	71	1	0	8	61	40	0	370	1419
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	12	336	116	4	192	152	0	0	0	260	0	0	40	312	96	0	1520	
Heavy Trucks	0	4	4		4	4	0		0	16	0		4	0	8		44	
Buses																		
Pedestrians		0				0				4				0			4	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

Comments:

LOCATION: Gross Point Rd -- N Crawford Ave
CITY/STATE: Evanston, IL

QC JOB #: 15687802
DATE: Tue, Jan 25 2022

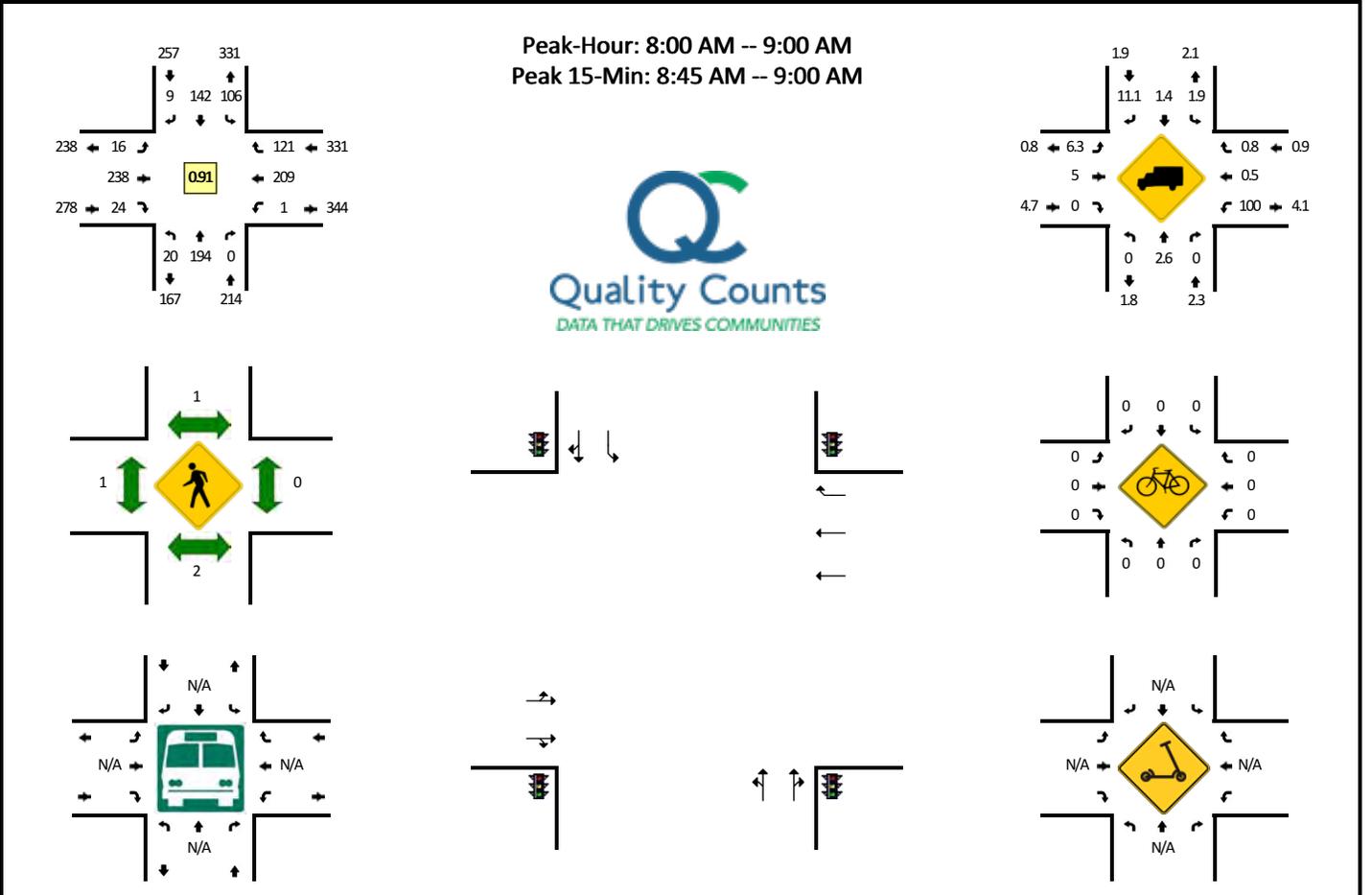


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				N Crawford Ave (Eastbound)				N Crawford Ave (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	10	39	0	0	37	42	1	0	2	68	5	0	1	67	19	0	291	
4:15 PM	8	24	0	0	32	45	4	0	1	60	10	0	0	43	18	0	245	
4:30 PM	3	42	0	0	30	39	2	0	3	59	4	0	0	56	34	0	272	
4:45 PM	11	42	0	0	39	35	4	0	2	64	16	0	2	49	23	0	287	1095
5:00 PM	8	52	0	0	32	58	4	0	2	57	9	0	0	56	24	0	302	1106
5:15 PM	10	39	0	0	51	67	1	0	2	72	13	0	0	62	24	0	341	1202
5:30 PM	9	42	0	0	23	50	4	0	2	56	8	0	0	51	33	0	278	1208
5:45 PM	10	47	0	0	25	30	1	0	1	58	4	0	0	57	27	0	260	1181
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	40	156	0	0	204	268	4	0	8	288	52	0	0	248	96	0	1364	
Heavy Trucks	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

Comments:

LOCATION: Gross Point Rd -- N Crawford Ave
CITY/STATE: Evanston, IL

QC JOB #: 15687801
DATE: Tue, Jan 25 2022



15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				N Crawford Ave (Eastbound)				N Crawford Ave (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	3	24	0	0	8	16	1	0	0	18	5	0	0	33	19	0	127	
7:15 AM	0	52	0	0	14	15	2	0	1	51	5	0	0	61	26	0	227	
7:30 AM	2	39	1	0	16	32	1	0	1	56	3	0	1	67	37	0	256	
7:45 AM	3	44	0	0	17	44	3	0	0	72	9	0	1	65	36	0	294	904
8:00 AM	8	40	0	0	25	24	1	0	2	65	3	0	0	52	22	0	242	1019
8:15 AM	5	49	0	0	19	40	3	0	3	54	5	0	1	58	28	0	265	1057
8:30 AM	4	47	0	0	32	43	4	0	3	57	10	0	0	45	30	0	275	1076
8:45 AM	3	58	0	0	30	35	1	0	8	62	6	0	0	54	41	0	298	1080

Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
All Vehicles	12	232	0	0	120	140	4	0	32	248	24	0	0	216	164	0	1192
Heavy Trucks	0	4	0	0	0	4	0	0	4	12	0	0	0	0	4	0	28
Buses																	
Pedestrians		0				0				0				0			0
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0
Scooters																	

Comments:

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Counts: All Vehicles - by Mvmt

Intersection # 3 central/unitychurch

Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	0	0	0	0	0	0	4	0	4
715	0	0	0	0	2	0	0	0	0	0	1	0	3
730	0	0	0	0	0	0	0	0	0	0	7	0	7
745	0	0	0	0	5	1	0	0	0	0	4	0	10
800	0	0	0	0	4	0	0	0	0	0	4	0	8
815	0	0	0	0	2	0	1	0	0	1	1	0	5
830	0	0	0	0	6	0	0	0	0	0	8	0	14
845	0	0	0	0	4	0	0	0	0	0	4	0	8
1600	0	0	0	0	3	2	0	0	0	0	2	0	7
1615	0	0	0	0	2	0	1	0	1	0	4	0	8
1630	0	0	0	0	4	0	0	0	0	0	4	0	8
1645	0	0	0	0	2	0	1	0	0	0	3	0	6
1700	0	0	0	0	2	0	0	0	0	0	3	0	5
1715	0	0	0	0	4	1	0	0	0	0	2	0	7
1730	0	0	0	0	5	0	0	0	0	0	1	0	6
1745	0	0	0	0	4	0	0	0	0	0	2	0	6
Total	0	0	0	0	49	4	3	0	1	1	54	0	112

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Counts: All Vehicles - Totals

Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	0	0	4	0	4	0	0	4
715	0	2	0	1	0	1	0	2	3
730	0	0	0	7	0	7	0	0	7
745	0	6	0	4	0	4	1	5	10
800	0	4	0	4	0	4	0	4	8
815	0	2	1	2	0	2	1	2	5
830	0	6	0	8	0	8	0	6	14
845	0	4	0	4	0	4	0	4	8
1600	0	5	0	2	0	2	2	3	7
1615	0	2	2	4	0	5	0	3	8
1630	0	4	0	4	0	4	0	4	8
1645	0	2	1	3	0	4	0	2	6
1700	0	2	0	3	0	3	0	2	5
1715	0	5	0	2	0	2	1	4	7
1730	0	5	0	1	0	1	0	5	6
1745	0	4	0	2	0	2	0	4	6
Total	0	53	4	55	0	57	5	50	112

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Flow Rates: by Movement

Intersection # 3 central/unitychurch

Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	0	0	0	0	0	0	16	0	16
715	0	0	0	0	8	0	0	0	0	0	4	0	12
730	0	0	0	0	0	0	0	0	0	0	28	0	28
745	0	0	0	0	20	4	0	0	0	0	16	0	40
800	0	0	0	0	16	0	0	0	0	0	16	0	32
815	0	0	0	0	8	0	4	0	0	4	4	0	20
830	0	0	0	0	24	0	0	0	0	0	32	0	56
845	0	0	0	0	16	0	0	0	0	0	16	0	32
1600	0	0	0	0	12	8	0	0	0	0	8	0	28
1615	0	0	0	0	8	0	4	0	4	0	16	0	32
1630	0	0	0	0	16	0	0	0	0	0	16	0	32
1645	0	0	0	0	8	0	4	0	0	0	12	0	24
1700	0	0	0	0	8	0	0	0	0	0	12	0	20
1715	0	0	0	0	16	4	0	0	0	0	8	0	28
1730	0	0	0	0	20	0	0	0	0	0	4	0	24
1745	0	0	0	0	16	0	0	0	0	0	8	0	24

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Flow Rates: Appr/Exit Totals

Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	0	0	16	0	16	0	0	16
715	0	8	0	4	0	4	0	8	12
730	0	0	0	28	0	28	0	0	28
745	0	24	0	16	0	16	4	20	40
800	0	16	0	16	0	16	0	16	32
815	0	8	4	8	0	8	4	8	20
830	0	24	0	32	0	32	0	24	56
845	0	16	0	16	0	16	0	16	32
1600	0	20	0	8	0	8	8	12	28
1615	0	8	8	16	0	20	0	12	32
1630	0	16	0	16	0	16	0	16	32
1645	0	8	4	12	0	16	0	8	24
1700	0	8	0	12	0	12	0	8	20
1715	0	20	0	8	0	8	4	16	28
1730	0	20	0	4	0	4	0	20	24
1745	0	16	0	8	0	8	0	16	24

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 60-Minute Volumes: by Movement

Intersection # 3 central/unitychurch

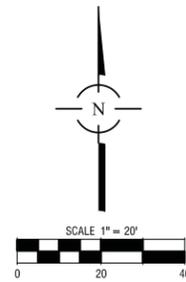
Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	7	1	0	0	0	0	16	0	24
715	0	0	0	0	11	1	0	0	0	0	16	0	28
730	0	0	0	0	11	1	1	0	0	1	16	0	30
745	0	0	0	0	17	1	1	0	0	1	17	0	37
800	0	0	0	0	16	0	1	0	0	1	17	0	35
815	0	0	0	0	12	0	1	0	0	1	13	0	27*
830	0	0	0	0	10	0	0	0	0	0	12	0	22*
845	0	0	0	0	4	0	0	0	0	0	4	0	8*
1600	0	0	0	0	11	2	2	0	1	0	13	0	29
1615	0	0	0	0	10	0	2	0	1	0	14	0	27
1630	0	0	0	0	12	1	1	0	0	0	12	0	26
1645	0	0	0	0	13	1	1	0	0	0	9	0	24
1700	0	0	0	0	15	1	0	0	0	0	8	0	24
1715	0	0	0	0	13	1	0	0	0	0	5	0	19*
1730	0	0	0	0	9	0	0	0	0	0	3	0	12*
1745	0	0	0	0	4	0	0	0	0	0	2	0	6*

URNS/TEAPAC[Ver 3.61.12] - 60-Minute Volumes: Appr/Exit Totals

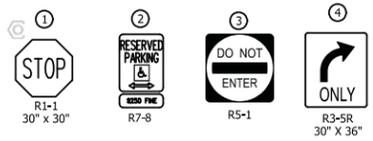
Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	8	0	16	0	16	1	7	24
715	0	12	0	16	0	16	1	11	28
730	0	12	1	17	0	17	2	11	30
745	0	18	1	18	0	18	2	17	37
800	0	16	1	18	0	18	1	16	35
815	0	12	1	14	0	14	1	12	27*
830	0	10	0	12	0	12	0	10	22*
845	0	4	0	4	0	4	0	4	8*
1600	0	13	3	13	0	15	2	12	29
1615	0	10	3	14	0	16	0	11	27
1630	0	13	1	12	0	13	1	12	26
1645	0	14	1	9	0	10	1	13	24
1700	0	16	0	8	0	8	1	15	24
1715	0	14	0	5	0	5	1	13	19*
1730	0	9	0	3	0	3	0	9	12*
1745	0	4	0	2	0	2	0	4	6*

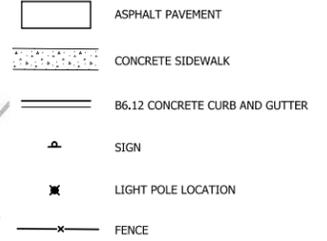
Site Plan



SIGN LEGEND:



LEGEND



CENTRAL STREET
BITUMINOUS PAVEMENT
80' PUBLIC RIGHT OF WAY

GROSS POINT ROAD
BITUMINOUS PAVEMENT
80' PUBLIC RIGHT OF WAY

**2-STORY
PRE-SCHOOL BUILDING**

EMILE A OKAL

EXCEPTION
MARGARET OKAL

FIRST WILLIAMSBURG CRP

S 89°30'15" E 408.33' (408.33')

N 00°11'30" E 190.93'

S 65°53'30" W 136.04'

N 49°32'06" W 131.10' (131.15')

NO.	DATE	REMARKS

NO.	DATE	REMARKS

PRELIMINARY GEOMETRIC PLAN
KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS

CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

9575 W. Higgins Road, Suite 700,
Rosemont, Illinois 60018
Phone: (847) 698-0960 Fax: (847) 698-0965



SPACECO INC.

FILENAME:
11641P-GM

DATE:
02/04/22

JOB NO.
11641

SHEET
P-GM
3 OF 5

CMAP Projections Letter



Chicago Metropolitan
Agency for Planning

433 West Van Buren Street
Suite 450
Chicago, IL 60607
312-454-0400
cmap.illinois.gov

January 13, 2022

Kelly Pachowicz
Consultant
Kenig, Lindgren, O'Hara and Aboona, Inc.
9575 West Higgins Road
Suite 400
Rosemont, IL 60018

Subject: Gross Point Road - Central Street - Crawford Avenue
IDOT

Dear Ms. Pachowicz:

In response to a request made on your behalf and dated January 12, 2022, we have developed year 2050 average daily traffic (ADT) projections for the subject location.

ROAD SEGMENT	Current ADT	Year 2050 ADT
Gross Point Rd fr Golf Rd to Lake Ave	11,100	12,400
Central St east of Crawford Ave	12,500	13,900
Crawford Ave south of Gross Point Rd	9,350	11,100
Crawford Ave south of Gross Point Rd	6,000	7,100

Traffic projections are developed using existing ADT data provided in the request letter and the results from the June 2021 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2050 socioeconomic projections and assumes the implementation of the ON TO 2050 Comprehensive Regional Plan for the Northeastern Illinois area. The provision of this data in support of your request does not constitute a CMAP endorsement of the proposed development or any subsequent developments.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

Jose Rodriguez, PTP, AICP
Senior Planner, Research & Analysis

cc: Rios (IDOT)
2022_ForecastTraffic\Evanston\ck-11-22\ck-11-22.docx

Level of Service Table

LEVEL OF SERVICE CRITERIA

Signalized Intersections		
Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤10
B	Good progression, with more vehicles stopping than for Level of Service A.	>10 - 20
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	>20 - 35
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	>35 - 55
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	>55 - 80
F	The volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue.	>80.0
Unsignalized Intersections		
Level of Service	Average Total Delay (SEC/VEH)	
A	0 - 10	
B	> 10 - 15	
C	> 15 - 25	
D	> 25 - 35	
E	> 35 - 50	
F	> 50	

Source: *Highway Capacity Manual*, 2010.

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2022 Base Conditions

Lanes, Volumes, Timings 3: Gross Point Road & Central Street

02/09/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕↔		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	5	12	5	319	5	0	0	240	356	0	214	7
Future Volume (vph)	5	12	5	319	5	0	0	240	356	0	214	7
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Frt		0.971							0.850		0.995	
Flt Protected		0.989		0.950	0.954							
Satd. Flow (prot)	0	1643	0	1681	1689	0	0	1863	1568	0	3558	0
Flt Permitted		0.989		0.950	0.954							
Satd. Flow (perm)	0	1643	0	1681	1689	0	0	1863	1568	0	3558	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5							383		4	
Link Speed (mph)		30		30				30			30	
Link Distance (ft)		363		221				310			255	
Travel Time (s)		8.3		5.0				7.0			5.8	
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Shared Lane Traffic (%)				49%								
Lane Group Flow (vph)	0	23	0	175	173	0	0	258	383	0	238	0
Turn Type	Split	NA		Split	NA			NA	Perm		NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2		6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0		5.0	
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0		24.0	
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0		54.0	
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%		49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0		4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0		2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effect Green (s)		6.8		69.9	69.9			22.5	22.5		22.5	
Actuated g/C Ratio		0.06		0.64	0.64			0.20	0.20		0.20	
v/c Ratio		0.22		0.16	0.16			0.68	0.61		0.33	
Control Delay		45.4		7.2	7.2			48.7	7.9		15.3	
Queue Delay		0.0		1.2	1.2			0.0	0.1		0.0	
Total Delay		45.4		8.5	8.4			48.7	8.0		15.3	
LOS		D		A	A			D	A		B	
Approach Delay		45.4			8.4			24.4			15.3	
Approach LOS		D			A			C			B	
Queue Length 50th (ft)		12		24	23			169	0		21	
Queue Length 95th (ft)		38		56	56			231	71		101	

Lanes, Volumes, Timings

3: Gross Point Road & Central Street

02/09/2022

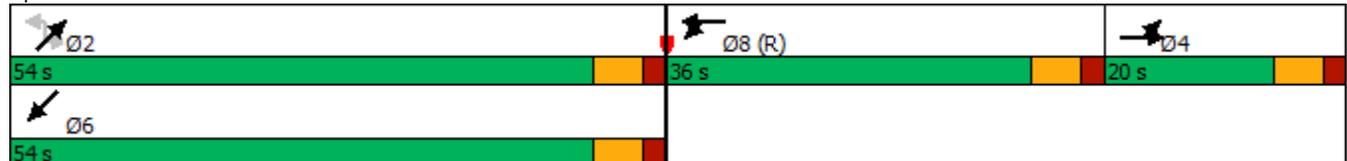


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		213		1067	1072			812	900		1554	
Starvation Cap Reductn		0		699	704			0	0		0	
Spillback Cap Reductn		0		0	0			0	83		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.11		0.48	0.47			0.32	0.47		0.15	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay:	18.6
Intersection LOS:	B
Intersection Capacity Utilization	38.3%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	10	290	32	0	263	139	24	222	0	112	188	13
Future Volume (vph)	10	290	32	0	263	139	24	222	0	112	188	13
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Frt		0.986				0.850						0.990
Flt Protected		0.998						0.995		0.950		
Satd. Flow (prot)	0	3228	0	0	3574	1599	0	3497	0	1770	1851	0
Flt Permitted		0.944						0.894		0.403		
Satd. Flow (perm)	0	3053	0	0	3574	1599	0	3142	0	751	1851	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		13				153						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%
Parking (#/hr)	0	0	0									
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	365	0	0	289	153	0	270	0	123	221	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		69.0			69.0	69.0		14.7		31.5	29.0	
Actuated g/C Ratio		0.63			0.63	0.63		0.13		0.29	0.26	
v/c Ratio		0.19			0.13	0.14		0.64		0.39	0.45	
Control Delay		9.3			4.1	0.6		31.6		32.5	35.3	
Queue Delay		0.0			0.3	0.6		0.0		0.0	0.0	
Total Delay		9.3			4.5	1.1		31.6		32.5	35.3	
LOS		A			A	A		C		C	D	
Approach Delay		9.3			3.3			31.6			34.3	
Approach LOS		A			A			C			C	
Queue Length 50th (ft)		52			11	0		20		66	127	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		84			22	0		79		107	187	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1918			2240	1059		999		331	860	
Starvation Cap Reductn		0			1441	617		0		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.19			0.36	0.35		0.27		0.37	0.26	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.64
Intersection Signal Delay:	17.7
Intersection LOS:	B
Intersection Capacity Utilization	48.9%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

Ø2 (R)	Ø3	Ø4
53 s	16 s	41 s
Ø6 (R)	Ø8	
53 s	57 s	

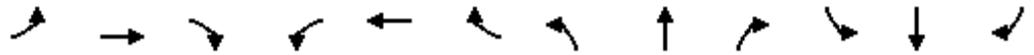
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	358	11	47	306	102	18	302	167	214	195	1
Future Volume (vph)	0	358	11	47	306	102	18	302	167	214	195	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.995			0.962			0.946				0.999
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3458	0	1752	3380	0	1805	3369	0	1770	3403	0
Flt Permitted				0.290			0.621			0.414		
Satd. Flow (perm)	0	3458	0	535	3380	0	1180	3369	0	771	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		3			50			89				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	393	0	50	435	0	19	499	0	228	208	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effect Green (s)		17.9		29.8	27.3		64.2	55.7		73.2	66.7	
Actuated g/C Ratio		0.16		0.27	0.25		0.58	0.51		0.67	0.61	
v/c Ratio		0.70		0.22	0.50		0.03	0.28		0.37	0.10	
Control Delay		52.9		28.8	31.9		9.2	15.1		9.7	9.5	
Queue Delay		0.7		0.0	0.0		0.0	0.0		0.4	0.4	
Total Delay		53.7		28.8	31.9		9.2	15.1		10.0	9.9	
LOS		D		C	C		A	B		B	A	
Approach Delay		53.7			31.6			14.9			10.0	
Approach LOS		D			C			B			A	
Queue Length 50th (ft)		139		26	120		4	85		66	28	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

02/09/2022

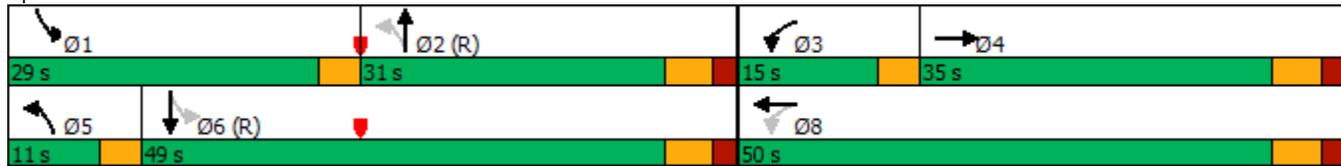


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		183		50	152		16	152		100	53	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		913		272	1382		747	1751		744	2062	
Starvation Cap Reductn		251		0	0		0	0		189	1415	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.59		0.18	0.31		0.03	0.28		0.41	0.32	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay:	26.5
Intersection LOS:	C
Intersection Capacity Utilization	56.6%
ICU Level of Service	B
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	591	0	4	534
Future Vol, veh/h	1	5	591	0	4	534
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	642	0	4	580

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	940	642	0	0	642	0
Stage 1	642	-	-	-	-	-
Stage 2	298	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	277	473	-	-	941	-
Stage 1	523	-	-	-	-	-
Stage 2	728	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	275	473	-	-	941	-
Mov Cap-2 Maneuver	275	-	-	-	-	-
Stage 1	523	-	-	-	-	-
Stage 2	724	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	13.7	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	422	941	-
HCM Lane V/C Ratio	-	-	0.015	0.005	-
HCM Control Delay (s)	-	-	13.7	8.8	0
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2022 Base Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

02/09/2022

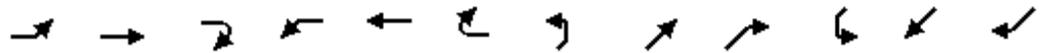


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕↔		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	2	6	1	410	14	6	0	247	254	2	302	5
Future Volume (vph)	2	6	1	410	14	6	0	247	254	2	302	5
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Frt		0.985			0.996				0.850		0.998	
Flt Protected		0.989		0.950	0.957							
Satd. Flow (prot)	0	1851	0	1681	1681	0	0	1863	1599	0	3546	0
Flt Permitted		0.989		0.950	0.957						0.953	
Satd. Flow (perm)	0	1851	0	1681	1681	0	0	1863	1599	0	3379	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			1				265			2
Link Speed (mph)		30			30				30			30
Link Distance (ft)		363			221				310			255
Travel Time (s)		8.3			5.0				7.0			5.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Shared Lane Traffic (%)				48%								
Lane Group Flow (vph)	0	9	0	222	226	0	0	257	265	0	322	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2	6		
Detector Phase	4	4		8	8		2	2	2	6		6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0		5.0
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0	24.0		24.0
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0	54.0		54.0
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%	49.1%		49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0		4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0		2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0			0.0
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0			6.0
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min		Min
Act Effect Green (s)		6.1		73.8	73.8			21.6	21.6			21.6
Actuated g/C Ratio		0.06		0.67	0.67			0.20	0.20			0.20
v/c Ratio		0.09		0.20	0.20			0.70	0.50			0.48
Control Delay		48.1		13.6	13.6			51.1	7.6			19.2
Queue Delay		0.0		1.6	1.6			0.0	0.0			0.0
Total Delay		48.1		15.2	15.2			51.1	7.6			19.2
LOS		D		B	B			D	A			B
Approach Delay		48.1			15.2			29.0				19.2
Approach LOS		D			B			C				B
Queue Length 50th (ft)		6		54	55			170	0			33
Queue Length 95th (ft)		22		128	129			236	62			128

Lanes, Volumes, Timings

3: Gross Point Road & Central Street

02/09/2022

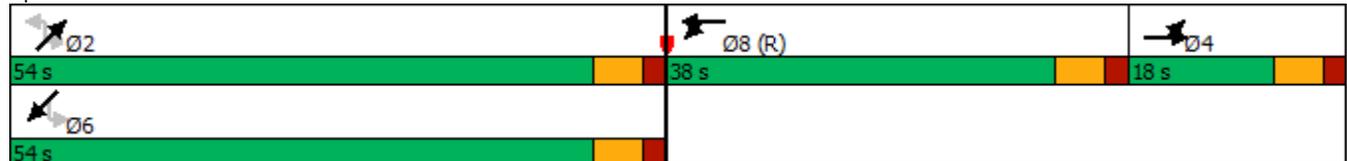


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		202		1127	1127			812	847		1475	
Starvation Cap Reductn		0		730	729			0	0		183	
Spillback Cap Reductn		0		0	0			0	13		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.04		0.56	0.57			0.32	0.32		0.25	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay:	22.0
Intersection LOS:	C
Intersection Capacity Utilization	43.5%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street



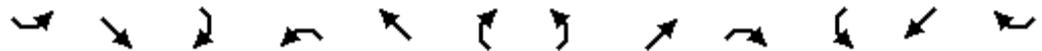
Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

													
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR	
Lane Configurations													
Traffic Volume (vph)	10	295	55	0	262	125	36	219	0	174	252	16	
Future Volume (vph)	10	295	55	0	262	125	36	219	0	174	252	16	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Storage Length (ft)	0		0	0		0	0		0	78		0	
Storage Lanes	0		0	0		1	0		0	1		0	
Taper Length (ft)	25			25			25			95			
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00	
Frt		0.977				0.850						0.991	
Flt Protected		0.999						0.993		0.950			
Satd. Flow (prot)	0	3283	0	0	3574	1599	0	3539	0	1787	1857	0	
Flt Permitted		0.944						0.846		0.392			
Satd. Flow (perm)	0	3102	0	0	3574	1599	0	3015	0	737	1857	0	
Right Turn on Red			Yes			Yes			Yes			Yes	
Satd. Flow (RTOR)		23				140						4	
Link Speed (mph)		30			30			30				30	
Link Distance (ft)		386			211			255				600	
Travel Time (s)		8.8			4.8			5.8				13.6	
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%	
Parking (#/hr)	0	0	0										
Shared Lane Traffic (%)													
Lane Group Flow (vph)	0	404	0	0	294	140	0	286	0	196	301	0	
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA		
Protected Phases		6			2			4		3	8		
Permitted Phases	6					2	4			8			
Detector Phase	6	6			2	2	4	4		3	8		
Switch Phase													
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0		
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0		
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0		
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%		
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0		
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0		
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0		
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0		
Lead/Lag							Lag	Lag		Lead			
Lead-Lag Optimize?							Yes	Yes		Yes			
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None		
Act Effct Green (s)		66.9			66.9	66.9		15.6		33.6	31.1		
Actuated g/C Ratio		0.61			0.61	0.61		0.14		0.31	0.28		
v/c Ratio		0.21			0.14	0.14		0.67		0.58	0.57		
Control Delay		10.0			9.7	2.3		24.8		36.2	37.2		
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0		
Total Delay		10.0			10.2	3.1		24.8		36.2	37.2		
LOS		A			B	A		C		D	D		
Approach Delay		10.0			7.9			24.8			36.8		
Approach LOS		A			A			C			D		
Queue Length 50th (ft)		59			27	2		15		107	178		

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		93			43	13		78		159	248	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1894			2172	1026		959		344	863	
Starvation Cap Reductn		0			1452	641		12		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.21			0.41	0.36		0.30		0.57	0.35	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.67
Intersection Signal Delay:	20.3
Intersection LOS:	C
Intersection Capacity Utilization:	53.6%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

Ø2 (R)	Ø3	Ø4
53 s	16 s	41 s
Ø6 (R)	Ø8	
53 s	57 s	

Lanes, Volumes, Timings

8: Crawford Avenue & Central Street

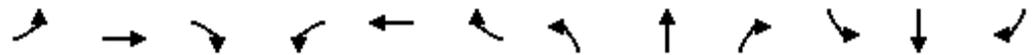
02/09/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑		↖	↑↑		↖	↑↑		↖	↑↑	
Traffic Volume (vph)	0	256	6	112	416	136	13	253	82	164	304	1
Future Volume (vph)	0	256	6	112	416	136	13	253	82	164	304	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.997			0.963			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3530	0	1787	3400	0	1805	3425	0	1787	3539	0
Flt Permitted				0.387			0.559			0.511		
Satd. Flow (perm)	0	3530	0	728	3400	0	1062	3425	0	961	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			48			38				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	273	0	117	575	0	14	349	0	171	318	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		13.8		30.0	27.5		65.7	57.4		73.0	66.6	
Actuated g/C Ratio		0.13		0.27	0.25		0.60	0.52		0.66	0.61	
v/c Ratio		0.62		0.39	0.65		0.02	0.19		0.24	0.15	
Control Delay		53.5		34.0	36.9		8.0	13.8		7.2	9.3	
Queue Delay		0.1		0.0	0.0		0.0	0.0		0.6	0.6	
Total Delay		53.6		34.0	36.9		8.0	13.8		7.8	10.0	
LOS		D		C	D		A	B		A	A	
Approach Delay		53.6			36.4			13.6			9.2	
Approach LOS		D			D			B			A	
Queue Length 50th (ft)		94		64	173		3	59		54	54	

Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

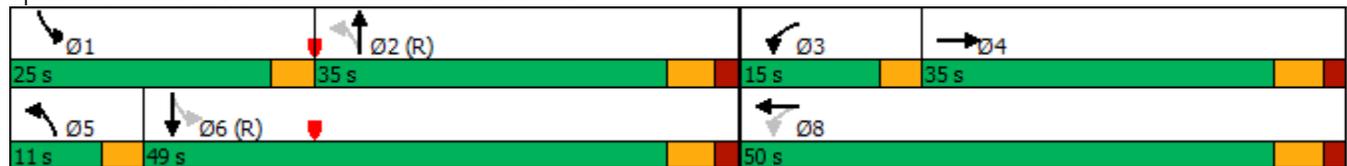


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		138		106	222		11	100		82	82	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		932		309	1388		701	1804		798	2142	
Starvation Cap Reductn		139		0	0		0	0		354	1460	
Spillback Cap Reductn		0		0	0		11	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.34		0.38	0.41		0.02	0.19		0.39	0.47	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.65
Intersection Signal Delay:	27.1
Intersection LOS:	C
Intersection Capacity Utilization	48.8%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	0	1	500	0	5	708
Future Vol, veh/h	0	1	500	0	5	708
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	543	0	5	770

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	938	543	0	0	543	0
Stage 1	543	-	-	-	-	-
Stage 2	395	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	278	539	-	-	1024	-
Stage 1	581	-	-	-	-	-
Stage 2	650	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	275	539	-	-	1024	-
Mov Cap-2 Maneuver	275	-	-	-	-	-
Stage 1	581	-	-	-	-	-
Stage 2	644	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	11.7	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NERNWLn1	SWL	SWT
Capacity (veh/h)	-	-	539	1024
HCM Lane V/C Ratio	-	-	0.002	0.005
HCM Control Delay (s)	-	-	11.7	8.5
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2028 No-Build
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

02/09/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕↔		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	5	12	5	329	5	1	0	248	367	0	220	7
Future Volume (vph)	5	12	5	329	5	1	0	248	367	0	220	7
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Frt		0.971			0.999				0.850		0.995	
Flt Protected		0.989		0.950	0.954							
Satd. Flow (prot)	0	1643	0	1681	1688	0	0	1863	1568	0	3558	0
Flt Permitted		0.989		0.950	0.954							
Satd. Flow (perm)	0	1643	0	1681	1688	0	0	1863	1568	0	3558	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5							395			4
Link Speed (mph)		30			30				30			30
Link Distance (ft)		363			221				310			255
Travel Time (s)		8.3			5.0				7.0			5.8
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Shared Lane Traffic (%)				49%								
Lane Group Flow (vph)	0	23	0	181	179	0	0	267	395	0	245	0
Turn Type	Split	NA		Split	NA			NA	Perm		NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2		6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0		5.0	
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0		24.0	
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0		54.0	
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%		49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0		4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0		2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effect Green (s)		6.8		69.3	69.3			23.0	23.0		23.0	
Actuated g/C Ratio		0.06		0.63	0.63			0.21	0.21		0.21	
v/c Ratio		0.22		0.17	0.17			0.69	0.62		0.33	
Control Delay		45.4		7.8	7.7			48.6	7.8		15.2	
Queue Delay		0.0		1.3	1.2			0.0	0.2		0.0	
Total Delay		45.4		9.0	9.0			48.6	8.0		15.2	
LOS		D		A	A			D	A		B	
Approach Delay		45.4			9.0			24.4			15.2	
Approach LOS		D			A			C			B	
Queue Length 50th (ft)		12		25	25			175	0		21	
Queue Length 95th (ft)		38		63	63			238	72		102	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

02/09/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		213		1059	1064			812	906		1554	
Starvation Cap Reductn		0		687	693			0	0		0	
Spillback Cap Reductn		0		0	0			0	100		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.11		0.49	0.48			0.33	0.49		0.16	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.69
Intersection Signal Delay:	18.7
Intersection LOS:	B
Intersection Capacity Utilization	39.0%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street

Ø2	Ø8 (R)	Ø4
54 s	36 s	20 s
Ø6		
54 s		

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	10	307	33	0	271	143	25	229	0	115	193	14
Future Volume (vph)	10	307	33	0	271	143	25	229	0	115	193	14
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Frt		0.986				0.850						0.990
Flt Protected		0.999						0.995		0.950		
Satd. Flow (prot)	0	3231	0	0	3574	1599	0	3497	0	1770	1850	0
Flt Permitted		0.944						0.893		0.394		
Satd. Flow (perm)	0	3053	0	0	3574	1599	0	3139	0	734	1850	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		12				157						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%
Parking (#/hr)	0	0	0									
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	384	0	0	298	157	0	279	0	126	227	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		68.6			68.6	68.6		15.0		31.9	29.4	
Actuated g/C Ratio		0.62			0.62	0.62		0.14		0.29	0.27	
v/c Ratio		0.20			0.13	0.15		0.65		0.40	0.46	
Control Delay		9.6			4.2	0.6		31.3		32.5	35.2	
Queue Delay		0.0			0.3	0.6		0.0		0.0	0.0	
Total Delay		9.6			4.5	1.1		31.3		32.5	35.2	
LOS		A			A	A		C		C	D	
Approach Delay		9.6			3.3			31.3			34.2	
Approach LOS		A			A			C			C	
Queue Length 50th (ft)		56			11	0		22		67	130	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		90			23	0		80		109	191	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1907			2227	1055		998		330	859	
Starvation Cap Reductn		0			1415	606		12		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.20			0.37	0.35		0.28		0.38	0.26	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.65
Intersection Signal Delay:	17.7
Intersection LOS:	B
Intersection Capacity Utilization	49.9%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

53 s	16 s	41 s
53 s	57 s	

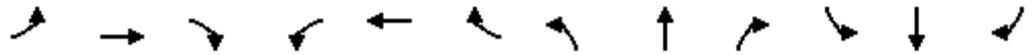
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	369	11	48	315	105	19	311	172	220	201	1
Future Volume (vph)	0	369	11	48	315	105	19	311	172	220	201	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.996			0.962			0.947			0.999	
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3461	0	1752	3380	0	1805	3373	0	1770	3403	0
Flt Permitted				0.281			0.617			0.404		
Satd. Flow (perm)	0	3461	0	518	3380	0	1172	3373	0	753	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		3			50			88				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	405	0	51	447	0	20	514	0	234	215	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effect Green (s)		18.2		30.3	27.8		63.6	55.1		72.7	66.2	
Actuated g/C Ratio		0.17		0.28	0.25		0.58	0.50		0.66	0.60	
v/c Ratio		0.70		0.22	0.50		0.03	0.30		0.39	0.10	
Control Delay		52.6		28.6	31.9		9.4	15.7		10.1	9.6	
Queue Delay		0.9		0.0	0.0		0.0	0.0		0.3	0.4	
Total Delay		53.5		28.6	31.9		9.4	15.7		10.5	10.0	
LOS		D		C	C		A	B		B	B	
Approach Delay		53.5			31.5			15.5			10.3	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)		143		26	123		5	90		67	31	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

02/09/2022

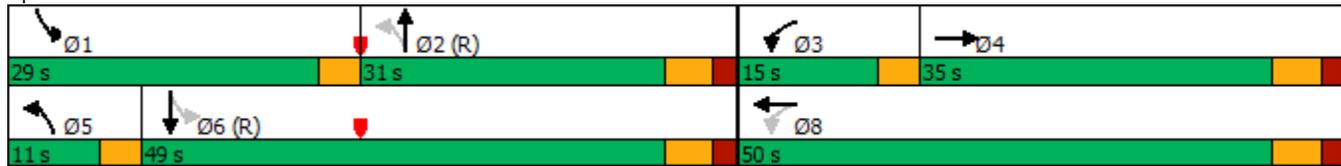


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		187		51	155		16	160		101	54	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		914		271	1382		736	1733		733	2048	
Starvation Cap Reductn		261		0	0		0	0		175	1393	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.62		0.19	0.32		0.03	0.30		0.42	0.33	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay:	26.6
Intersection LOS:	C
Intersection Capacity Utilization	57.7%
ICU Level of Service	B
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	610	0	4	550
Future Vol, veh/h	1	5	610	0	4	550
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	663	0	4	598

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	970	663	0	0	663	0
Stage 1	663	-	-	-	-	-
Stage 2	307	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	265	460	-	-	924	-
Stage 1	511	-	-	-	-	-
Stage 2	720	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	263	460	-	-	924	-
Mov Cap-2 Maneuver	263	-	-	-	-	-
Stage 1	511	-	-	-	-	-
Stage 2	716	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	13.9	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NERNWLn1	SWL	SWT
Capacity (veh/h)	-	-	409	924
HCM Lane V/C Ratio	-	-	0.016	0.005
HCM Control Delay (s)	-	-	13.9	8.9
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2028 No-Build
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

02/09/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	2	6	1	423	15	6	0	255	262	2	311	5
Future Volume (vph)	2	6	1	423	15	6	0	255	262	2	311	5
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Frt		0.985			0.996				0.850		0.998	
Flt Protected		0.989		0.950	0.957							
Satd. Flow (prot)	0	1851	0	1681	1681	0	0	1863	1599	0	3547	0
Flt Permitted		0.989		0.950	0.957						0.953	
Satd. Flow (perm)	0	1851	0	1681	1681	0	0	1863	1599	0	3380	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			1				273			2
Link Speed (mph)		30			30				30			30
Link Distance (ft)		363			221				310			255
Travel Time (s)		8.3			5.0				7.0			5.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Shared Lane Traffic (%)				48%								
Lane Group Flow (vph)	0	9	0	229	234	0	0	266	273	0	331	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2	6		
Detector Phase	4	4		8	8		2	2	2	6		6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0		5.0
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0	24.0		24.0
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0	54.0		54.0
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%	49.1%		49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0		4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0		2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0			0.0
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0			6.0
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min		Min
Act Effct Green (s)		6.1		72.9	72.9			22.4	22.4			22.4
Actuated g/C Ratio		0.06		0.66	0.66			0.20	0.20			0.20
v/c Ratio		0.09		0.21	0.21			0.70	0.50			0.48
Control Delay		48.1		14.7	14.6			49.9	7.3			18.5
Queue Delay		0.0		1.7	1.8			0.0	0.0			0.0
Total Delay		48.1		16.4	16.4			49.9	7.3			18.5
LOS		D		B	B			D	A			B
Approach Delay		48.1			16.4			28.3				18.5
Approach LOS		D			B			C				B
Queue Length 50th (ft)		6		60	61			176	0			34
Queue Length 95th (ft)		22		135	137			239	61			126

Lanes, Volumes, Timings

3: Gross Point Road & Central Street

02/09/2022

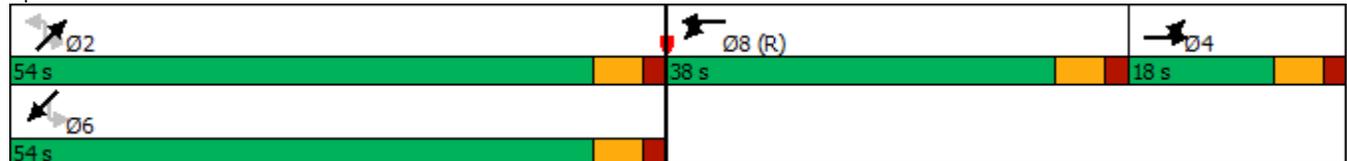


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		202		1114	1114			812	851		1476	
Starvation Cap Reductn		0		718	715			0	0		198	
Spillback Cap Reductn		0		0	0			0	16		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.04		0.58	0.59			0.33	0.33		0.26	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay:	21.9
Intersection LOS:	C
Intersection Capacity Utilization	44.2%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		 			 			 		 		
Traffic Volume (vph)	10	304	56	0	270	129	37	226	0	179	260	16
Future Volume (vph)	10	304	56	0	270	129	37	226	0	179	260	16
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Frt		0.977				0.850						0.991
Flt Protected		0.999						0.993		0.950		
Satd. Flow (prot)	0	3283	0	0	3574	1599	0	3539	0	1787	1857	0
Flt Permitted		0.944						0.843		0.383		
Satd. Flow (perm)	0	3102	0	0	3574	1599	0	3005	0	720	1857	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		23				145						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%
Parking (#/hr)	0	0	0									
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	416	0	0	303	145	0	296	0	201	310	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		66.4			66.4	66.4		16.0		34.1	31.6	
Actuated g/C Ratio		0.60			0.60	0.60		0.15		0.31	0.29	
v/c Ratio		0.22			0.14	0.14		0.68		0.59	0.58	
Control Delay		10.3			9.9	2.3		24.8		36.4	37.2	
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0	
Total Delay		10.3			10.4	3.1		24.8		36.4	37.2	
LOS		B			B	A		C		D	D	
Approach Delay		10.3			8.0			24.8			36.9	
Approach LOS		B			A			C			D	
Queue Length 50th (ft)		62			28	3		16		109	184	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		96			45	14		80		161	255	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1882			2158	1022		956		344	863	
Starvation Cap Reductn		0			1430	632		13		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.22			0.42	0.37		0.31		0.58	0.36	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay:	20.4
Intersection LOS:	C
Intersection Capacity Utilization	54.5%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

Ø2 (R)	Ø3	Ø4
53 s	16 s	41 s
Ø6 (R)	Ø8	
53 s	57 s	

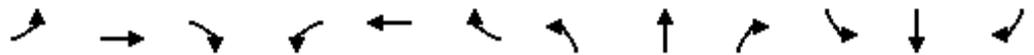
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	264	6	115	429	140	14	261	84	169	313	1
Future Volume (vph)	0	264	6	115	429	140	14	261	84	169	313	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.997			0.963			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3530	0	1787	3400	0	1805	3425	0	1787	3539	0
Flt Permitted				0.378			0.554			0.503		
Satd. Flow (perm)	0	3530	0	711	3400	0	1053	3425	0	946	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			48			39				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	281	0	120	593	0	15	360	0	176	327	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effect Green (s)		14.0		30.3	27.8		65.2	56.9		72.7	66.2	
Actuated g/C Ratio		0.13		0.28	0.25		0.59	0.52		0.66	0.60	
v/c Ratio		0.62		0.41	0.66		0.02	0.20		0.25	0.15	
Control Delay		53.1		34.0	37.1		8.1	14.1		7.4	9.4	
Queue Delay		0.1		0.0	0.0		0.0	0.0		0.6	0.7	
Total Delay		53.3		34.0	37.1		8.1	14.1		7.9	10.1	
LOS		D		C	D		A	B		A	B	
Approach Delay		53.3			36.6			13.9			9.3	
Approach LOS		D			D			B			A	
Queue Length 50th (ft)		97		65	180		3	62		55	55	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

02/09/2022

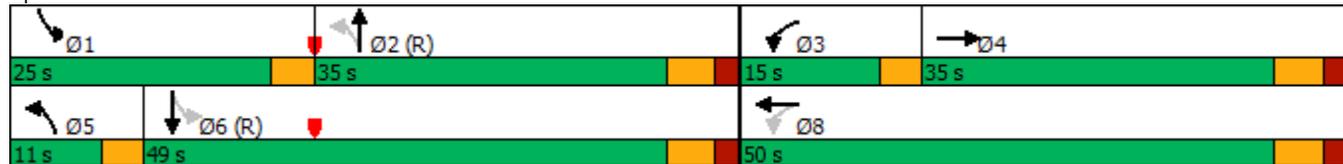


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		142		108	228		12	104		84	83	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		932		308	1388		691	1789		789	2130	
Starvation Cap Reductn		143		0	0		0	0		340	1444	
Spillback Cap Reductn		0		0	24		54	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.36		0.39	0.43		0.02	0.20		0.39	0.48	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.66
Intersection Signal Delay:	27.2
Intersection LOS:	C
Intersection Capacity Utilization	49.8%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	0	1	516	0	5	730
Future Vol, veh/h	0	1	516	0	5	730
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	561	0	5	793

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	968	561	0	0	561	0
Stage 1	561	-	-	-	-	-
Stage 2	407	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	266	526	-	-	1008	-
Stage 1	570	-	-	-	-	-
Stage 2	641	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	264	526	-	-	1008	-
Mov Cap-2 Maneuver	264	-	-	-	-	-
Stage 1	570	-	-	-	-	-
Stage 2	635	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	11.9	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	526	1008	-
HCM Lane V/C Ratio	-	-	0.002	0.005	-
HCM Control Delay (s)	-	-	11.9	8.6	0
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2028 Projected
Conditions

Lanes, Volumes, Timings 3: Gross Point Road & Central Street

02/09/2022

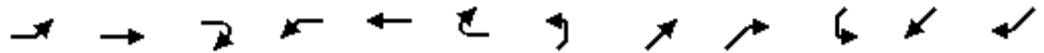


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕↔		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	19	31	8	352	5	0	0	252	371	0	238	7
Future Volume (vph)	19	31	8	352	5	0	0	252	371	0	238	7
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Frt		0.980							0.850		0.995	
Flt Protected		0.984		0.950	0.954							
Satd. Flow (prot)	0	1682	0	1681	1689	0	0	1863	1568	0	3557	0
Flt Permitted		0.984		0.950	0.954							
Satd. Flow (perm)	0	1682	0	1681	1689	0	0	1863	1568	0	3557	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		6							399		4	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		363			221			310			255	
Travel Time (s)		8.3			5.0			7.0			5.8	
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Shared Lane Traffic (%)				49%								
Lane Group Flow (vph)	0	62	0	193	190	0	0	271	399	0	264	0
Turn Type	Split	NA		Split	NA			NA	Perm		NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2		6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0		5.0	
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0		24.0	
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0		54.0	
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%		49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0		4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0		2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effect Green (s)		9.1		61.8	61.8			23.4	23.4		23.4	
Actuated g/C Ratio		0.08		0.56	0.56			0.21	0.21		0.21	
v/c Ratio		0.43		0.20	0.20			0.68	0.62		0.35	
Control Delay		52.0		10.1	10.1			48.0	7.6		18.3	
Queue Delay		0.0		0.8	0.8			0.0	0.2		0.0	
Total Delay		52.0		10.9	10.9			48.0	7.9		18.3	
LOS		D		B	B			D	A		B	
Approach Delay		52.0			10.9			24.1			18.3	
Approach LOS		D			B			C			B	
Queue Length 50th (ft)		38		32	31			177	0		90	
Queue Length 95th (ft)		79		74	73			240	72		122	

Lanes, Volumes, Timings

3: Gross Point Road & Central Street

02/09/2022

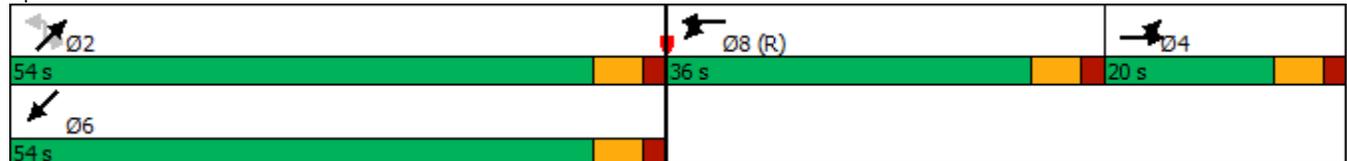


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		219		945	949			812	909		1554	
Starvation Cap Reductn		0		510	516			0	0		0	
Spillback Cap Reductn		1		0	0			0	125		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.28		0.44	0.44			0.33	0.51		0.17	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay:	20.6
Intersection LOS:	C
Intersection Capacity Utilization	39.8%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street



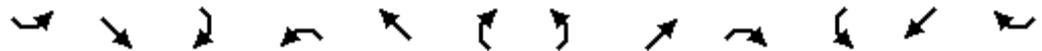
Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

													
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR	
Lane Configurations													
Traffic Volume (vph)	10	307	42	0	271	143	34	238	0	115	202	14	
Future Volume (vph)	10	307	42	0	271	143	34	238	0	115	202	14	
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Storage Length (ft)	0		0	0		0	0		0	78		0	
Storage Lanes	0		0	0		1	0		0	1		0	
Taper Length (ft)	25			25			25			95			
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00	
Frt		0.982				0.850						0.991	
Flt Protected		0.999						0.994		0.950			
Satd. Flow (prot)	0	3221	0	0	3574	1599	0	3496	0	1770	1853	0	
Flt Permitted		0.944						0.869		0.378			
Satd. Flow (perm)	0	3044	0	0	3574	1599	0	3057	0	704	1853	0	
Right Turn on Red			Yes			Yes			Yes			Yes	
Satd. Flow (RTOR)		16				157						4	
Link Speed (mph)		30			30			30				30	
Link Distance (ft)		386			211			255				600	
Travel Time (s)		8.8			4.8			5.8				13.6	
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%	
Parking (#/hr)	0	0	0										
Shared Lane Traffic (%)													
Lane Group Flow (vph)	0	394	0	0	298	157	0	299	0	126	237	0	
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA		
Protected Phases		6			2			4		3	8		
Permitted Phases	6					2	4			8			
Detector Phase	6	6			2	2	4	4		3	8		
Switch Phase													
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0		
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0		
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0		
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%		
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0		
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0		
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0		
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0		
Lead/Lag							Lag	Lag		Lead			
Lead-Lag Optimize?							Yes	Yes		Yes			
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None		
Act Effct Green (s)		67.7			67.7	67.7		15.9		32.8	30.3		
Actuated g/C Ratio		0.62			0.62	0.62		0.14		0.30	0.28		
v/c Ratio		0.21			0.14	0.15		0.68		0.40	0.46		
Control Delay		10.0			4.4	0.6		52.0		31.8	34.7		
Queue Delay		0.0			0.4	0.6		0.0		0.0	0.0		
Total Delay		10.0			4.8	1.1		52.0		31.8	34.7		
LOS		A			A	A		D		C	C		
Approach Delay		10.0			3.5			52.0			33.7		
Approach LOS		A			A			D			C		
Queue Length 50th (ft)		58			12	0		70		66	135		

Lanes, Volumes, Timings
 6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		94			24	0		107		107	196	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1879			2199	1044		972		330	861	
Starvation Cap Reductn		0			1389	595		15		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.21			0.37	0.35		0.31		0.38	0.28	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay:	22.0
Intersection LOS:	C
Intersection Capacity Utilization	51.2%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

Ø2 (R)	Ø3	Ø4
53 s	16 s	41 s
Ø6 (R)	Ø8	
53 s	57 s	

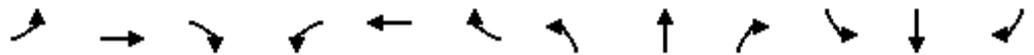
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑		↵	↑↑		↵	↑↑		↵	↑↑	
Traffic Volume (vph)	0	384	19	48	330	105	27	311	172	220	201	1
Future Volume (vph)	0	384	19	48	330	105	27	311	172	220	201	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.993			0.964			0.947				0.999
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3453	0	1752	3388	0	1805	3373	0	1770	3403	0
Flt Permitted				0.268			0.617			0.401		
Satd. Flow (perm)	0	3453	0	494	3388	0	1172	3373	0	747	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		4			46			88				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	429	0	51	463	0	29	514	0	234	215	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effect Green (s)		19.1		31.1	28.6		62.8	54.1		71.9	63.3	
Actuated g/C Ratio		0.17		0.28	0.26		0.57	0.49		0.65	0.58	
v/c Ratio		0.71		0.22	0.51		0.04	0.30		0.39	0.11	
Control Delay		51.8		28.0	31.9		9.8	16.3		10.5	10.9	
Queue Delay		1.2		0.0	0.0		0.0	0.0		0.3	0.2	
Total Delay		53.0		28.0	31.9		9.8	16.3		10.9	11.1	
LOS		D		C	C		A	B		B	B	
Approach Delay		53.0			31.5			15.9			11.0	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)		152		26	129		7	91		67	35	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

02/09/2022

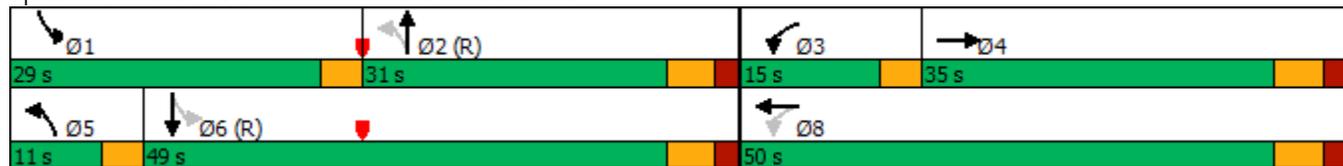


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		187		50	161		22	162		101	54	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		913		271	1382		726	1703		725	1958	
Starvation Cap Reductn		276		0	0		0	0		169	1188	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.67		0.19	0.34		0.04	0.30		0.42	0.28	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.71
Intersection Signal Delay:	27.1
Intersection LOS:	C
Intersection Capacity Utilization	58.3%
ICU Level of Service	B
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	618	0	4	594
Future Vol, veh/h	1	5	618	0	4	594
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	672	0	4	646

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1003	672	0	0	672	0
Stage 1	672	-	-	-	-	-
Stage 2	331	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	253	455	-	-	917	-
Stage 1	506	-	-	-	-	-
Stage 2	701	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	251	455	-	-	917	-
Mov Cap-2 Maneuver	251	-	-	-	-	-
Stage 1	506	-	-	-	-	-
Stage 2	696	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	14.1	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	401	917	-
HCM Lane V/C Ratio	-	-	0.016	0.005	-
HCM Control Delay (s)	-	-	14.1	8.9	0
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0	-

HCM 6th TWSC
12: Righ-Out Drive & Central Street

02/08/2022

Intersection						
Int Delay, s/veh	4.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↑		↑
Traffic Vol, veh/h	21	0	0	12	0	37
Future Vol, veh/h	21	0	0	12	0	37
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	23	0	0	13	0	40

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	6.22
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	3.318
Pot Cap-1 Maneuver	-	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1054
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.6
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	1054	-	-
HCM Lane V/C Ratio	0.038	-	-
HCM Control Delay (s)	8.6	-	-
HCM Lane LOS	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-

HCM 6th TWSC
 14: Gross Point Road & Access Drive

02/08/2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations	↔			↔ ↗		
Traffic Vol, veh/h	8	15	18	615	553	42
Future Vol, veh/h	8	15	18	615	553	42
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	16	20	668	601	46

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1332	324	647	0	-	0
Stage 1	624	-	-	-	-	-
Stage 2	708	-	-	-	-	-
Critical Hdwy	6.63	6.93	4.13	-	-	-
Critical Hdwy Stg 1	5.83	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	2.219	-	-	-
Pot Cap-1 Maneuver	157	672	937	-	-	-
Stage 1	497	-	-	-	-	-
Stage 2	487	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	152	672	937	-	-	-
Mov Cap-2 Maneuver	152	-	-	-	-	-
Stage 1	480	-	-	-	-	-
Stage 2	487	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s	17.8	0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NEL	NET	SELn1	SWT	SWR
Capacity (veh/h)	937	-	307	-	-
HCM Lane V/C Ratio	0.021	-	0.081	-	-
HCM Control Delay (s)	8.9	0	17.8	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0.1	-	0.3	-	-

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2028 Projected
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

02/09/2022

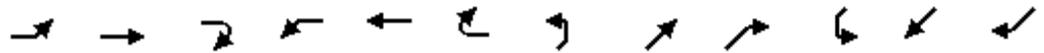


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	18	27	5	446	15	6	0	259	266	2	328	5
Future Volume (vph)	18	27	5	446	15	6	0	259	266	2	328	5
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Frt		0.987			0.996				0.850		0.998	
Flt Protected		0.982		0.950	0.956							
Satd. Flow (prot)	0	1842	0	1681	1679	0	0	1863	1599	0	3548	0
Flt Permitted		0.982		0.950	0.956						0.953	
Satd. Flow (perm)	0	1842	0	1681	1679	0	0	1863	1599	0	3381	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		4			1				277			2
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Shared Lane Traffic (%)				48%								
Lane Group Flow (vph)	0	52	0	242	245	0	0	270	277	0	349	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2	6		
Detector Phase	4	4		8	8		2	2	2	6		6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0		5.0
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0	24.0		24.0
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0	54.0		54.0
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%	49.1%		49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0		4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0		2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0			0.0
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0			6.0
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min		Min
Act Effct Green (s)		8.3		63.3	63.3			22.7	22.7			22.7
Actuated g/C Ratio		0.08		0.58	0.58			0.21	0.21			0.21
v/c Ratio		0.37		0.25	0.25			0.70	0.50			0.50
Control Delay		51.8		19.3	19.2			49.8	7.2			22.1
Queue Delay		0.0		1.5	1.6			0.0	0.0			0.1
Total Delay		51.8		20.8	20.8			49.8	7.2			22.2
LOS		D		C	C			D	A			C
Approach Delay		51.8			20.8			28.2				22.2
Approach LOS		D			C			C				C
Queue Length 50th (ft)		33		70	71			179	0			123
Queue Length 95th (ft)		71		147	149			243	62			161

Lanes, Volumes, Timings

3: Gross Point Road & Central Street

02/09/2022

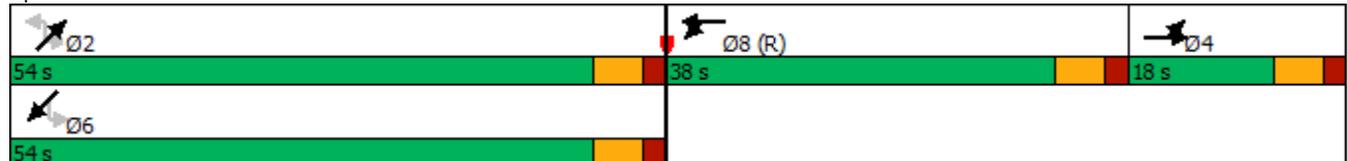


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		204		967	966			812	853		1476	
Starvation Cap Reductn		0		547	544			0	0		244	
Spillback Cap Reductn		0		0	0			0	15		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.25		0.58	0.58			0.33	0.33		0.28	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay:	25.1
Intersection LOS:	C
Intersection Capacity Utilization	44.9%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 3: Gross Point Road & Central Street



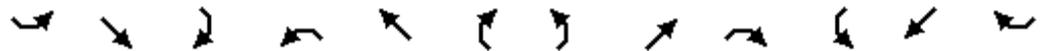
Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	10	304	64	0	270	129	47	236	0	179	269	16
Future Volume (vph)	10	304	64	0	270	129	47	236	0	179	269	16
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Frt		0.975				0.850						0.992
Flt Protected		0.999						0.992		0.950		
Satd. Flow (prot)	0	3277	0	0	3574	1599	0	3534	0	1787	1859	0
Flt Permitted		0.945						0.824		0.370		
Satd. Flow (perm)	0	3100	0	0	3574	1599	0	2936	0	696	1859	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		27				145						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%
Parking (#/hr)	0	0	0									
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	425	0	0	303	145	0	318	0	201	320	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		65.3			65.3	65.3		17.2		35.2	32.7	
Actuated g/C Ratio		0.59			0.59	0.59		0.16		0.32	0.30	
v/c Ratio		0.23			0.14	0.14		0.69		0.59	0.58	
Control Delay		10.8			10.5	2.4		47.5		35.3	36.2	
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0	
Total Delay		10.8			10.9	3.1		47.5		35.3	36.2	
LOS		B			B	A		D		D	D	
Approach Delay		10.8			8.4			47.5			35.9	
Approach LOS		B			A			D			D	
Queue Length 50th (ft)		65			29	3		70		107	188	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

02/09/2022



Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Queue Length 95th (ft)		101			45	14		108		157	257	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1850			2121	1008		934		346	864	
Starvation Cap Reductn		0			1395	616		16		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.23			0.42	0.37		0.35		0.58	0.37	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.69
Intersection Signal Delay:	24.6
Intersection LOS:	C
Intersection Capacity Utilization	55.8%
ICU Level of Service	B
Analysis Period (min)	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue

Ø2 (R)	Ø3	Ø4
53 s	16 s	41 s
Ø6 (R)	Ø8	
53 s	57 s	

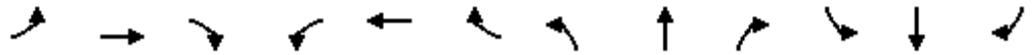
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

02/09/2022

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	280	15	115	444	140	22	261	84	169	313	1
Future Volume (vph)	0	280	15	115	444	140	22	261	84	169	313	1
Ideal Flow (vphp)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.992			0.964			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3514	0	1787	3404	0	1805	3425	0	1787	3539	0
Flt Permitted				0.351			0.554			0.502		
Satd. Flow (perm)	0	3514	0	660	3404	0	1053	3425	0	944	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5			46			39				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Parking (#/hr)						0						
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	308	0	120	609	0	23	360	0	176	327	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effect Green (s)		14.8		31.1	28.6		64.6	56.0		71.9	65.4	
Actuated g/C Ratio		0.13		0.28	0.26		0.59	0.51		0.65	0.59	
v/c Ratio		0.65		0.41	0.66		0.03	0.20		0.25	0.16	
Control Delay		53.2		33.6	36.8		8.4	14.6		7.7	9.9	
Queue Delay		0.2		0.0	0.0		0.0	0.0		0.6	0.7	
Total Delay		53.4		33.6	36.8		8.4	14.6		8.3	10.5	
LOS		D		C	D		A	B		A	B	
Approach Delay		53.4			36.3			14.2			9.8	
Approach LOS		D			D			B			A	
Queue Length 50th (ft)		103		64	185		5	63		55	55	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

02/09/2022

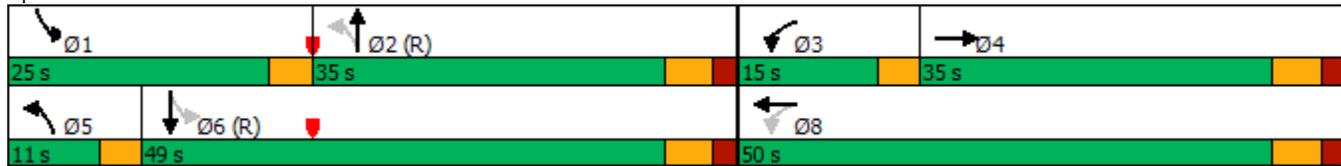


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 95th (ft)		141		106	233		16	106		85	84	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		930		304	1389		683	1763		781	2102	
Starvation Cap Reductn		162		0	0		0	0		333	1415	
Spillback Cap Reductn		0		0	59		63	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.40		0.39	0.46		0.04	0.20		0.39	0.48	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.66
Intersection Signal Delay:	27.7
Intersection LOS:	C
Intersection Capacity Utilization	50.5%
ICU Level of Service	A
Analysis Period (min)	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 6th TWSC
 10: Gross Point Road & Wellington Court

02/08/2022

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	0	1	524	0	5	779
Future Vol, veh/h	0	1	524	0	5	779
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	570	0	5	847

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1004	570	0	0	570	0
Stage 1	570	-	-	-	-	-
Stage 2	434	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	253	520	-	-	1000	-
Stage 1	565	-	-	-	-	-
Stage 2	622	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	251	520	-	-	1000	-
Mov Cap-2 Maneuver	251	-	-	-	-	-
Stage 1	565	-	-	-	-	-
Stage 2	616	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s	11.9	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NERNWLn1	SWL	SWT
Capacity (veh/h)	-	-	520	1000
HCM Lane V/C Ratio	-	-	0.002	0.005
HCM Control Delay (s)	-	-	11.9	8.6
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

HCM 6th TWSC
12: Righ-Out Drive & Central Street

02/08/2022

Intersection						
Int Delay, s/veh	5.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↑		↑
Traffic Vol, veh/h	8	0	0	20	0	42
Future Vol, veh/h	8	0	0	20	0	42
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	0	0	22	0	46

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	-	-	-	9
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	-	1073
Stage 1	-	0	0	-	-
Stage 2	-	0	0	-	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	1073
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	8.5
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	WBT
Capacity (veh/h)	1073	-	-
HCM Lane V/C Ratio	0.043	-	-
HCM Control Delay (s)	8.5	-	-
HCM Lane LOS	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-

HCM 6th TWSC
 14: Gross Point Road & Access Drive

02/08/2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations	↔			↔	↔	
Traffic Vol, veh/h	8	16	18	516	739	40
Future Vol, veh/h	8	16	18	516	739	40
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	2	2	0
Mvmt Flow	9	17	20	561	803	43

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1426	423	846	0	-	0
Stage 1	825	-	-	-	-	-
Stage 2	601	-	-	-	-	-
Critical Hdwy	6.6	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	139	585	800	-	-	-
Stage 1	396	-	-	-	-	-
Stage 2	551	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	134	585	800	-	-	-
Mov Cap-2 Maneuver	134	-	-	-	-	-
Stage 1	382	-	-	-	-	-
Stage 2	551	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s	19.4	0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NEL	NET	SELn1	SWT	SWR
Capacity (veh/h)	800	-	276	-	-
HCM Lane V/C Ratio	0.024	-	0.095	-	-
HCM Control Delay (s)	9.6	0	19.4	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0.1	-	0.3	-	-

**DESIGN AND PROJECT REVIEW COMMITTEE (DAPR) MINUTES
April 12, 2022**

Voting Members Present: J. Nyden, A. Schnur, L. Biggs, M. Callahan, I. Eckersberg, R. Papa, M. Tristan, C. Pratt, M. Jones, K. Asbaugh.

Staff Present: M. Turenne Jr., M. Griffith

Others Present:

Presiding Member: J. Nyden

A quorum being present, J. Nyden called the meeting to order at 2:05 p.m.

Approval of Minutes:

1. March 29, 2022, meeting minutes.

L. Biggs made a motion to approve the March 29, 2022, meeting minutes, seconded by A. Schnur.

The Committee voted by voice vote, 10-0, to approve the meeting minutes.

New Business:

1. 3434 Central Street Planned Development

Charles Marlas, applicant, applies for a Special Use for a Planned Development and a Special Use to demolish the existing church and other site improvements and to construct a new 2-story, 22,416 square foot building for a Daycare Center-Child, Kensington School. The following site development allowances are needed: 1) Off-street parking located within the front yard where parking is not permitted, 2) Off-street parking located within the south interior side yard where parking is not permitted, 3) Detached accessory use, refuse enclosure, located within the south interior side yard where a detached accessory use is not permitted, 4) Reduce the required transition landscape strip along the south property from 10' to 6', 5) Eliminate the required 10-foot wide transition landscape strip along the west property line, and 6) Reduce the two-way driveway aisle width from 24' to 16', in the R2 Single-Family Residential District.

APPLICATION PRESENTED BY: Charles Marlas, applicant
Lance Lauderdale, architect
Michael Werthmann, KLOA, traffic consultant

DISCUSSION:

- Applicant described the evolution of the proposed development plan, noting the different site plans considered with the one submitted for approval providing the best layout.

- Applicant noted parking, traffic, and access are significant concerns. He stated there is an access easement for the adjacent Co-Op residents running through the site that needs to be modified.
- Applicant noted the plan is for a full access driveway off Gross Point Road and a right-out only driveway off Central Street. He stated the Co-Op residents will be able to use the right-out only driveway off Central Street.
- Applicant acknowledged that they need to relocate the water service for the Co-Op residents.
- L. Biggs stated traffic movement in the area is tricky due to traffic light signal timing. She stated east bound traffic on Central is a concern. She stated the proposed plan provides a reasonable solution. She stated that if there is a traffic problem on Central Street then the right-out driveway could be restricted to non-peak hours or the applicant may need to use an IDOT consultant to do a traffic light signal timing study.
- Applicant stated they can remove the right-out driveway.
- M. Werthmann stated traffic volume on Central Street is low and the proposed development will add approximately 30 vehicles. He stated the right-out driveway off Central Street is to relieve left turns onto Gross Point Road. He stated the proposed development will have two peak traffic periods, in the morning and afternoon.
- Applicant stated child drop-offs occur over a couple of hours in the morning, generally before the morning rush., and child pick-ups occur between 3:00 - 6:00 p.m, with the bulk before the evening rush.
- L. Biggs stated leaving the right-out driveway in the plan is preferred but use may need to be restricted if problems arise.
- J. Nyden stated a barrier could be placed to block the driveway if needed, such as traffic cones.
- M. Callahan noted the location of the trees and the proposed fence along the west property line, noting several of the existing trees appear to straddle the property line and both property owners share maintenance responsibilities. He stated access to these trees, the fence, and landscaping is needed for maintenance.
- L. Biggs stated the City encourages composting for any food service operation.
- Applicant stated meals are catered, there is no on-site cooking.
- J. Nyden noted the City has a franchise agreement for refuse pick-up, the City's franchised refuse pick-up company is required to be used.
- L. Biggs stated the specific pick-up schedule can be adjusted.
- J. Nyden stated snow is not to be pushed onto public streets.
- J. Nyden asked about the status of the easement with the Co-Op.
- Applicant stated there are access and utility easements running through the site. He stated the Co-Op residents will have full access to their parking from Gross Point Road and the ability to use the driveway off Central Street.
- J. Nyden stated signage may be needed to indicate the Co-Op parking is private.
- M. Turrene Jr. asked if there is adequate parking for staff or if staff would be parking in the neighborhood.

- Applicant stated staff will park in the parking lot the same as is done at their other locations.
- A. Schnur asked if there would be activities after hours.
- Applicant stated there are seasonal evening events. These events do not have the full staff or all of the classes participating at the same time.
- M. Tristan stated the Fire Department needs access that is at least 20' wide, the driveway off Central Street is less than 20' wide. He stated a maneuvering diagram is needed showing their largest vehicle is able to get in and off the site.
- Applicant noted a wider driveway may encourage more use of the driveway.
- M. Jones asked where HVAC will be located.
- Applicant stated HVAC equipment will be located on the roof.
- J. Nyden asked if the equipment would be screened. She asked about the sound.
- Applicant stated the equipment will be hidden by the roof parapet and they have not had an issue with sound at other locations.
- K. Ashbaugh stated a separate sign permit will be required.
- C. Pratt noted the Green Building Ordinance applies and requires LEED Silver certification.
- J. Nyden asked if a barrier between the parking lot and streets had been considered due to the busy streets, as a safety precaution for children.
- Applicant stated they have not and it hasn't been an issue at other locations.

Public Comment:

- Larry Raffel stated he lives across the street on Central Street. He stated he prefers there not be a driveway onto Central Street, and suggested prohibiting parking on Central Street. He raised concerns with traffic and signal timing on Central Street. He stated there aren't sidewalks in the neighborhood, the sidewalk on the plan goes nowhere. He asked about the construction schedule.
- Applicant stated construction would take about 14 months.
- J. Nyden stated a Construction Management Plan is required which addresses many items, including where construction workers access the property and park, for example. She noted construction hours are restricted.
- Brian Mahoney stated he is an attorney representing the estate of two Co-Op units. He stated the application is not valid since full ownership is not disclosed, objected to changing the access easement location, there isn't an easement agreement, and questioned why the City was considering the proposal without IDOT's approval of the driveway off Gross Point Road.
- Mary Wilson stated she lives across the street on Central Street. She raised concerns with the lack of sidewalks in the neighborhood which cause people to walk in the street, concerned with the right-out driveway off Central Street and wonders if people will try to turn left.
- L. Biggs stated the City requires sidewalks for new development and recognized there are gaps in the City's sidewalks across the City that they are working to fix.

L. Biggs made a motion to recommend approval to the Land Use Commission subject to the following conditions:

1. After operations begin, if traffic added by the daycare center use becomes an issue on Central Street, the right-only turn onto Central Street will be restricted to outside peak traffic hours. If this is unsuccessful in resolving traffic issues, the owner will be required to engage an IDOT pre-qualified SCAT consultant to complete the analysis to retune the traffic signals,
2. Staff is not allowed to park in the neighborhood,
3. Provide access to maintain trees and plantings along the west property line, and
4. Provide a drawing indicating in/out access for emergency vehicles,

seconded by A. Shnur.

The Committee voted by voice vote, 10-0, to recommend approval to the Land Use Commission subject to the conditions noted above.

Adjournment

A. Schnur made a motion to adjourn, seconded by C. Pratt. The Committee voted by voice vote, 10-0, to adjourn. The Committee adjourned at 2:57 p.m.

The next regularly scheduled DAPR meeting is **Tuesday, April 19, 2022 at 2:00 pm at the Lorraine H. Morton Civic Center, 2100 Ridge Avenue, Room G300.**

Respectfully submitted,
Michael Griffith, Planner



Michael Griffith <mgriffith@cityofevanston.org>

Kensington School project

1 message

Peter Roothaan <proothaan@gmail.com>

Mon, Apr 11, 2022 at 5:56 PM

To: Michael Griffith <mgriffith@cityofevanston.org>

Cc: Roseanne Mark <rmark@northwestern.edu>, Robert Feldman <rkf860@live.com>

Hi Michael,

Thank you for following up.

Kensington School has not contacted First Williamsburg Corporation in regard to the easement. First Williamsburg held an internal meeting recently to discuss shareholders thoughts about the proposed development. After careful consideration of drawings of the proposed development, shareholders were unanimous in the opinion that the Kensington development would drastically impair vehicle traffic in and out of First Williamsburg Corporation, severely reduce the size of our access drive, and make our parking area difficult to negotiate. We believe that it would negatively affect our quality of life and property values.

In addition, we do not think that the development fits in with the current character of the neighborhood. The area has been zoned for residential and a house of worship and we believe that a zoning change that would allow the construction of a for-profit enterprise will negatively affect our neighborhood, encouraging further encroachment into the neighborhood by other business enterprises, and lead to the degradation of the residential character of the neighborhood. The replacement of a non-profit church with a for-profit school is not in our view a positive development. Traffic frequency would dramatically increase, from the church's usual Sunday morning usage to the for-profit school's daily pickups and dropoffs that could number over a hundred cars twice a day 5 days per week. We also believe the neighborhood will be adversely affected by the demolition of a mid century, architecturally significant Church. The corporation also believes that a curb cut to Gross Point Road will add another traffic choke point to an already overburdened street, and as a practical matter, will eliminate any possible left hand turn from the property; a quite unnecessary inconvenience.

Also, we are not sure how anyone could evaluate the proposal without Kensington first providing an ALTA Survey identifying the location of the existing easement on both parcels, and Kensington's proposal for the forfeiture of that right-of-way, identifying area of land lost by the Corporation. Have those drawings been submitted to the Village? Wouldn't the City need that information to properly evaluate the proposal, including health and public safety/access and the impact on both parcels?

First Williamsburg Corporation has been in existence since 1953 and is committed to maintain the non-commercial residential character of our neighborhood that is so attractive to our residents and neighbors.

First Williamsburg Corporation is strongly opposed to this project.

Peter Roothaan

President, First Williamsburg Corporation



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Tue, Jun 14, 2022 at 6:11 PM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/14/22 7:11 PM

Name: Craig McClure

Address of Residence: 2507 Princeton Ave

Phone: (847) 513-2595

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: For the proposed Kensington School at 3434 Central Street (PIN # 10-10-200-073-0000), my partner and I are concerned about the number of site development allowances being requested.

In particular, the following allowances concern us:

Reduction of the transition landscape strip along the south property line from 10' to 6'

Elimination of the required 10 foot transition landscape strip along the west property line

Reduction of the two-way driveway width from 24' to 16'

Agenda Item (or comment on item not on the agenda): 3434 Central Street (PIN # 10-10-200-073-0000)

**Position on
Agenda Item:**

Other: the plan needs fewer site development allowances

[Quoted text hidden]



Meagan Jones <mmjones@cityofevanston.org>

Land Use Commission Public Comment

noreply@formstack.com <noreply@formstack.com>

Wed, Jun 8, 2022 at 9:46 AM

Reply-To: noreply@formstack.com

To: mmjones@cityofevanston.org, kashbaugh@cityofevanston.org, mklotz@cityofevanston.org



Formstack Submission For: Land Use Commission Public Comment

Submitted at 06/08/22 10:46 AM

Name: Larry Raffel

Address of Residence: 3509 Central Street

Phone: (847) 409-6376

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: I am unable to attend the Land Use Commission Public Meeting on June 22, 2022 for Planned Development at 3434 Central Street (Kensington School).

However, I want to reiterate the comments I expressed in my email to Michael Griffith on February 23, 2022 as well as my spoken comments at the April 12, 2022 DARP Committee Meeting.

The safety of residents on Central Street continues to be the main concern. The 3' wide carriage walk on the north side of Central Street is mostly unuseable and residents tend to walk with their children and dogs in the street. It was clearly stated at the DARP meeting that we will not be getting new sidewalks so this continues to be a hazard. As the weather has gotten nicer, more and more people are walking in the street.

Futhermore, vehicular traffic at the intersection of Central Street, Gross Point Road and Crawford Street is very dangerous. It also appears that the timing of the lights has changed again making it even more dangerous.

At the DARP meeting, Staff expressed a wait and see attitude regarding these

issues. Hopefully Staff, Commission Members and the City Council will address these issues early on.

Thank you,

Lawrence Raffel, ASLA, PLA

Agenda Item (or comment on item not on the agenda):

3434 Central Street, PIN: 10-10-200-073-0000 Case #22PLND-0012- Planned Development

Position on Agenda Item:

Other: Concerned Neighbor

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Michael Griffith <mgriffith@cityofevanston.org>

Re: Kensington School of Evanston Site Plan - 3434 Central Street

1 message

Katie Ashbaugh <kashbaugh@cityofevanston.org>

Wed, Feb 23, 2022 at 2:16 PM

To: Larry Raffel <larryraffel50@gmail.com>

Cc: Bruce Baumberger <bruce.baumberger@outlook.com>, Michael Griffith <mgriffith@cityofevanston.org>

Hi Mr. Raffel,

Thank you very much for your detailed comments. I, unfortunately, am not the reviewing Planner for this project, but I have copied Planner Michael Griffith on this email so that he is aware. You may continue any correspondence with him regarding this particular project. Best,

Katie Ashbaugh, AICP
Planner
Planning & Zoning Division
Community Development Department

On Wed, Feb 23, 2022 at 2:14 PM Larry Raffel <larryraffel50@gmail.com> wrote:

Ms Ashbaugh,

My name is Larry Raffel and I live at [3509 Central Street](#).

I recently received what I assume is the most current Site Plan for Kensington School of [Evanston \(3434 Central Street\)](#). It is dated 10-1-21 however this is not the same plan that was presented to the neighborhood on October 19, 2021.

This plan still does not adequately address a major concern of the neighborhood, traffic on Central Street as well as the safety of the residents. While the major access into the site has been relocated to Gross Point Road and a right turn exit (east) only drive is included on Central Street, this will not alleviate safety issues on Central Street for the following reasons:

1. The parking lot has no designated drop-off/pick-up, and with two-way traffic in every drive aisle, vehicles are always going to move in whatever direction is quickest. Even though the drive on Central Street is exit right only, and is designed as such, vehicles will use it in unintended ways including left turns (in and out). I also believe this drive will become a drop-off/pick-up spot.
2. Without the designated drop-off/pick-up, drivers will quickly learn that the easiest and fastest (and safest for users of the school) way to drop-off and pick-up children is to park or simply stand on Central Street. 'No Parking' signs can help but since there is no enforcement, it won't matter.
3. Central Street between Gross Point Road and Princeton only has a small carriage walk on the north side of the street. This forces adults and children to walk in the street. Any additional traffic on Central Street is a serious hazard.

The plan that I received does not show any existing utilities however, using the location of the existing trees as a reference, it appears that there is an existing street light pole and fire hydrant where the new drive on to Central Street is located.

I recently retired after 35 years as a Landscape Architect and Planner (including a term on the Evanston Plan Commission) and I think that by simply designing the parking lot with a clear and cohesive traffic pattern and designated drop-off/pick-up, some of these serious safety issues can be alleviated.

I look forward to attending the DAPR meeting to speak directly to these issues .

Sincerely,

--

Lawrence Raffel, PLA, ASLA
Landscape Architect / Planner

larryraffel50@gmail.com
847.409.6376

--

Katie Ashbaugh, AICP

Planner

Planning & Zoning Division

Community Development Department

City of Evanston

She, Her, Hers

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Michael Griffith <mgriffith@cityofevanston.org>

Kensington school plan

1 message

Patricia Vaughan Tremmel <pattremm@gmail.com>

Tue, Mar 15, 2022 at 12:57 PM

To: mgriffith@cityofevanston.org

Cc: Pat Tremmel <pattremm@gmail.com>

Dear Mr. Griffith,

Please add my email to the email blast the city sends out every Friday before a public planning committee meeting. I'm a longtime resident of Evanston and First Williamsburg, who has spent most of my career working in the city, which I truly love. When I first moved to the city, more than 30 years ago, friends joked that I should do commercials for Evanston.

But now I am highly concerned about Kensington's proposed plan to build a new school at Gross Point Road and Central Street. As a resident of the housing complex that shares a parking area with the adjacent Unity Church property on which Kensington is seeking approval to build the school, I'm among the key stakeholders in the outcome of the proposed plan. The changes being proposed are significant and would greatly affect access to my property, for service and emergency vehicles, as well for all residents of our complex. I'm not even sure if the proposed plan would allow enough room for residents to back in and out of parking spaces.

Moving access to the adjoining properties from Central Street to Gross Point Road is of particular concern, related to traffic and safety issues. I write for an hour or two in the morning before a window facing Gross Point Road, and I'm often struck with how the clusters of cars turning on the short curve that leads from Central Street to Gross Point Road often scramble quickly, and way to close for comfort, to change lanes.

The proposed entrance and exits on Gross Point Road that Kensington is proposing are just a little south on that short stretch of Gross Point Road, where all the changing of lanes occurs. It seems to me that entering and exiting the adjoining properties from Gross Point Road would be very troublesome during busy times of the day, particularly during the school's busy drop-off and pick-up times and the city's snow removal times. It is important to consider that a chief reason for all the scrambling of cars after the turn onto Gross Point Road has to do with drivers needing to quickly get into the right lane if they want to turn onto Old Orchard Road, a short distance from I94, and, at this juncture, the gateway to Evanston.

Thus, as discussions and plans progress, I would very much like to be fully informed of everything related to this plan. Perhaps, as a very concerned citizen, I might even want to participate in public meetings on the subject.

Thanks very much for any help you can provide,
Patricia Tremmel