

123-O-21

AN ORDINANCE

**Granting A Major Adjustment to the Planned Development
Located at 1900 Sherman Avenue**

WHEREAS, the City of Evanston is a home-rule municipality pursuant to Article VII of the Illinois Constitution of 1970; and

WHEREAS, as a home rule unit of government, the City has the authority to adopt ordinances and to promulgate rules and regulations that protect the public health, safety, and welfare of its residents; and

WHEREAS, Article VII, Section (6)a of the Illinois Constitution of 1970, which states that the “powers and functions of home rule units shall be construed liberally,” was written “with the intention that home rule units be given the broadest powers possible” (*Scadron v. City of Des Plaines*, 153 Ill.2d 164); and

WHEREAS, it is a well-established proposition under all applicable case law that the power to regulate land use through zoning regulations is a legitimate means of promoting the public health, safety, and welfare; and

WHEREAS, Division 13 of the Illinois Municipal Code (65 ILCS 5/11-13-1, *et seq.*) grants each municipality the power to establish zoning regulations; and

WHEREAS, pursuant to its home rule authority and the Illinois Municipal Code, the City has adopted a set of zoning regulations, set forth in Title 6 of the Evanston City Code of 2012, as amended, (“the Zoning Ordinance”); and

WHEREAS, in November 2020, the City Council enacted Ordinance 109-O-20, attached hereto as Exhibit 1 and incorporated herein by reference, which granted a Special Use Permit for a Planned Development (the “Planned Development”) at 1900 Sherman Avenue (the “Subject Property”), which is legally described in Exhibit 1; and

WHEREAS, Ordinance 109-O-20 approved a map amendment to rezone the property from R6 General Residential to C1a Commercial Mixed-Use in addition to the construction and operation of a Planned Development with, among other things, a sixteen (16) story, one hundred seventy-two foot eight inch (172’, 8”) age-restricted multi-family residential building with one hundred sixty-eight (168) dwelling units, with thirty-seven (37) on-site subterranean parking spaces, while retaining the existing eleven (11) story one hundred (100) unit building authorized as a planned development in 1976 pursuant to ordinance 61-O-76, which is detailed at length in Exhibit 1; and

WHEREAS, the Applicant, the Housing Authority of Cook County (“the Applicant”) has submitted for a major adjustment to the Planned Development, seeking the following changes: a decrease in the number of new dwelling units from one hundred sixty-eight (168) to one hundred fifty-two (152), a decrease in the number of below grade parking spaces from thirty-seven (37) to twenty-five (25), and a decrease in zoning height from one hundred seventy-two feet eight inches (172’, 8”) to one hundred sixty-eight feet four inches (168’, 4”); and

WHEREAS, the proposed changes trigger the need for a major adjustment to the 2020 planned development; and

WHEREAS, in order to approve the major adjustment requested, the Applicant requests amendments to Ordinance 109-O-20 (the “Previously Approved Ordinance”); and

WHEREAS, the Previously Approved Ordinance is a piece of legislation enacted by the City Council of the City of Evanston, subject to revision only by said City Council; and

WHEREAS, on October 13, 2021 and October 27, 2021, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq.*) and the Zoning Ordinance, the Plan Commission held a public hearing on the application for a Major Adjustment to a Planned Development, case no. 21PLND-0076 heard extensive testimony and public comment, received other evidence, and made written minutes, findings, and recommendations; and

WHEREAS, the Plan Commission recommended City Council approval of the application for a major adjustment with the amendment that the Applicant obtain twelve (12) additional parking spaces to offset the loss of subterranean parking and that parking for caregivers is paid for, if requested, by the Applicant; and

WHEREAS, on November 22, 2021, the Planning and Development Committee (“P&D Committee”) held a meeting, in compliance with the provisions of the Illinois Open Meetings Act (5 ILCS 120/1 *et seq.*), during which it considered the Applicant’s request; and

WHEREAS, during said meeting, the P&D Committee received input from the public, carefully deliberated on the major adjustment, and recommended approval thereof by the City Council; and

WHEREAS, at its meeting on December 13, 2021, held in compliance with the provisions of the Illinois Open Meetings Act, the City Council considered the P&D Committee's recommendation, heard public comment, made findings, and adopted said recommendation; and

WHEREAS, it is well-settled law in Illinois that the legislative judgment of the City Council must be considered presumptively valid (see *Glenview State Bank v. Village of Deerfield*, 213 Ill.App.3d 747) and is not subject to courtroom fact-finding (see *National Paint & Coating Ass'n v. City of Chicago*, 45 F.3d 1124),

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS, THAT:

SECTION 1: The foregoing recitals are hereby found as fact and incorporated herein by reference.

SECTION 2: Pursuant to the terms and conditions of this ordinance, the City Council hereby grants an amendment to the Special Use Permit previously authorized by Ordinance 109-O-20 to allow a decrease in the number of new dwelling units from one hundred sixty-eight (168) to one hundred fifty-two (152); a decrease in the number of below grade on-site parking spaces from thirty-seven (37) to twenty-five (25), with the requirement that the Applicant obtain twelve (12) additional off-site parking spaces and that parking for caregivers is paid for, if requested, by the Applicant; and a

decrease in zoning height from one hundred seventy-two feet eight inches (172', 8") to one hundred sixty-eight feet four inches (168', 4").

SECTION 3: The conditions on the Special Use Permit imposed pursuant to Subsection 6-3-5-12 of the Zoning Ordinance by City Council in Ordinance 109-O-20 remain applicable to the Planned Development,

SECTION 4: Except as otherwise provided for in this 123-O-21, all applicable regulations of the Ordinance 109-O-20, the Zoning Ordinance, and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and/or provisions of any of said documents conflict with any of the terms herein, this Ordinance 123-O-21 shall govern and control.

SECTION 5: When necessary to effectuate the terms, conditions, and purposes of this ordinance, "Applicant" shall be read as "Applicant and its agents, assigns, and successors in interest."

SECTION 6: This ordinance shall be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

SECTION 7: Except as otherwise provided for in this ordinance, all applicable regulations of the Zoning Ordinance and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and provisions of any of said documents conflict with the terms herein, this ordinance shall govern and control.

SECTION 8: All ordinances or parts of ordinances that are in conflict with the terms of this ordinance are hereby repealed.

SECTION 9: If any provision of this ordinance or application thereof to any person or circumstance is ruled unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this ordinance is severable.

SECTION 10: The findings and recitals herein are hereby declared to be prima facie evidence of the law of the City and shall be received in evidence as provided by the Illinois Compiled Statutes and the courts of the State of Illinois.

Introduced: November 08, 2021

Approved:

Adopted: December 13, 2021

12/14

_____, 2021

Daniel Biss

Daniel Biss, Mayor

Attest:

Stephanie Mendoza

Stephanie Mendoza, City Clerk

Approved as to form:

Nicholas E. Cummings

Nicholas E. Cummings,
Corporation Counsel

EXHIBIT 1

Ordinance 109-O-20

10/23/2020
11/10/2020

109-O-20
AN ORDINANCE

Amending the Zoning Map to Rezone 1900 Sherman Avenue From the R6 General Residential District to the C1a Commercial Mixed Use District and Granting A Special Use Permit for A Planned Development at 1900 Sherman Avenue

WHEREAS, the City of Evanston is a home-rule municipality pursuant to Article VII of the Illinois Constitution of 1970; and

WHEREAS, as a home rule unit of government, the City has the authority to adopt ordinances and to promulgate rules and regulations that protect the public health, safety, and welfare of its residents; and

WHEREAS, Article VII, Section (6)a of the Illinois Constitution of 1970, which states that the “powers and functions of home rule units shall be construed liberally,” was written “with the intention that home rule units be given the broadest powers possible” (*Scadron v. City of Des Plaines*, 153 Ill.2d 164); and

WHEREAS, it is a well-established proposition under all applicable case law that the power to regulate land use through zoning regulations is a legitimate means of promoting the public health, safety, and welfare; and

WHEREAS, Division 13 of the Illinois Municipal Code (65 ILCS 5/11-13-1, *et seq.*) grants each municipality the power to establish zoning regulations; and

WHEREAS, pursuant to its home rule authority and the Illinois Municipal Code, the City has adopted a set of zoning regulations, set forth in Title 6 of the Evanston City Code of 2012, as amended, (“the Zoning Ordinance”); and

WHEREAS, the Housing Authority of Cook County (the “Applicant,”), for the proposed development located at 1900 Sherman Avenue, Evanston, Illinois (the “Subject Property”), legally described in Exhibit A, which is attached hereto and incorporated herein by reference, applied, pursuant to the provisions of the Zoning Ordinance, specifically Section 6-7-2, “Zoning Map”, Section 6-3-5, “Special Uses”, Section 6-3-6, “Planned Developments”, and Subsection 6-10-1-9, “Planned Developments” in Commercial Zoning Districts, to amend the Zoning Map to move the Subject Property from the R6 Residential District (“R6 District”) to the C1a Commercial Mixed Use District (“C1a District”), and to permit the construction and operation of a Planned Development located at the Subject Property in the C1a District; and

WHEREAS, the Applicant sought approval to construct a new sixteen (16) story, one hundred seventy-two foot eight inch (172’, 8”) tall, age-restricted multi-family residential building with one hundred sixty-eight (168) residential units and thirty-seven (37) subterranean on-site parking spaces, while retaining the existing eleven (11) story one hundred (100) unit building authorized as a Planned Development in 1976 pursuant to ordinance 61-O-76.

WHEREAS, construction of the Planned Development, as proposed in the application, requires exception from the strict application of the Zoning Ordinance with regards to minimum lot area density, building height, number of loading berths, loading berth dimensions, and number of parking spaces; and

WHEREAS, pursuant to Subsection 6-3-6-5 of the Zoning Ordinance, the City Council may grant Site Development Allowances to the normal district regulations established in the Zoning Ordinance; and

WHEREAS, on September 9, 2020 and October 14, 2020, the Plan Commission held a public hearing, pursuant to proper notice, regarding case no. 20PLND-0039, to consider amendments to the Zoning Map, cited in Section 6-7-2 of the Zoning Ordinance, to place 1900 Sherman Avenue from the R6 General Residential District to the C1a Commercial Mixed Use District and to consider the Applicant's application for Special Use Permit for Planned Development; and

WHEREAS, the Plan Commission received extensive testimony, heard public comment, and made findings pursuant to Subsections 6-3-4-6, 6-3-5-10, 6-3-6-9, and 6-10-1-9 of the Zoning Ordinance, and recommended City Council approval of the amendment to the Zoning Map and made no recommendation as to the application for Special Use Permit for Planned Development due to a 3-3 Plan Commission vote; and

WHEREAS, at its meeting of November 9, 2020, the Planning and Development Committee of the City Council considered and reviewed the findings and recommendation of the Plan Commission in case no. 20PLND-0039 and recommended City Council approval thereof; and

WHEREAS, at its meetings of November 9, 2020 and November 23, 2020, the City Council considered and adopted the records and recommendations of the Planning and Development Committee; and

WHEREAS, while two of the requested site development allowances are above the maximum site development allowances for planned developments, a favorable supermajority (two-thirds) vote of the City Council is not required because of Applicant's Inclusionary Housing Ordinance (IHO) compliance; and

WHEREAS, it is well-settled law that the legislative judgment of the City Council must be considered presumptively valid (see *Glenview State Bank v. Village of Deerfield*, 213 Ill.App.3d 747) and is not subject to courtroom fact-finding (see *National Paint & Coating Ass'n v. City of Chicago*, 45 F.3d 1124),

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS, THAT:

SECTION 1: The foregoing recitals are found as fact and incorporated herein by reference.

SECTION 2: The City Council hereby amends the Zoning Map to remove the property with the address and PIN listed in Exhibit B and identified in Exhibit C, both attached hereto and incorporated herein by reference, from the R6 General Residential District and place it within the C1a Commercial Mixed Use District.

SECTION 3: Pursuant to the terms and conditions of this ordinance, the City Council hereby grants the Special Use Permit applied for in case no. 20PLND-0039, to allow construction and operation of the Planned Development described herein.

SECTION 4: The City Council hereby grants the following Site Development Allowances:

- (A) **Minimum Lot Area (Density):** A Site Development Allowance is hereby granted for two hundred sixty-eight (268) dwelling units, whereas subsection 6-10-3-4 of the Zoning Ordinance allows a maximum of one hundred ninety-six (196) dwelling units including an IHO bonus in the C1a District pursuant to subsection 6-10-1-10.
- (B) **Building Height:** A Site Development Allowance is hereby granted for an approximately one hundred seventy-two, eight inch (172' 8") maximum building height, whereas subsection 6-10-3-9 of the Zoning Ordinance requires a

maximum allowed building height of sixty-seven (67) feet in the C1a District, with Subsection 6-10-1-9(C) allowing for an additional thirty (30) feet of height in the C1a District as a site development allowance.

- (C) **Number of Loading Berths:** A Site Development Allowance is hereby granted permitting one (1) short loading berth, whereas subsection 6-16-5 requires that there be two (2) short loading berths.
- (D) **Loading Berth Dimensions:** A Site Development Allowance is hereby granted permitting loading berth dimensions of ten (10) feet by twenty-five (25) feet, whereas subsection 6-16-6-4 requires that loading berth dimensions be ten (10) feet by thirty-five (35) feet.
- (E) **Number of Parking Spaces:** A Site Development Allowance is hereby granted permitting a net increase of fifteen (15) parking spaces on the Subject Property, whereas Table 16-B of Chapter 16 of the Zoning Ordinance requires a minimum net increase of ninety-six (96) parking spaces on the Subject Property.

SECTION 4: Pursuant to Subsection 6-3-5-12 of the Zoning Ordinance, the City Council imposes the following conditions on the Special Use Permit granted hereby, which may be amended by future ordinance(s), and violation of any of which shall constitute grounds for penalties or revocation of said Special Use Permit pursuant to Subsections 6-3-10-5 and 6-3-10-6 of the Zoning Ordinance:

1. **Compliance with Applicable Requirements:** The Applicant shall develop and operate the Planned Development authorized by the terms of this ordinance in substantial compliance with: the terms of this ordinance; the Site and Landscape Plans in Exhibits D and E, attached hereto and incorporated herein by reference; all applicable legislation; the Applicant's testimony and representations to the Design and Project Review Committee, the Plan Commission, the P&D Committee, and the City Council; and the approved documents on file in this case.

2. **Construction Management Plan:** The Applicant shall sign and agree to a Construction Management Plan (CMP) with the City of Evanston prior to issuance of the Building Permit. The CMP shall include but is not limited to the following: construction phasing/staging plans; construction hours; site access including traffic and pedestrian safety plans; contractor parking; damage control and vibration monitoring; construction exhibits; project communication and signage.

3. **LEED Silver Certification:** The Applicant agrees to comply with the City of Evanston Green Building Ordinance and will obtain a LEED Silver Certification Rating for the Planned Development on the Subject Property.
4. **Building Resident Parking Permits:** Building residents shall not be eligible for residential on-street parking permits.
5. **Inclusionary Housing Ordinance (IHO) and Additional Affordable Housing:** The applicant shall provide on-site affordable housing for the following thirty years in compliance with the Inclusionary Housing Ordinance. Required unit types and affordability restrictions are as follows: 16 Studio units affordable to households at 50% AMI, 18 One-Bedroom units at 50% AMI, and 17 units at between 80% and 120% AMI. The 80-120% Tenants for the 80-120% AMI units shall utilize the City of Evanston's IHO centralized waitlist, giving priority to households that qualify for the Evanston preference.
6. **Harm Mitigation for Migratory Birds:** The building shall incorporate bird-friendly measures, including bird-friendly treated windows at or below 40 feet above grade, bird-friendly glass treatment at all glass balcony railings, extinguishing any external lighting at the top of the building and dimming or turning out lights at the interior when possible during Spring and Fall migrations. All lights at the top of the building shall be extinguished between the hours of 12:00 am until 6:00 am throughout the year.
7. **Security:** Security Cameras shall be installed to monitor the exterior of the property and adjacent public ways. The recordings shall be made available to the Evanston Police Department upon request.
8. **Evanston Business Enterprises Hiring:** The Applicant will have, as a primary goal, the employment of ten (10) Evanston residents, with a required minimum amount of five (5) Evanston resident employees during construction. Said residents, without regard to sex, race or ethnicity, can be sub-contractors or vendors to the development. The Applicant will hire as many competent minority and/or women Evanston subcontractors, workers, and residents as possible for the construction project.
9. **Sidewalk Widening:** Sidewalks adjacent to the property along Emerson Street and Sherman Avenue shall be widened at the applicant's expense to the satisfaction of the Public Works Agency Director.
10. **Off-Site Parking:** Prior to issuance of the building permit the Applicant shall provide a long-term lease for a minimum of fifty (50) parking spaces within one thousand (1,000) feet of the subject property.
11. **Residential Priority for Perlman Apartments:** Residents of the City of Evanston shall be given priority for occupancy of the dwelling units at the Jane R. Perlman Apartments as originally required by Ordinance 61-O-76.

12. **Payment in Lieu of Taxes for Perlman Apartments:** In addition to any property taxes for the proposed building, the owners shall make an annual payment in lieu of taxes at the rate of 10 percent of the gross amount realized annually for the rental of the units in said Planned Development as originally required by Ordinance 61-O-76.

13. **Changes in Building Use:** Any material changes in the use of the building on the Subject Property must be approved as an amendment to this Planned Development in accordance with Subsection 6-3-6-12 of the Zoning Ordinance.

14. **Recordation:** Pursuant to Subsection 6-3-6-10 of the Zoning Ordinance, the Applicant shall, at its cost, record a certified copy of this ordinance, including all exhibits attached hereto, with the Cook County Recorder of Deeds, and provide proof of such recordation to the City, before the City may issue any permits pursuant to the Planned Development authorized by the terms of this ordinance.

SECTION 5: Except as otherwise provided for in this Ordinance 109-O-20, all applicable regulations of Ordinance 61-O-76 attached hereto as Exhibit F, the Zoning Ordinance, and the entire City Code shall apply to the Subject Property and remain in full force and effect with respect to the use and development of the same. To the extent that the terms and/or provisions of any of said documents conflict with any of the terms herein, this Ordinance 109-O-20 shall govern and control.

SECTION 6: All ordinances or parts of ordinances in conflict herewith are hereby repealed.

SECTION 7: If any provision of this ordinance or application thereof to any person or circumstance is held unconstitutional or otherwise invalid, such invalidity shall not affect other provisions or applications of this ordinance that can be given effect without the invalid application or provision, and each invalid provision or invalid application of this ordinance is severable.

SECTION 8: This ordinance shall be in full force and effect from and after its passage, approval and publication in the manner provided by law.

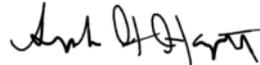
SECTION 9: The findings and recitals contained herein are declared to be prima facie evidence of the law of the City and shall be received in evidence as provided by the Illinois Compiled Statutes and the courts of the State of Illinois.

Introduced: November 9, 2020

Approved:

Adopted: November 23, 2020

April 23, 2021



Stephen H. Hagerty, Mayor

Attest:

Approved as to form:



Devon Reid, City Clerk



Kelley A. Gandurski, Corporation Counsel

EXHIBIT A
LEGAL DESCRIPTION

LOT 1 IN HOUSING AUTHORITY OF COOK COUNTY'S PLAT OF CONSOLIDATION OF LOTS 7, 8, 9, 10, 11 AND 12 IN HUSE AND POWERS ADDITION TO EVANSTON, BEING A SUBDIVISION OF LOT 7 OF THE ASSESSOR'S DIVISION OF THE NORTH EAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 18, TOWNSHIP 41 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS. PURSUANT TO THE PLAT RECORDED MAY 29, 1874 AS DOCUMENT NO. 163502.

Commonly Known As: 1900 Sherman Avenue

PIN: 11-18-109-059-0000

EXHIBIT B

**Address and PIN of Property Removed from the R6 General Residential District
and Placed Within the C1a Commercial Mixed Use District**

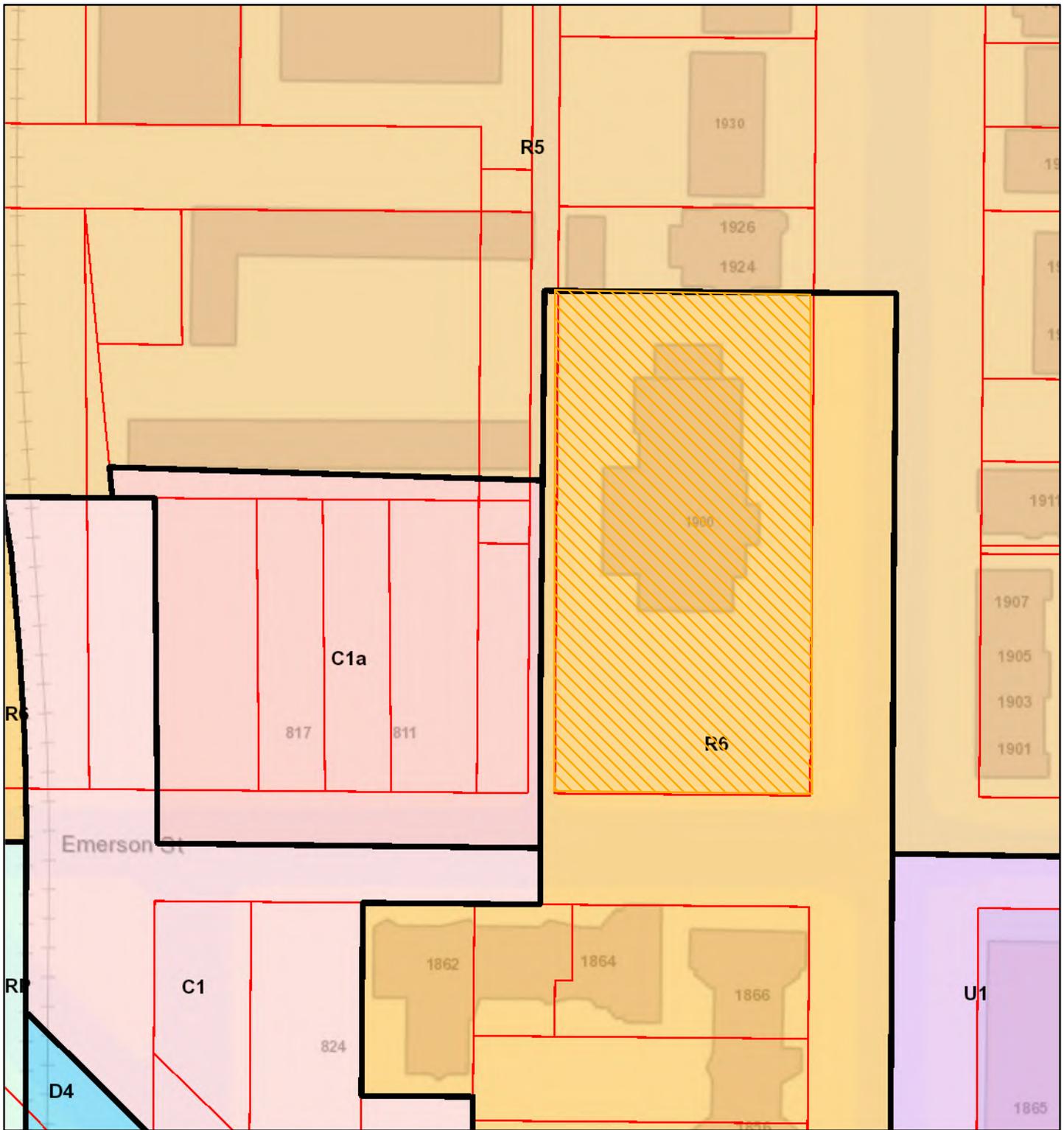
Commonly Known As: 1900 Sherman Avenue

PIN: 11-18-109-059-0000

EXHIBIT C

**Map of Properties Removed from the R6 General Residential District and
Placed Within the C1a Commercial Mixed Use District**

Map Amendment from R6 to C1a



October 26, 2020

1:1,000

drawGraphics_poly

 User drawn polygons

 Zoning Boundaries & Labels

 Tax Parcels

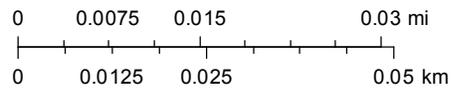
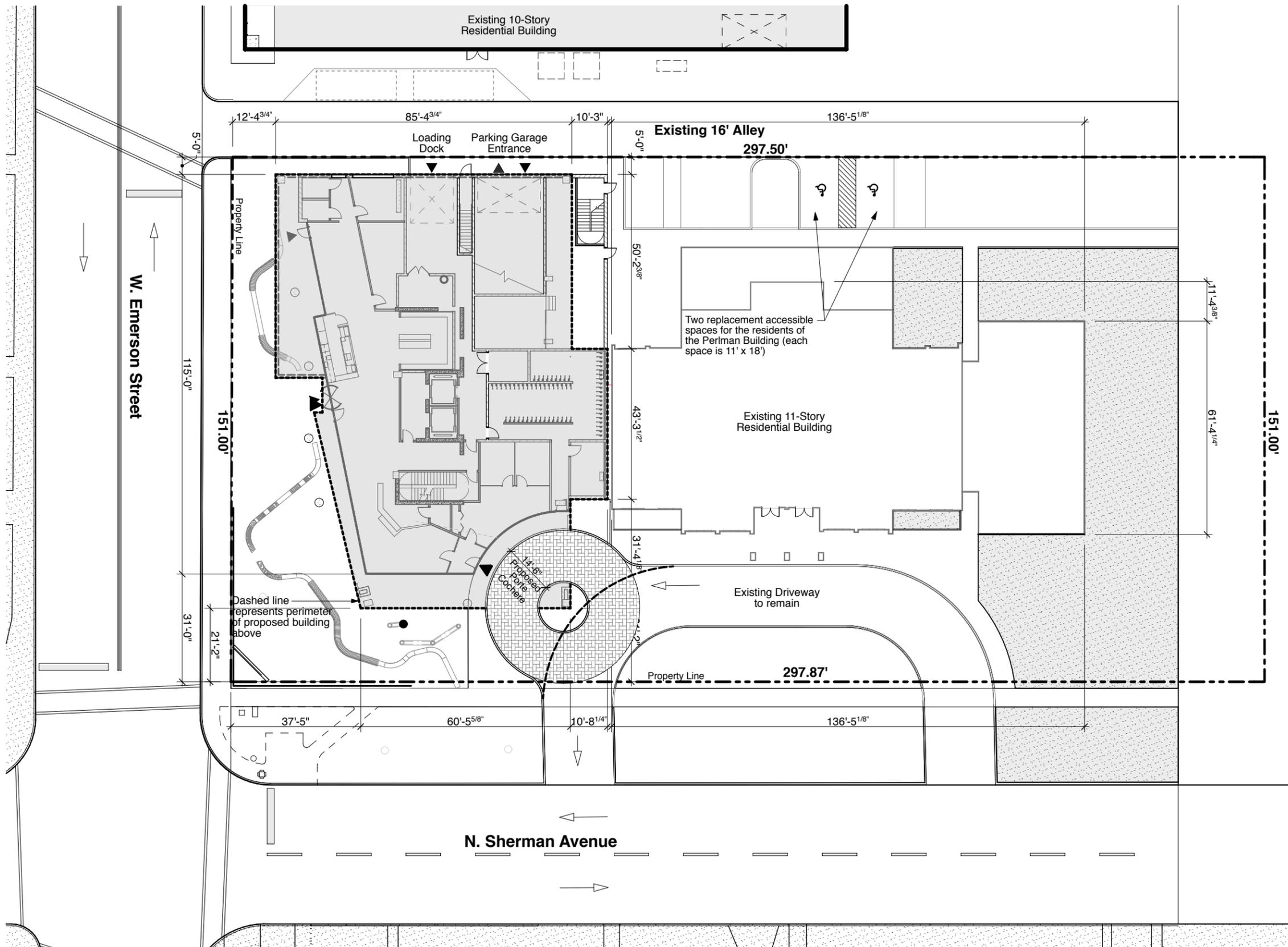


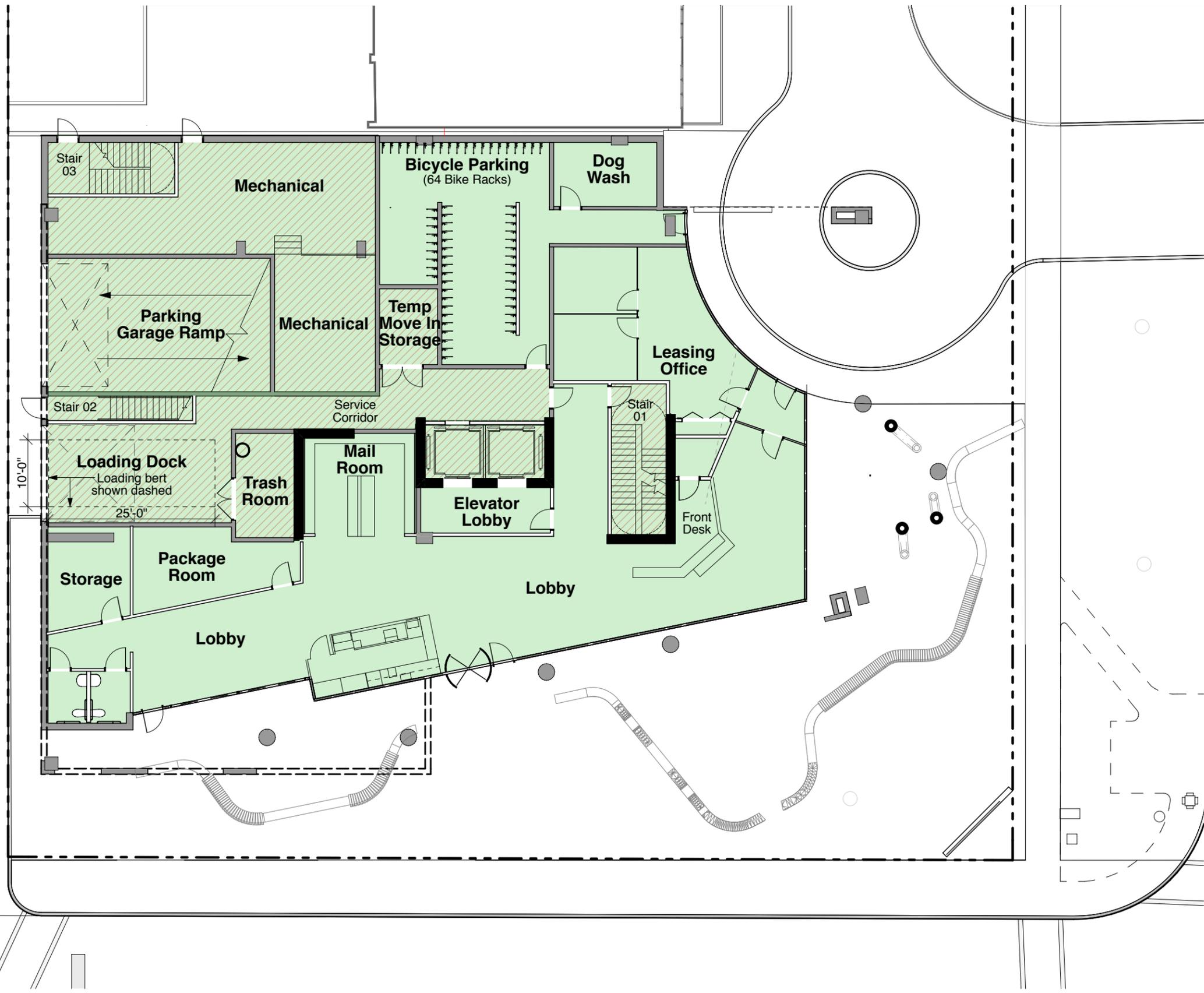
EXHIBIT D

Development Plans



Proposal for 1900 Sherman Ave Evanston IL

Zone	Current R6 Proposed C1a
Area (sf)	16,440 SF
Area (acres)	0.37 Acres
Construction Type	IA or IB
Use of Structure	Residential
Floor Area Typical Floor	10,109 sf
Total FAR Area	144,677 sf
Number of Dwelling Units	168
Type of Dwelling Units	Studio, Convertible One and Two Bedrooms
Size of Dwelling Units (Estimated)	Studios 450sf Convertible 515-550sf One Bed 670-804sf Two Bed 940-1160sf
Overall Dwelling Unit Density	454 units / acre
Parking Spaces	37 Spaces (see plans)
Loading Dock	1 (see plans)

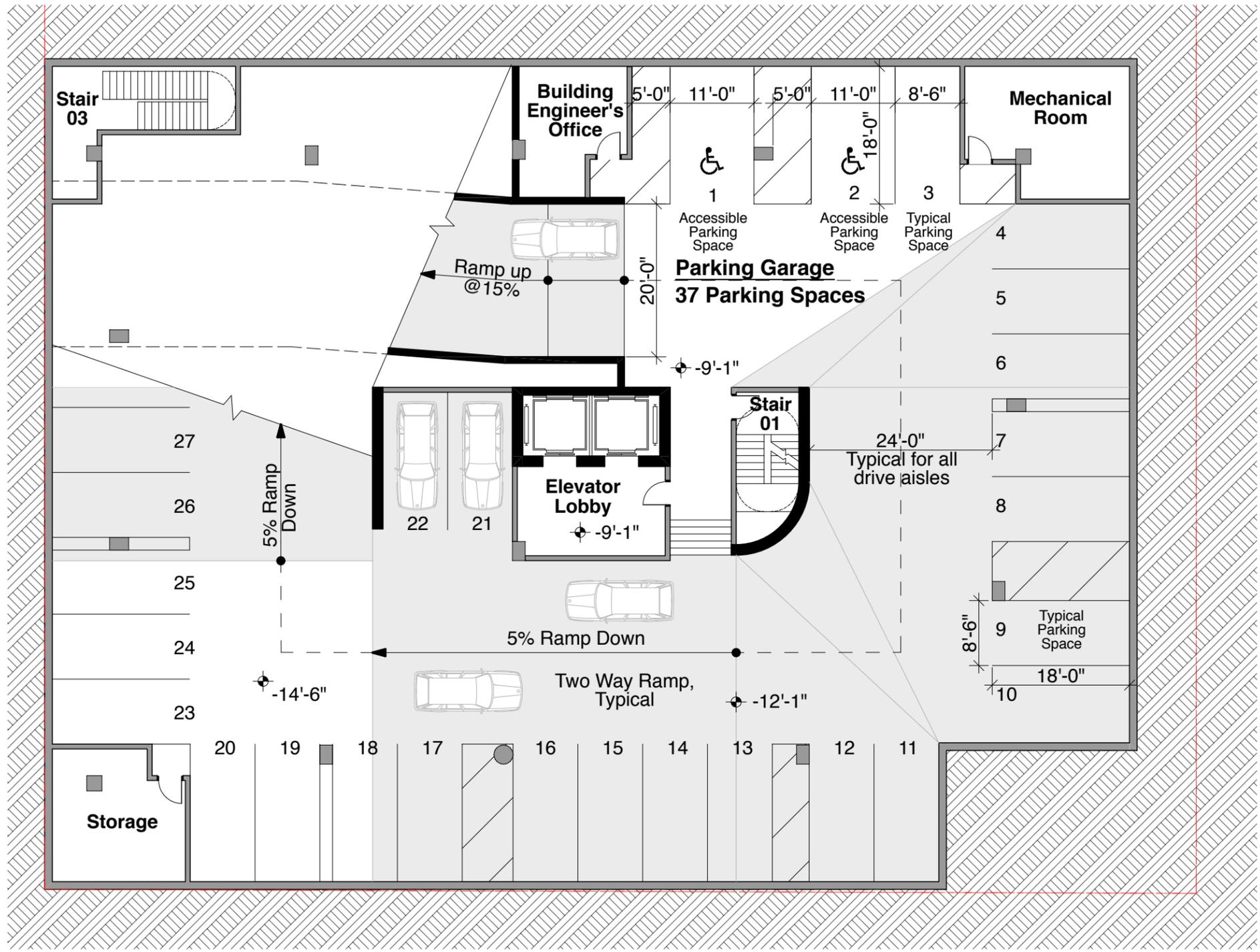


Legend
 Area excluded from floor area calculations

1st Floor Plan

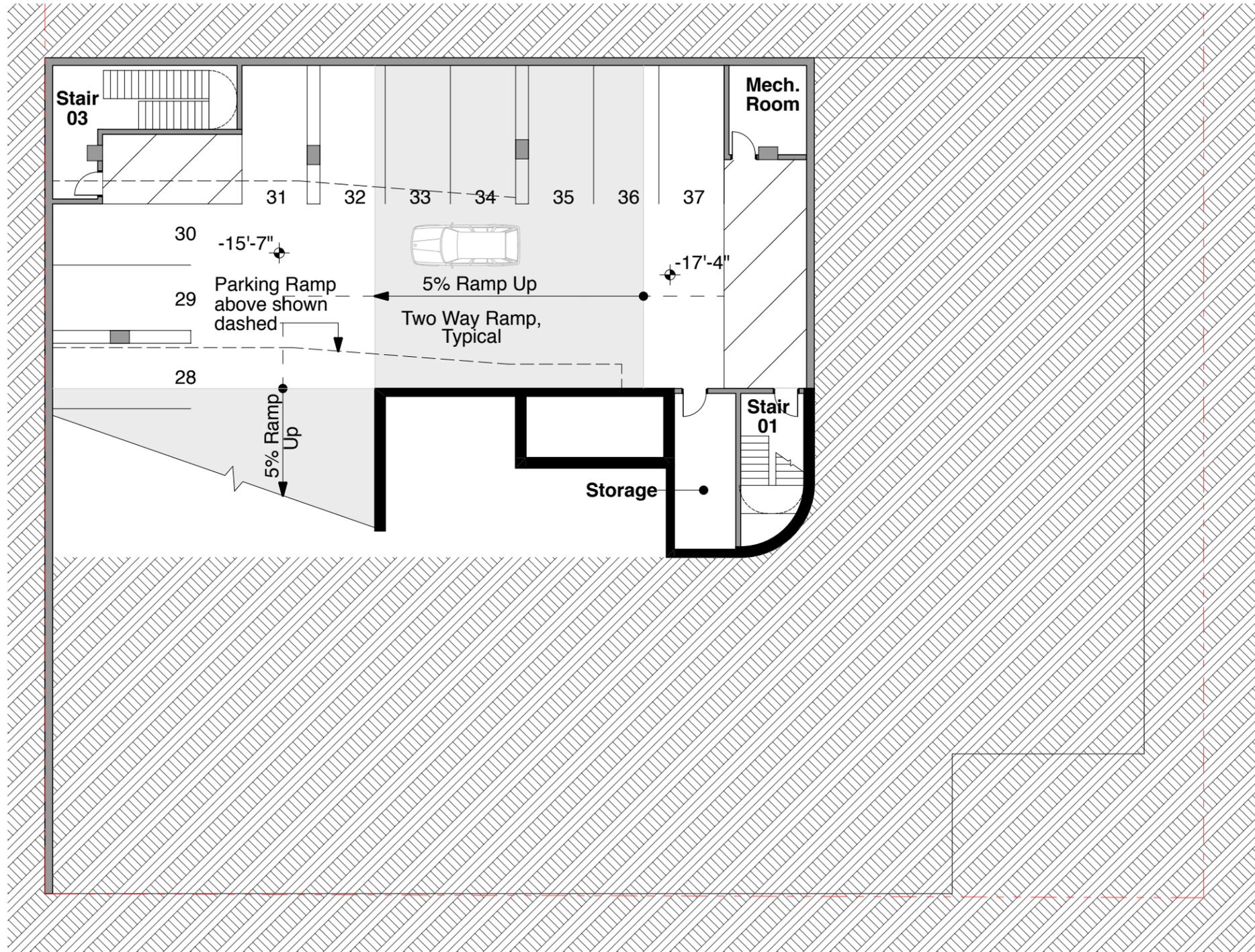
Floor Area (Gross)	8,622 sf
Areas Excluded:	
Elevators	177 sf
Stairwells	310 sf
Loading/Service Corridor	1,093 sf
Mechanical	1,241 sf
Parking Garage Entrance	716 sf
Total Excluded	3,537 sf
Floor Area (Net)	5,085 sf





Basement Garage Plan





Sub-Basement Garage Plan





Legend

 Area excluded from floor area calculations

Floors 2 & 3

Floor Area (Gross)	9,795 sf
Areas Excluded:	
Elevator Shaft	177 sf
Stairwells	355 sf
Trash Chute	26 sf
Electrical/Mechanical	95 sf
Total Excluded	653 sf
Floor Area (Net)	9,142 sf



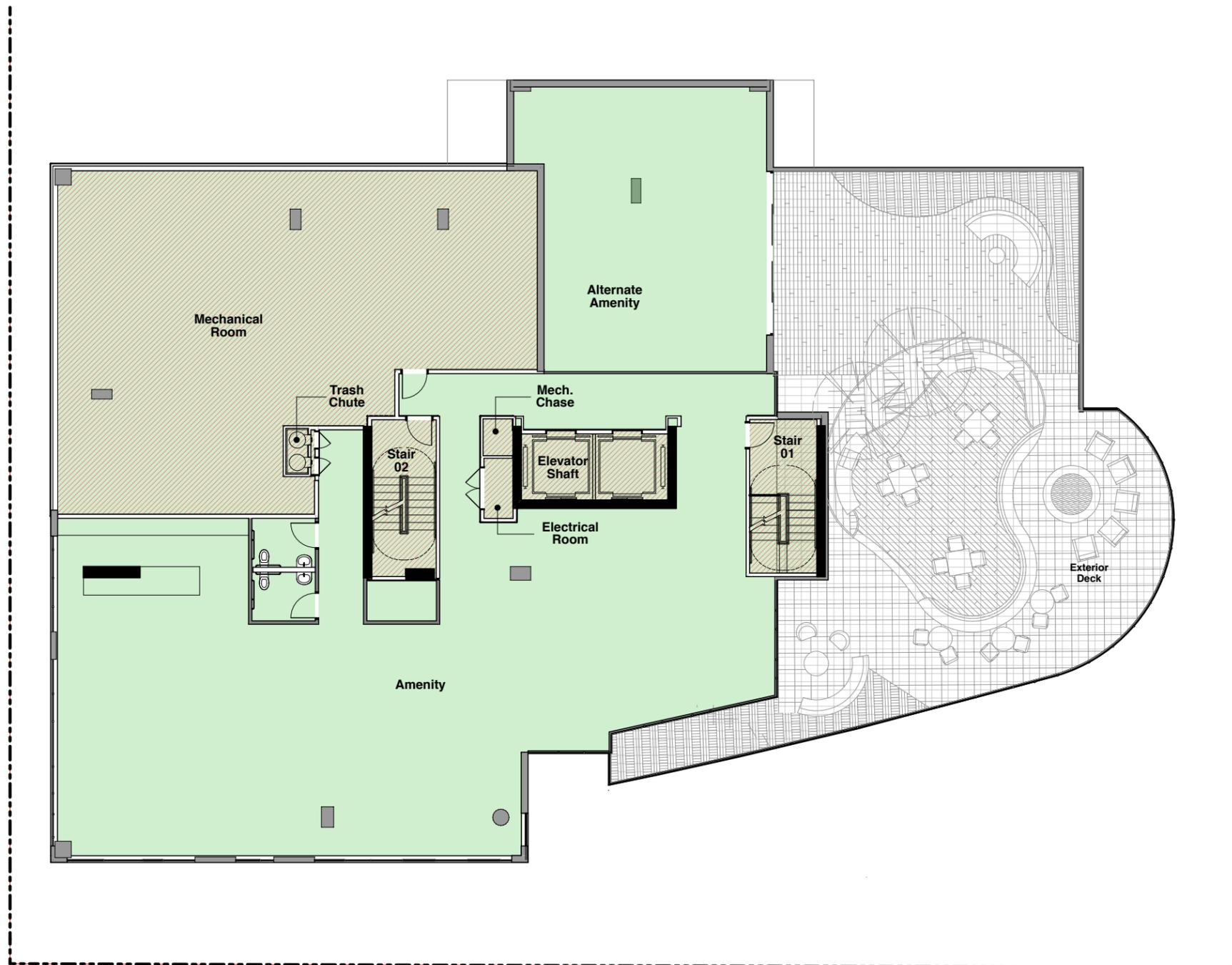


Legend

 Area excluded from floor area calculations

Floors 4-15	
Floor Area (Gross)	10,109 sf
Areas Excluded:	
Elevator Shaft	177 sf
Stairwells	355 sf
Trash Chute	26 sf
Electrical/Mechanical	95 sf
Total Excluded	653 sf
Floor Area (Net)	9,456 sf





Legend

 Area excluded from floor area calculations

16th Floor Plan

Floor Area (Gross)	7,498 sf
Areas Excluded:	
Elevator Shaft	174 sf
Stairwells	350 sf
Trash Chute	25 sf
Electrical/Mechanical	2,196 sf
Total Excluded	2,745 sf
Floor Area (Net)	4,753 sf



Screen wall beyond at mechanical space to be approximately 15' to 20' above the roof deck (range is shown by two dashed horizontal lines)

(Maximum height approximately +192', currently shown at approximately +189'-6")

Brick piers, utility-size brick, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Transition from masonry to window is expected to include a metal fin color matched to the window system so the masonry sits outboard of the windows by up to a few inches (detailing is not worked through at this stage)

Partially recessed balconies with glass and aluminum guardrails

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Brick piers, norman size brick below the fourth floor, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Dark color aluminum storefront system on a curb with granite or similar high-durability, quality finish material

Bird friendly glass treatment at glass panels facing the landscaped roof deck

Glass screen wall at the rooftop amenity deck. Include bird friendly glass treatment due to landscaping at amenity deck

Floor to ceiling window wall system with color coordinated slab edge covers - system segments at each vertical at the curves

Partially recessed balconies with glass and aluminum guardrails

Bird friendly glass treatment at all glass balcony railings

Bird friendly glass treatment at all glass panes at or below +40'-0"

Painted angled columns

Alternate color brick at floors 1-3 in recessed areas - slightly darker, norman size bricks this area only



Bird friendly glass treatment at glass panels facing the landscaped roof deck

Glass screen wall at the rooftop amenity deck. Include bird friendly glass treatment due to landscaping at amenity deck

Partially recessed balconies with glass and aluminum guardrails

Bird friendly glass treatment at all glass balcony railings

Floor to ceiling window wall system with color coordinated slab edge covers - system segments at each vertical at the curves

Bird friendly glass treatment at all glass panes at or below +40'-0"

Painted angled columns

Dark color aluminum storefront system on a curb with granite or similar high-durability, quality finish material

Brick at upper level walls and parapets to match wall below

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

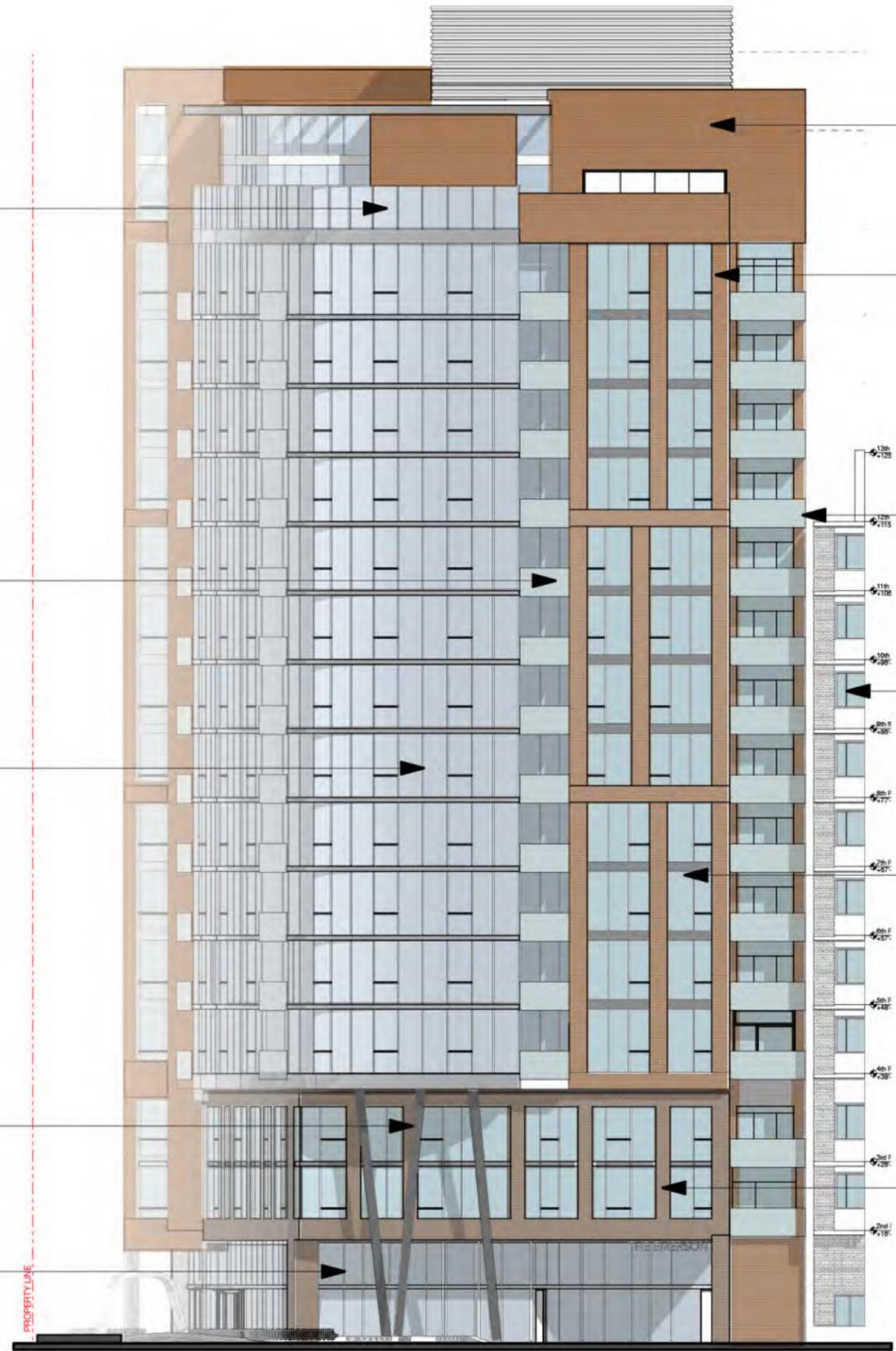
Partially recessed balconies with glass and aluminum guardrails

Existing Jane Perlman building to north to remain

Brick piers, utility-size brick, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Transition from masonry to window is expected to include a metal fin color matched to the window system so the masonry sits outboard of the windows by up to a few inches (detailing is not worked through at this stage)

Alternate color brick at floors 1-3 in recessed areas - slightly darker, norman size bricks



Brick or alternate complimentary material above roof of adjacent building only

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Partially recessed balconies with glass and aluminum guardrails

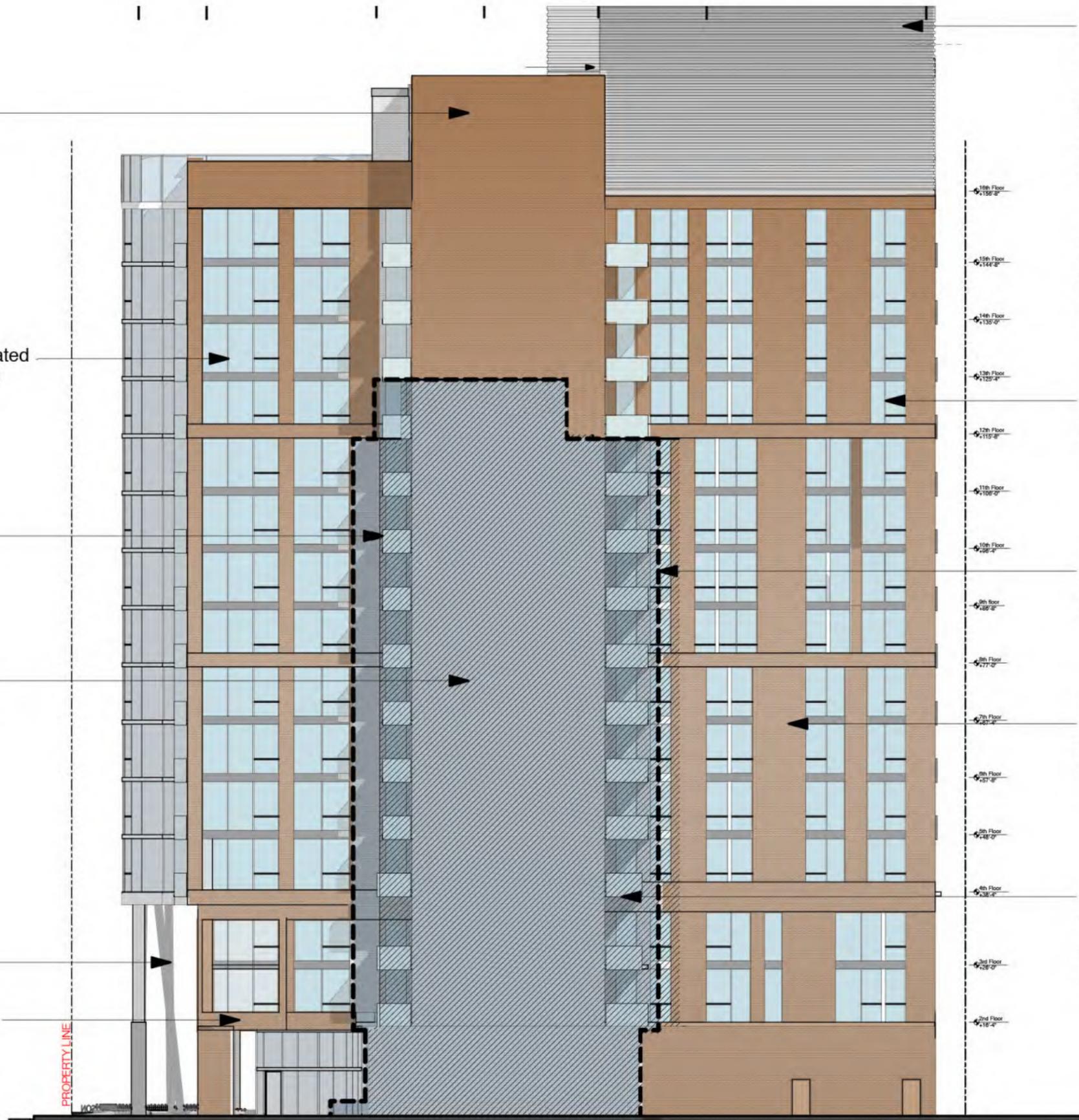
Bird friendly glass treatment at all glass balcony railings

CMU at portion concealed by the tower next door (brick is not possible due to proximity and wall will not be visible below the roofline of the building next door)

Bird friendly glass treatment at all glass panes at or below +40'-0"

Painted angled columns

Alternate color brick at floors 1-3 in recessed areas - slightly darker, norman size bricks this area only



Screen wall at mechanical space to be at the amenity level and approximately 15' to 20' above the roof deck

Screen to be high quality corrugated anodized aluminum panels. Dark grey color to compliment window and other metal detailing . Perforation in some areas if required by equipment for air flow

Screen construction and equipment specs to be evaluated by an acoustical engineer to meet the city requirement to keep the noise levels below 55db as heard from street level

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Existing Jane Perlman building to north to remain (shown with a heavy dash)

Brick piers, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood - norman size floors 1-3, utility size brick above

Partially recessed balconies with glass and aluminum guardrails

Bird friendly glass treatment at all glass balcony railings

Screen wall at mechanical space to be at the amenity level and approximately 15' to 20' above the roof deck

Screen to be high quality corrugated anodized aluminum panels. Dark grey color to compliment window and other metal detailing . Perforation in some areas if required by equipment for air flow

(Maximum height approximately +192', currently shown at approximately +189'-6")

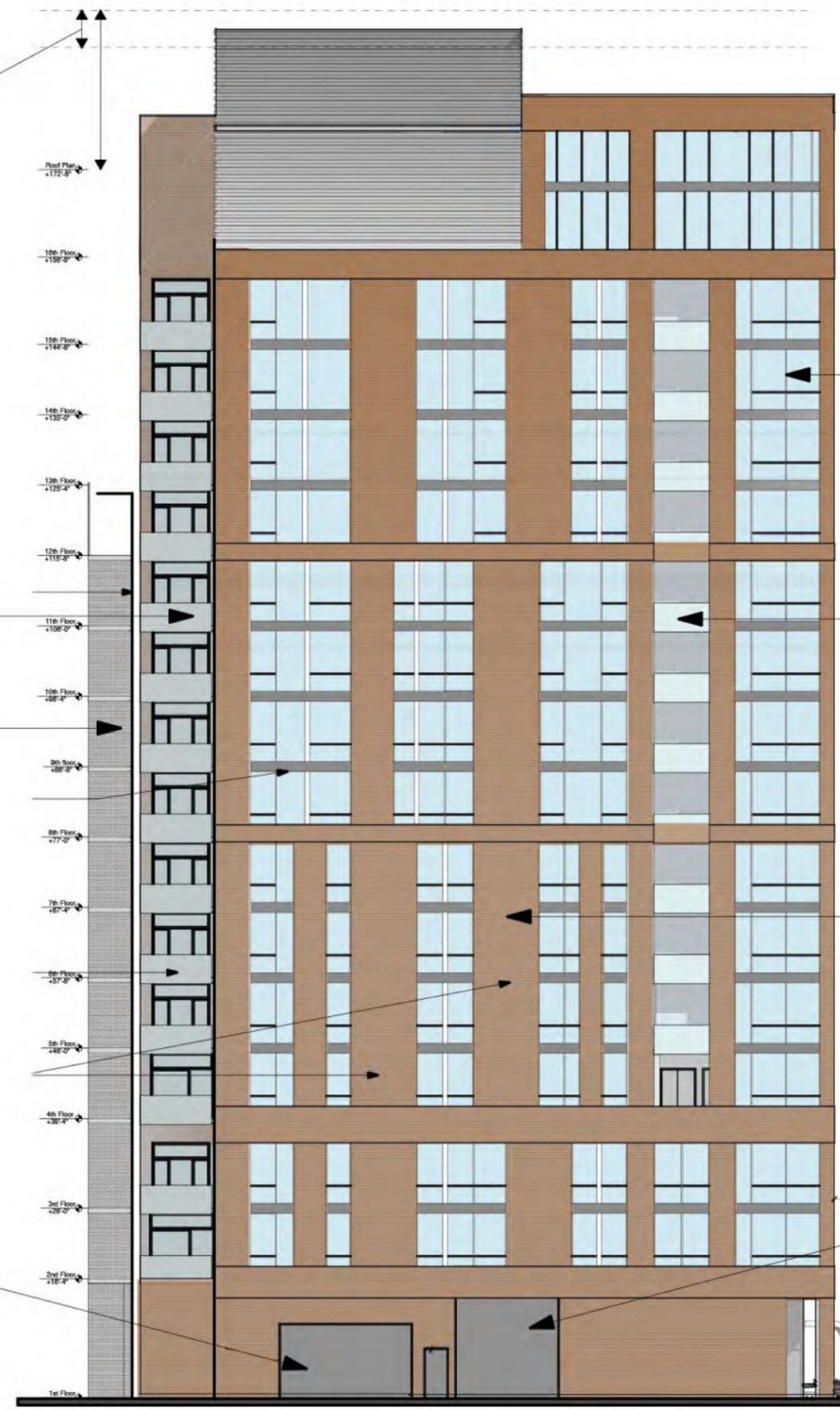
Bird friendly glass treatment at all glass balcony railings

Partially recessed balconies with glass and aluminum guardrails

Existing Jane Perlman building to north to remain

Bird friendly glass treatment at all glass panes at or below +40'-0"

High-speed rolling door with polycarbonate window slats and dark anodized grey aluminum to compliment brick and metal detailing - brick returns at jambs with color coordinated trims as needed



Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Partially recessed balconies with glass and aluminum guardrails

Brick piers, utility-side brick, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Rolling door with polycarbonate window slats and dark anodized grey aluminum to compliment brick and metal detailing - brick returns at jambs with color coordinated trims as needed

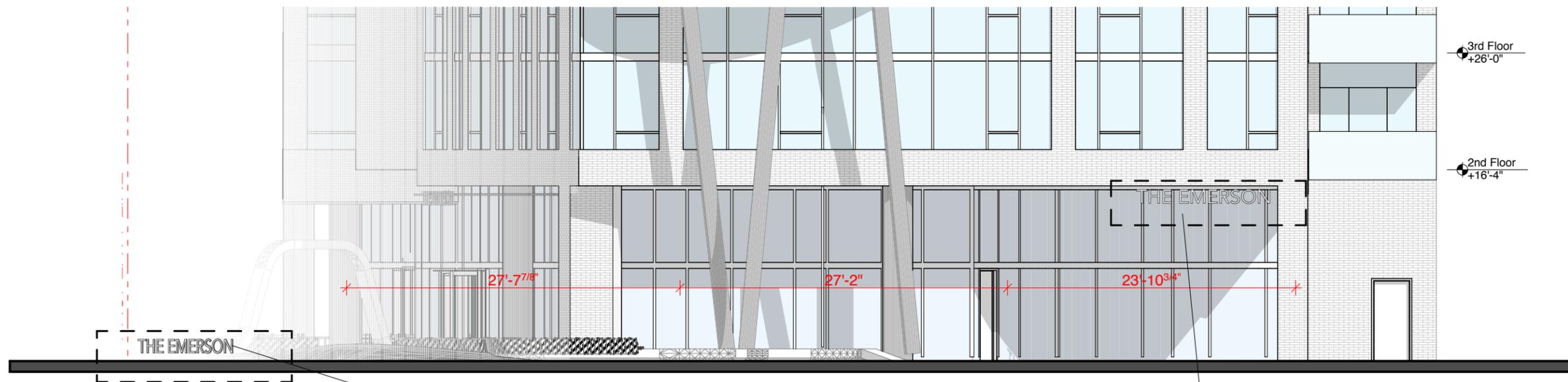


South Elevation

Proposed signage location 1 - refer to the next page for a more detailed elevation and notes

Proposed signage location 3 - set at angle in plan, see note below

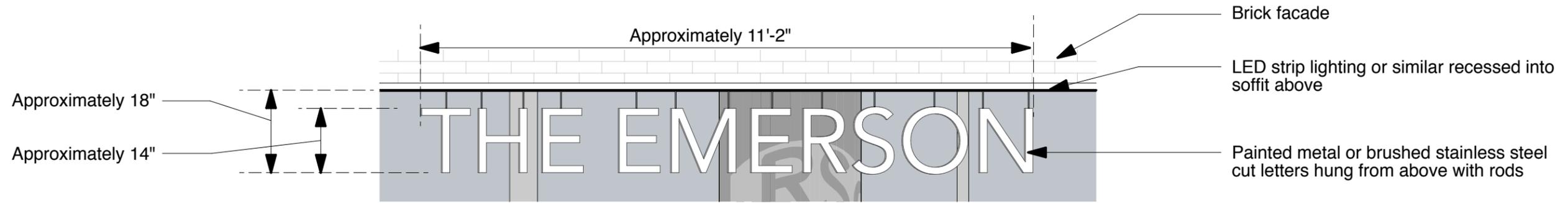
Note: Refer to following page for additional information for each signage location



East Elevation

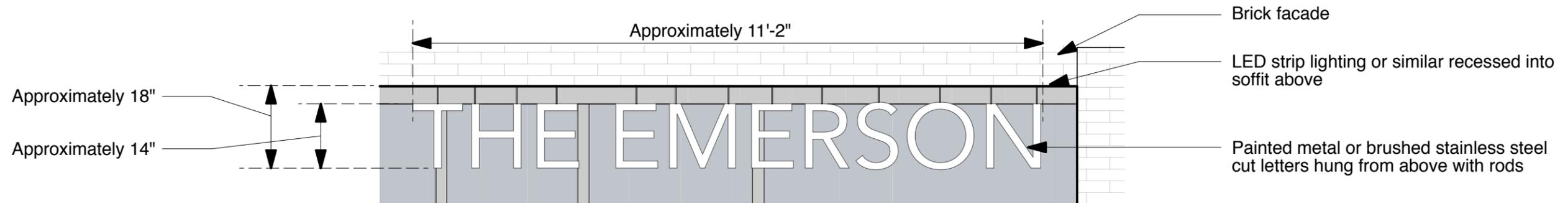
Proposed signage location 3 - refer to the next page for a more detailed elevation and notes and refer to the site plan for approximate location - to be coordinated with the landscape design

Proposed signage location 2 - refer to the next page for a more detailed elevation and notes

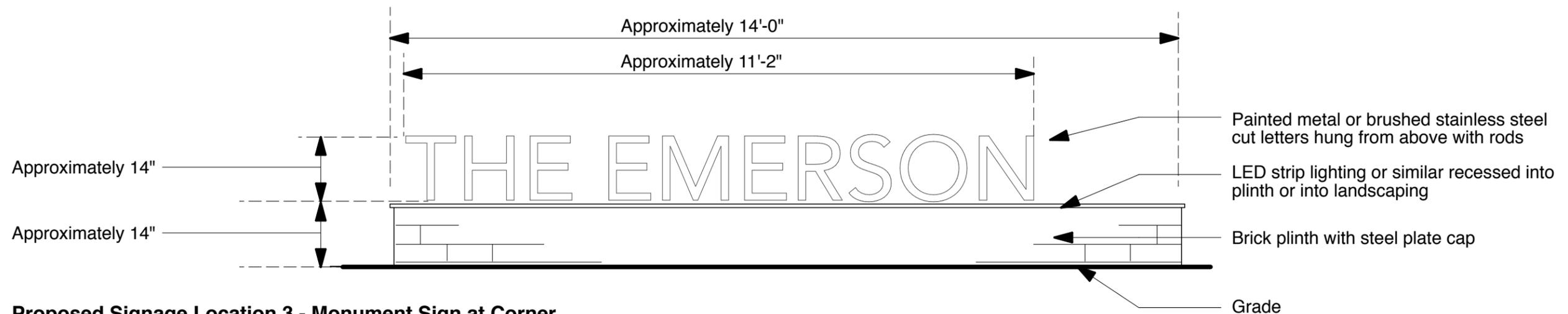


Note: The name and font are preliminary and will be updated once the branding is completed

Proposed Signage Location 1 - Suspended Sign at Sherman Entry



Proposed Signage Location 2 - Suspended Sign at Emerson Entry



Proposed Signage Location 3 - Monument Sign at Corner



Adjacent apartment building - The Link Evanston

Screen for mechanical equipment and elevator override

Rooftop amenity deck (refer to landscape plans for preliminary design)

Adjacent Jane Perlman building

Sherman Gardens

Porte cochere / drop off

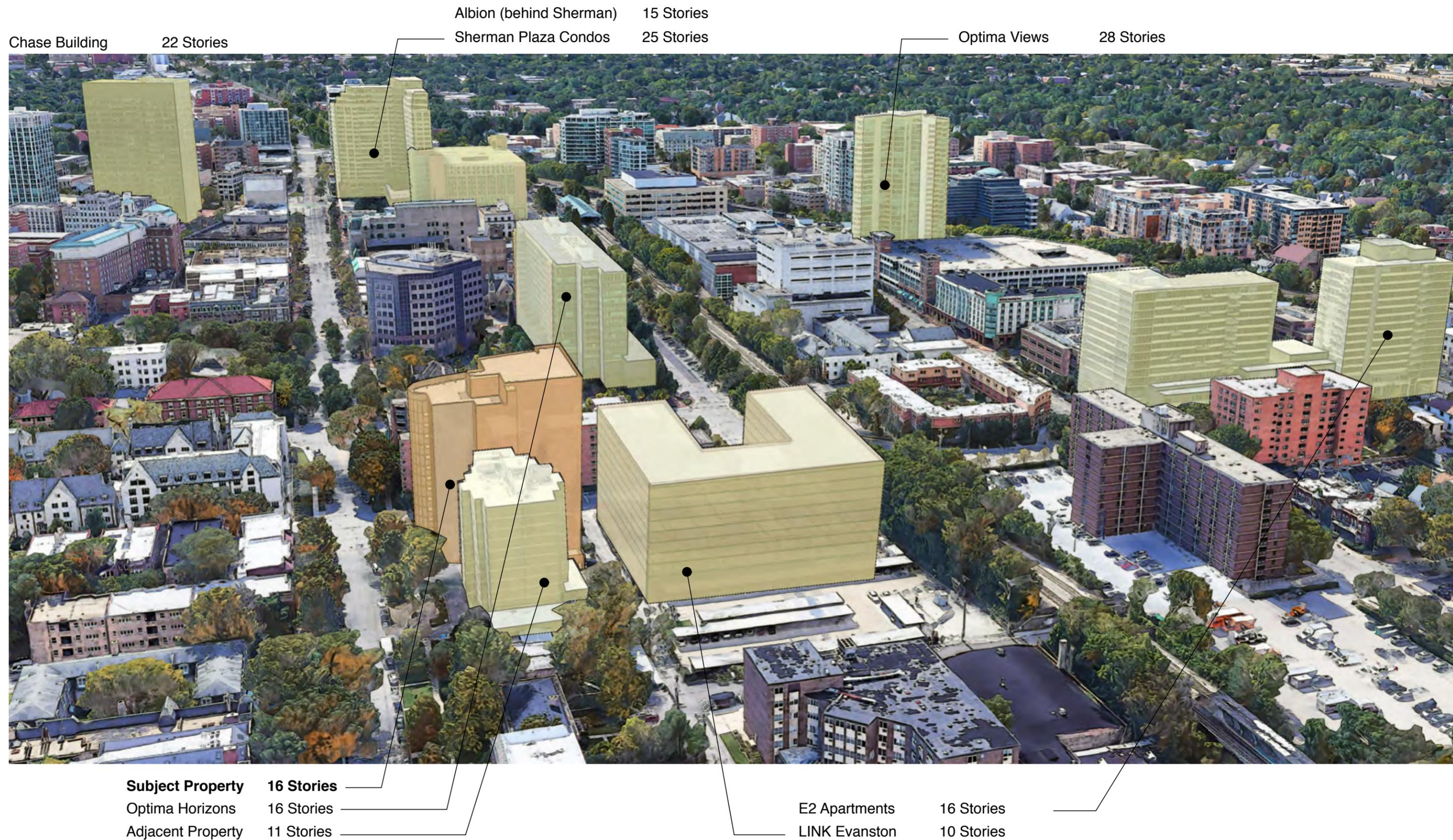
Existing tree to remain

Setback at lower floors (dashed)

Semi-public landscaping at corner

Note: as this is a diagram to identify components of the project, most trees and landscaping are not shown in full

Massing diagram to show primary components of the proposal



Context diagram to compare proposed project with other towers in the vicinity





Brick piers, utility-side brick, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Transition from masonry to window is expected to include a metal fin color matched to the window system so the masonry sits outboard of the windows by up to a few inches (detailing is not worked through at this stage)

Partially recessed balconies with glass and aluminum guardrails

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Dark color aluminum storefront system on a curb with granite or similar high-durability, quality finish material

Glass screen wall at the rooftop amenity deck

Floor to ceiling window wall system with color coordinated slab edge covers - system segments at each vertical at the curves

Partially recessed balconies with glass and aluminum guardrails

Alternate color brick at floors 1-3 in recessed areas - slightly darker and utility or norman size bricks this area only

Painted angled columns







Brick piers, utility-side brick, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Partially recessed balconies with glass and aluminum guardrails

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Painted angled columns

Alternate color brick at floors 1-3 in recessed areas - slightly darker and utility or norman size bricks this area only









Brick facades utility-size, custom blend - exact color to be determined - intent is to compliment the brick colors in the immediate neighborhood

Screen wall at mechanical space. Screen to be high quality corrugated anodized aluminum panels. Dark grey color to compliment window and other metal detailing . Perforation in some areas if required by equipment for air flow

Floor to ceiling window wall with color coordinated slab edges (exposed smooth with paint or with slab edge covers)

Existing Jane Perlman building to north to remain

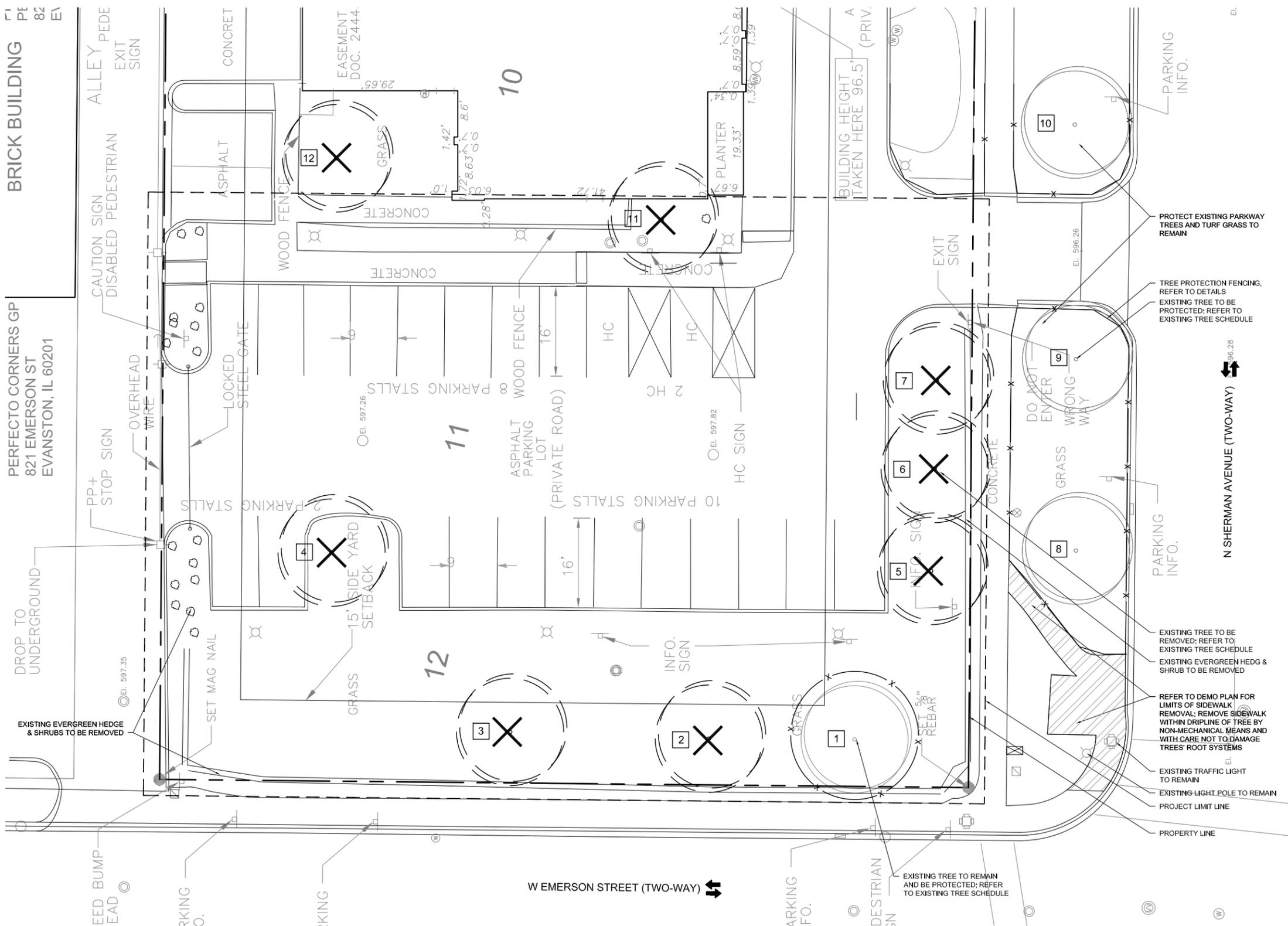




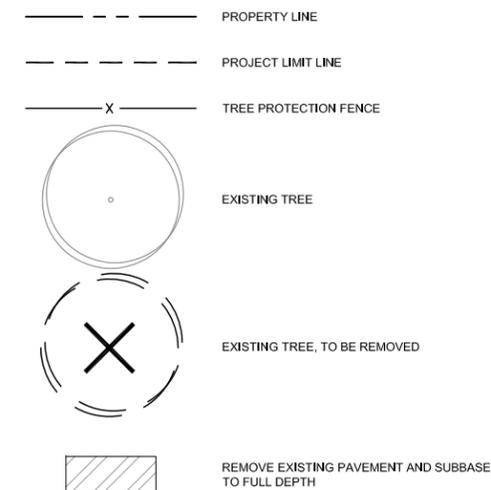
EXHIBIT E

Landscape Plans

1 TREE PRESERVATION AND REMOVAL PLAN
1" = 20'-0"



LEGEND



PLANTING PROTECTION AND REMOVAL NOTES

1. PRIOR TO THE COMMENCEMENT OF PLANTING PROTECTION AND REMOVAL SCOPE OF WORK, CONVENE A PRE-CONSTRUCTION CONFERENCE WITH AOR, LA, CONTRACTOR AND ANY ASSOCIATED AND RELEVANT DESIGN TEAM MEMBERS, SUBCONTRACTORS AND AHJ TO REVIEW WORK SCOPE, SCHEDULE AND TO REVIEW Q&A BY DESIGN TEAM, AHJ AND CONTRACTORS. CONTRACTOR TO PROVIDE MINIMUM 5 BUSINESS DAY NOTICE FOR ALL MEETINGS.
2. CONFIRM ALL PLANTING REMOVAL AND PROTECTION QUANTITIES AND DIMENSIONS.
3. INSTALL PLANT PROTECTION FENCING PRIOR TO SITE WORK AND MAINTAIN THROUGHOUT CONSTRUCTION PERIOD. NO STORAGE OF MATERIALS, VEHICULAR ACCESS OR ANY OTHER CONSTRUCTION ACTIVITIES PERMITTED WITHIN THE TREE PROTECTION ZONE UNO.
4. ALL PLANT AND SITE PROTECTION FENCING TO BE INSTALLED PRIOR TO HARDSCAPE DEMOLITION AND/OR CONSTRUCTION OPERATIONS.
5. ADJUST PLANT PROTECTION FENCING AS NEEDED THROUGHOUT CONSTRUCTION OPERATION TO PROTECT CRITICAL ROOT ZONES OF ALL TREES.
6. MARK ALL ROOT PRUNING LOCATIONS PRIOR TO EXECUTION FOR REVIEW AND APPROVAL BY LA.
7. PROVIDE 2 INCH (AFTER SETTLING) MULCH LAYER WITHIN CRITICAL ROOT ZONE OF EXISTING TREES AS MIN PROTECTION DURING CONSTRUCTION OPERATIONS. COORDINATE WITH AOR AND LA FOR ADDITIONAL MEASURES IF CRITICAL ROOT ZONES NEED TO BE ACCESSED FOR ANY REASON DURING CONSTRUCTION OPERATIONS. MULCH LAYER TO BE A 50/50 BLEND OF 3/4 INCH SHREDDED HARDWOOD BARK FINES AND 1/4 INCH PINE BARK FINES. VERIFY WHICH EXISTING TREES TO REQUIRE 2" MULCH LAYER WITH LANDSCAPE ARCHITECT.
8. PROVIDE TEMPORARY IRRIGATION FOR ALL EXISTING PLANT MATERIAL AND MAINTAIN THROUGHOUT CONSTRUCTION OPERATIONS. PROVIDE IN SUCH A MANNER AS TO NOT IMPACT EXISTING ROOT SYSTEMS OF PLANT MATERIAL.
9. REMOVE ALL EXISTING TREE AND SHRUB STUMPS AND ROOTS THAT IMPACT WORK. GRIND OUT STUMPS TO A MINIMUM DEPTH OF 18". BRING TO FINISH GRADE PRIOR TO COMPLETION OF WORK.
10. REMOVE MULCH, LAWN, AND PLANTING BEDS AND EXCAVATE EARTH TO COORDINATE WITH GRADING, EARTHWORK AND SITE IMPROVEMENTS.
11. FOR PLANTING REMOVAL AND PROTECTION DETAILS, SEE NEXT PAGE.
12. FOR PLANTING REMOVAL AND PROTECTION SCHEDULE, SEE NEXT PAGE.

EXISTING TREE SCHEDULE								
TAG	BOTANICAL NAME	COMMON NAME	SIZE	COND.	FORM	REMOVE	ROOT PRUNE	REMARKS
1	<i>Picea pungens</i>	Colorado Blue Spruce	18" cal	4	4			See note 1
2	<i>Malus spp.</i>	Crabapple	24" cal	2	2	X		minor deadwood
3	<i>Malus spp.</i>	Crabapple	22" cal	2	2	X		minor deadwood
4	<i>Acer saccharinum</i>	Silver Maple	54" cal	3	4	X		girdling roots; poor pruning; some deadwood in upper canopy
5	<i>Crataegus spp.</i>	Hawthorn	2 x 9" cal	2	2	X		thorned
6	<i>Crataegus spp.</i>	Hawthorn	1 x 8", 1 x 7" cal	2	2	X		thorned
7	<i>Crataegus spp.</i>	Hawthorn	2 x 7", 2 x 5" cal	2	2	X		thorned
8	<i>Aesculus hippocastanum</i>	Horse Chestnut	26" cal	2	2			
9	<i>Aesculus hippocastanum</i>	Horse Chestnut	28" cal	2	1			
10	<i>Aesculus hippocastanum</i>	Horse Chestnut	24" cal	2	3			
11	<i>Acer tataricum subsp. ginnala</i>	Amur Maple	1 x 12", 2 x 6" cal	2	2	X		
12	<i>Cornus spp.</i>	Dogwood	7" cal	3	4	X		poor pruning

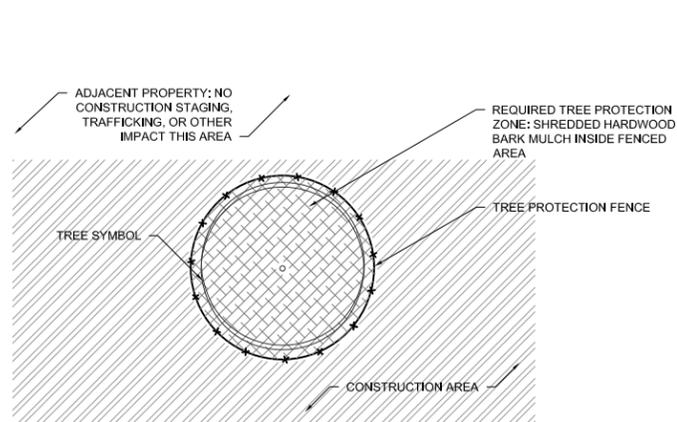
NOTES:

1. Certified Arborist Report forthcoming. It is suspected that the Colorado Blue Spruce is in decline. The prevalence of diseases on Colorado Blue Spruce has intensified in recent years and the trees are declining rapidly throughout the Chicagoland area. The key symptom is branch decline, which progresses over 2-4 years and renders both poor appearance and poor health. This tree exhibits substantial branch decline, particularly on its backside. This is often related to one or more diseases or pests: needlecast, tip blight, canker disease, or spruce spider mites.

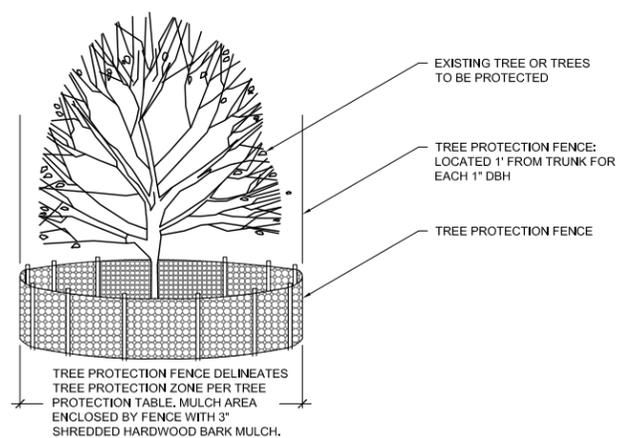
Tree Condition. A visual assessment and rating of the overall health and vigor of the tree, as follows:

- 1 - Excellent
- 2 - Very Good
- 3 - Good
- 4 - Below Average
- 5 - Poor
- 6 - Dead

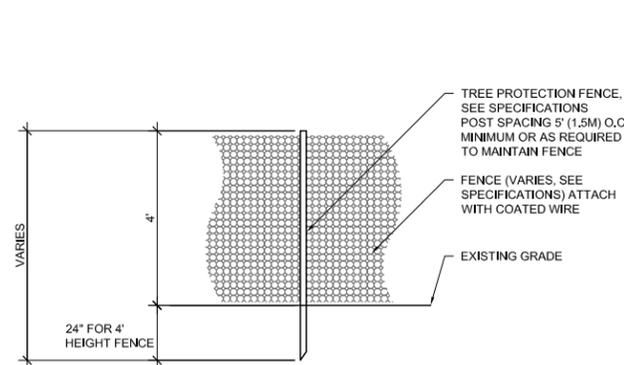
1 EXISTING TREE SCHEDULE
NTS



2 TREE PROTECTION ENTIRELY WITHIN CONSTRUCTION AREA PLAN
NOT TO SCALE



2A TREE PROTECTION ENTIRELY WITHIN CONSTRUCTION AREA ELEVATION
NOT TO SCALE



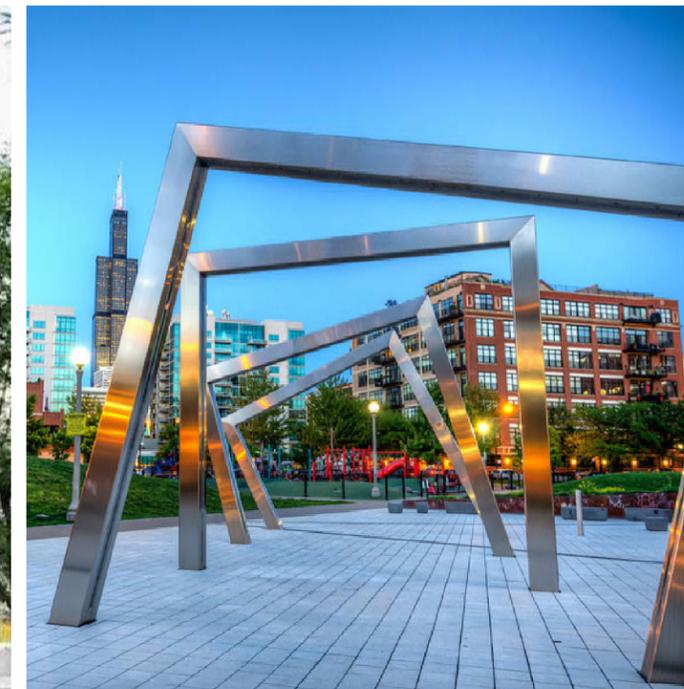
3 TREE PROTECTION FENCE
NOT TO SCALE

TREE PROTECTION	FLAG TAPE COLOR	RADIUS OF TREE PROTECTION FENCE FROM TRUNK	MULCH TREE PROTECTION ZONE
ADJACENT PROPERTY	NONE	NONE. TREE PROTECTED BY TREE PROTECTION FENCE AT PROJECT LIMITS	NO
TREE WITH TREE PROTECTION FENCE	GREEN	1' (30 cm) RADIUS FOR EACH 1" (2.54 cm) DBH*, OR AS APPROVED BY AOR OR PROJECT ARBORIST	YES, 3" DEPTH OF HARDWOOD BARK MULCH, NOT IN CONTACT WITH THE TRUNK
TREE WITH TREE PROTECTION FENCE, ROOTPRUNED	GREEN & YELLOW	1' (30 cm) RADIUS FOR EACH 1" (2.54 cm) DBH*, OR AS APPROVED BY AOR OR PROJECT ARBORIST	YES, 3" DEPTH OF HARDWOOD BARK MULCH, NOT IN CONTACT WITH THE TRUNK
TREE TO BE RELOCATED	BLUE	1' (30 cm) RADIUS FOR EACH 1" (2.54 cm) DBH*, OR AS APPROVED BY AOR OR PROJECT ARBORIST	YES, 3" DEPTH OF HARDWOOD BARK MULCH, NOT IN CONTACT WITH THE TRUNK
TREE TO BE REMOVED	RED	NONE. TREES MUST BE REMOVED PRIOR TO START OF CONSTRUCTION	NO

*DBH - DIAMETER AT BREAST HEIGHT MEASURED AT 4.5' FROM BASE OF TREE.

4 TREE PROTECTION TABLE AND NOTES
NOT TO SCALE

1900 SHERMAN AVE, EVANSTON, ILLINOIS
LANDSCAPE PD PACKAGE Project Number: 8639
May 13, 2020



CONNECTIVITY



1900 SHERMAN AVE, EVANSTON, ILLINOIS

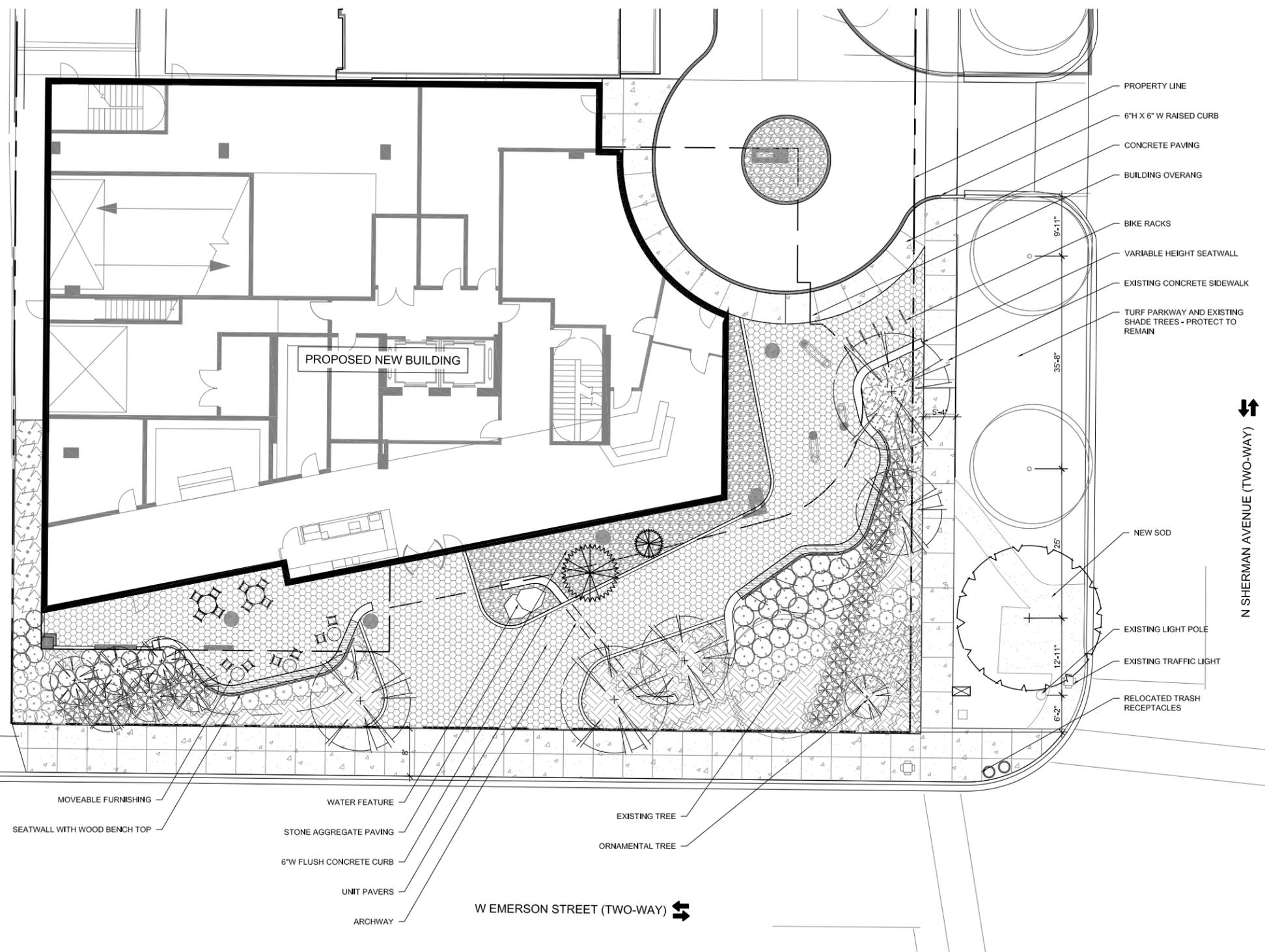
LANDSCAPE PD PACKAGE Project Number: 8639
May 13, 2020



LEGEND:

- 1** RIBBON BENCH
- 2** CAFE SEATING
- 3** UNIT PAVERS
- 4** ROCK GARDEN
- 5** WATER FEATURE
- 6** RIBBON GATEWAY W/ LIGHTING
- 7** TREES / SHRUBS
- 8** EXISTING BLUE SPRUCE
- 9** PROPOSED BUILDING COLUMNS
- 10** BIKE RACKS
- 11** SODDED PARKWAY WITH EXISTING TREES

1 RENDERED SITE PLAN
1" = 20'-0"

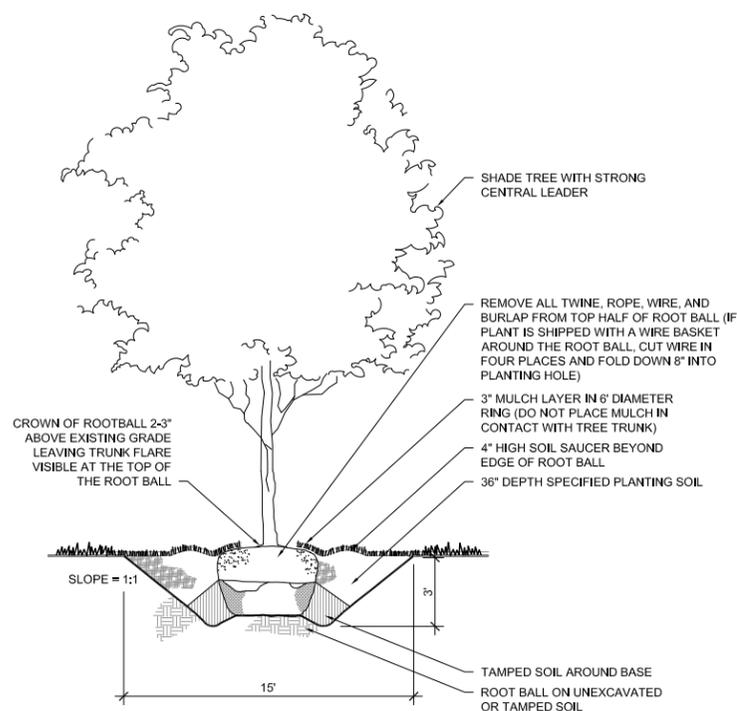


LEGEND

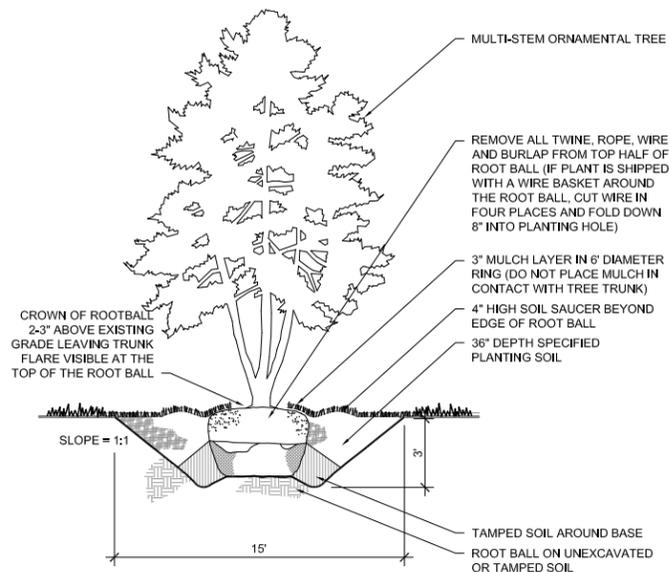
	PROPERTY LINE
	CONCRETE PAVING
	GROUNDCOVER/ PERENNIALS
	UNIT PAVERS
	STONE AGGREGATE PAVING
	SOD
	6"W FLUSH CONCRETE CURB
	6"H X 6"W RAISED CONCRETE CURB
	EXPANSION JOINT
	CONTROL JOINT
	SHRUBS
	EXISTING TREE
	SHADE TREE
	ORNAMENTAL TREE
	EVERGREEN TREE

1 OVERALL LANDSCAPE SITE PLAN
1" = 20'-0"

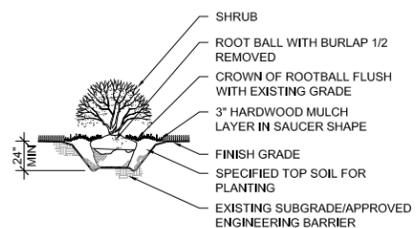




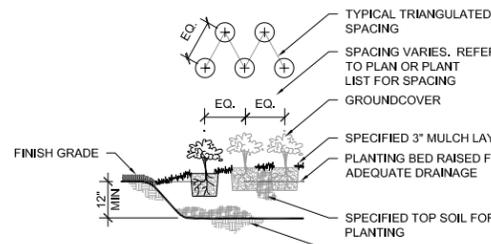
1 SHADE TREE PLANTING SECTION
NOT TO SCALE



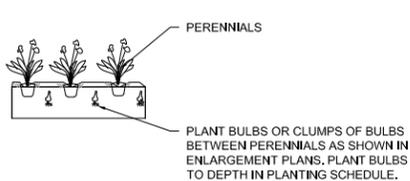
2 ORNAMENTAL TREE PLANTING SECTION
NOT TO SCALE



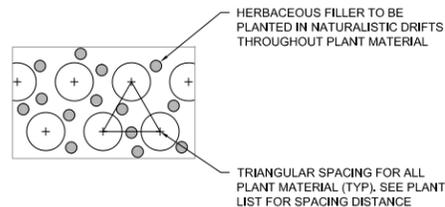
3 SHRUB PLANTING SECTION
NOT TO SCALE



4 PERENNIAL PLANTING SECTION
NOT TO SCALE



5 BULB INSTALLATION DETAIL
NOT TO SCALE



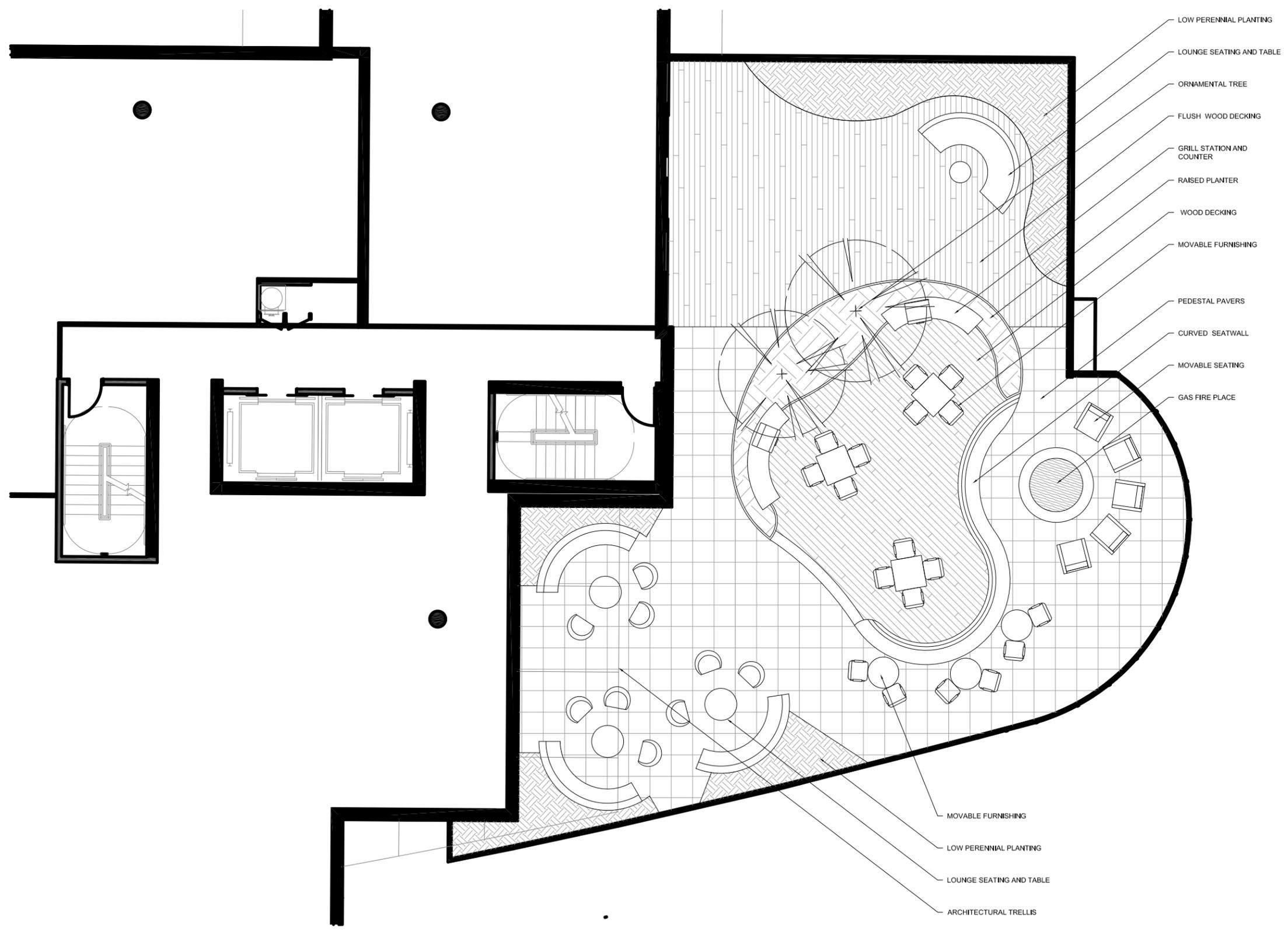
6 INTERPLANTING DETAIL
NOT TO SCALE

PLANT SCHEDULE

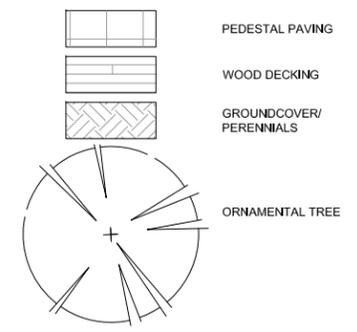
PLANT SCHEDULE	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE	REMARKS
SHADE TREES							
AE.FM	1		<i>Aesculus x carnea</i> 'Fort McNair'	Fort McNair Red Horsechestnut	B&B	4" cal	min. 7' clear
BE.WH	6		<i>Betula populifolia</i> 'Whitespire'	Whitespire Birch	B&B	2" cal	matching
ORNAMENTAL TREES							
AMAB	1		<i>Amelanchier x grandiflora</i> 'Autumn Brilliance'	Autumn Brilliance Serviceberry	B&B	12' ht	multistem; 5-7 cane; min. 7' clear
CE.12	1		<i>Cercis canadensis</i>	Eastern Redbud	B&B	12' ht	multistem; northern strain only; 3-5 separate canes
CE.14	1		<i>Cercis canadensis</i>	Eastern Redbud	B&B	14' ht	multistem; northern strain only; 3-5 separate canes
CE.16	1		<i>Cercis canadensis</i>	Eastern Redbud	B&B	16' ht	multistem; northern strain only; 3-5 separate canes
MA.JA	1		<i>Magnolia</i> 'Jane'	Jane Magnolia	B&B	8' ht	multistem
EVERGREEN TREES							
PLUM	1		<i>Pinus densiflora</i> 'Umbraculifera'	Tanyosho Japanese Red Pine	B&B	8' ht	specimen
PLUN	1		<i>Pinus densiflora</i> 'Umbraculifera Nana'	Dwarf Tanyosho Japanese Red Pine	B&B	5' ht	specimen
SHRUBS							
CO.CA	25		<i>Cornus sanguinea</i> 'Cato'	Arctic Sun Dogwood	pot	24" ht	36" o.c.
DILLO	16		<i>Diervilla lonicera</i>	Dwarf Bush Honeysuckle	pot	24" ht	30" o.c.
HY.LI	9		<i>Hydrangea paniculata</i> 'Limelight'	Limelight Panicle Hydrangea	pot	36" ht	36" o.c.
HY.BR	24		<i>Hydrangea quercifolia</i> 'Brenthil'	Gatsby Gal Oakleaf Hydrangea	pot	36" ht	42" o.c.
IT.SP	25		<i>Itea virginica</i> 'Sprich'	Little Henry Virginia Sweetspire	pot	24" ht	30" o.c.
RH.GL	14		<i>Rhus aromatica</i> 'Grow-Low'	Grow-Low Sumac	pot	18" ht	30" o.c.
SP.TO	22		<i>Spiraea betulifolia</i> 'Tor'	Birchleaf Spirea	pot	15" ht	30" o.c.
VLCA	8		<i>Viburnum x caricephalum</i> 'Cayuga'	Cayuga Koreanspice Viburnum	pot/B&B	42" ht	48" o.c.
EVERGREEN SHRUBS							
TA.EV	57		<i>Taxus x media</i> 'Everlow'	Everlow Yew	pot	24" w	30" o.c.
ORNAMENTAL GRASSES, PERENNIALS, AND GROUND COVER							
AL.SB	21		<i>Allium</i> 'Summer Beauty'	Summer Beauty Allium	pot	#1	15' o.c.
AG.BF	8		<i>Agastache</i> 'Blue Fortune'	Blue Fortune Anise Hyssop	pot	#1	18" o.c.
AM.HU	15		<i>Amsonia hubrichtii</i>	Narrow Leaf Blue Star	pot	#1	24" o.c.
AN.CA	33		<i>Anemone canadensis</i>	Meadow Anemone	pot	#1	20" o.c.
AS.SY	5		<i>Asclepias syriaca</i>	Common Milkweed	pot	#1	15" o.c.
AS.TU	8		<i>Asclepias tuberosa</i>	Butterfly Weed	pot	#1	15" o.c.
BA.TW	2		<i>Baptisia x varicolor</i> 'Twilite'	Twilite False Indigo	pot	#1	24" o.c.
CANE	15		<i>Calamintha nepeta ssp. nepeta</i>	Lesser Calamint	pot	#1	18" o.c.
CA.FL	74		<i>Carex flacca</i>	Blue Sedge	pot	quart	15" o.c.
EC.PS	15		<i>Echinacea purpurea</i> 'Prairie Splendor'	Prairie Splendor Coneflower	pot	#1	18" o.c.
GE.BV	142		<i>Geranium macrorrhizum</i> 'Bevan's Variety'	Bevan's Bigroot Geranium	pot	quart	18" o.c.
LI.KO	6		<i>Liatris spicata</i> 'Kobold'	Kobold Blazing Star	pot	#1	15" o.c.
LY.AU	36		<i>Lysimachia nummularia</i> 'Aurea'	Golden Moneywort	pot	2.5' cell	10" o.c.
MO.BR	10		<i>Monarda bradburiana</i>	Eastern Beebalm	pot	#1	18" o.c.
SE.AU	19		<i>Sesleria autumnalis</i>	Autumn Moor Grass	pot	#1	15" o.c.
SY.PD	9		<i>Symphoricarpon novae-angliae</i> 'Purple Dome'	Purple Dome Aster	pot	#1	15" o.c.
WATE	105		<i>Waldsteinia ternata</i>	Barren Strawberry	pot	2.5' cell	10" o.c.
BULBS							
AL.GL			<i>Allium</i> 'Globemaster'	Globemaster Allium	bulb	20 cm	12-16" o.c.; plant in drifts
AL.ME			<i>Allium</i> 'Mount Everest'	Mount Everest Allium	bulb	18 cm	12-16" o.c.; plant in drifts
CR.HC			<i>Crocus</i> 'Hokus Crocus' Blend	Hokus Crocus Crocus Blend	bulb	10 cm	2-4" o.c.; plant in drifts
GA.NI			<i>Gaianthus nivalis</i>	Snowdrop	bulb	8 cm	2-4" o.c.; plant in drifts
NA.3D			<i>Narcissus</i> '3D' Blend	3D Blend Daffodi	bulb	12 cm	8-12" o.c.; plant in drifts

PLANTING NOTES

- PRIOR TO THE COMMENCEMENT OF PLANTING SCOPE OF WORK, CONVENE A PRE-CONSTRUCTION CONFERENCE WITH AOR, LA, CONTRACTOR AND ANY ASSOCIATED AND RELEVANT DESIGN TEAM MEMBERS. SUBCONTRACTORS AND A/J TO REVIEW WORK SCOPE, SCHEDULE AND TO REVIEW Q&A BY DESIGN TEAM, A/J AND CONTRACTORS. CONTRACTOR TO PROVIDE MINIMUM 5 BUSINESS DAY NOTICE FOR ALL MEETINGS.
- CONFIRM ALL PLANTING QUANTITIES AND DIMENSIONS. QUANTITIES PROVIDED ARE FOR THE CONVENIENCE OF THE CONTRACTOR. CONTRACTOR TO VERIFY IMPLIED COVERAGE ON THE PLANTING PLANS, CONTRACTOR TO BRING ANY DISCREPANCIES TO THE ATTENTION OF THE LA AT THE TIME OF BID.
- ALL PLANT MATERIAL TO BE INSTALLED IN CONFORMANCE WITH ALL APPLICABLE CODES AND ORDINANCES.
- OBTAIN A PERMIT TO REMOVE, SPRAY OR OTHERWISE AFFECT THE GENERAL HEALTH OR STRUCTURE OF EXISTING OR NEW TREES AND/OR OTHER PLANT MATERIAL.
- EXISTING PARKWAY AND INTERIOR TREES/FENCING TO BE PROTECTED WHILE PROJECT IS UNDER CONSTRUCTION AND WILL BE REPLACED IF DAMAGED BY CONTRACTOR.
- ALL PLANT MATERIAL TO BE SPECIMEN QUALITY, WELL MATCHED IN FORM, NURSERY GROWN, SOUND, HEALTHY, VIGOROUS AND FREE OF INSECTS, DISEASE AND INJURIES WITH HABIT OF GROWTH THAT IS TYPICAL FOR THE SPECIES. SIZES SHALL BE EQUAL TO OR GREATER THAN THOSE NOTED ON THE PLANT LIST(S). DO NOT PROCEED WITH PLANTING INSTALLATION UNLESS LA HAS APPROVED ALL PLANT MATERIAL. ALL B&B MATERIAL TO BE OBTAINED FROM NURSERIES WITH SIMILAR SOIL CONDITIONS AS THE PROJECT SITE.
- PROVIDE AOR AND LA WITH PLANT LIST AND INTENDED NURSERY SUPPLIERS FOR EACH ITEM TO LA WITHIN 30 DAYS OF AWARD OF CONTRACT. ANY UNAVAILABLE PLANT MATERIAL SHOULD BE NOTED AT THAT TIME. AOR AND LA RESERVES THE RIGHT TO ASSIST IN PLANT SOURCING IF NECESSARY. PROVIDE AOR AND LA WITH SUBSTITUTION LIST IN THE EVENT PROPOSED MATERIALS ARE UNAVAILABLE PRIOR TO PROCUREMENT FOR FINAL REVIEW AND APPROVAL.
- COORDINATE NURSERY SELECTION AND TAGGING WITH AOR AND LA. AOR AND LA RESERVES THE RIGHT TO ACCEPT OR REJECT ANY AND ALL PLANT MATERIAL AS IT RELATES TO THE REQUIREMENTS IN THE SPECIFICATIONS. AOR AND LA RESERVES THE RIGHT TO REJECT PLANT MATERIAL IF DAMAGED OR UNHEALTHY UPON ARRIVAL TO THE PROJECT SITE EVEN AFTER INITIAL APPROVAL.
- PROVIDE IDENTIFICATION TAG FROM THE SUPPLYING NURSERY SHOWING COMMON AND BOTANICAL PLANT NAMES FOR AT LEAST ONE PLANT OF EACH UNIQUE SPECIES DELIVERED TO THE SITE.
- CONFIRM PLANTING AREAS ARE MIN 4 INCHES BELOW FINISH FLOOR ELEVATION UNO.
- FINISHED GRADE OF PLANTING AREAS TO MEET ADJACENT HARDSCAPES 2 INCHES BELOW ELEVATION OF HARDSCAPES UNO.
- IF PLANTING PITS ARE MACHINE EXCAVATED AND GLAZING IS PRONOUNCED, SCARIFY ALL SIDES.
- REMOVE ALL ROCK AND DEBRIS 1 INCH AND LARGER FROM PLANTING AREAS, LEGALLY DISPOSE ALL EXCESS MATERIALS RESULTING FROM THE WORK.
- WITHIN PLANTING EXCAVATIONS, REMOVE CRUSHED AGGREGATE TO AN ADEQUATE DEPTH TO ENSURE THAT NO PART OF THE PLANT MATERIAL OR FUTURE EXTENTS OF ROOT SYSTEM IS IN CONTACT OR AFFECTED BY THE LIME OR LIMESTONE WITHIN THE AGGREGATE.
- STAKE ALL TREES AND FLAG ALL SHRUB LOCATIONS FOR REVIEW AND APPROVAL BY AOR AND LA PRIOR TO PLANTING. PROVIDE LA WITH A MINIMUM OF 5 BUSINESS DAYS NOTICE PRIOR TO REVIEW.
- SET ALL TREES AND SHRUBS ON TOP OF FINISHED GRADE PER APPROVED STAKED AND FLAGGED LOCATIONS FOR FACING AND FINAL APPROVAL BY AOR AND LA PRIOR TO PLANTING.
- FOR ALL GRASSES, PERENNIALS, GROUND COVER AND ANNUALS, SET OUT PLANT MATERIAL ON TOP OF PROPOSED GRADE FOR AOR AND LA REVIEW PRIOR TO PLANTING IN ACCORDANCE WITH PLANTING PLANS, AOR AND LA RESERVE THE RIGHT TO MODIFY THE PLANTING AS NEEDED.
- INSTALL ALL PLANT MATERIAL IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS.
- REMOVE ALL PLANT TYING MATERIAL AND MARKING TAPES AT THE TIME OF PLANTING. LEAVE AOR AND LA NURSERY SEALS IN PLACE FOR THE DURATION OF THE SPECIFIED WARRANTY PERIOD.
- FOR B&B MATERIAL, REMOVE TOP 1/3 OF METAL CAGE AND BURLAP FROM ROOT BALL AND PROPERLY DISPOSE. DO NOT FOLD DOWN. CAGE MUST BE REMOVED. IMMEDIATELY REPORT ANY DAMAGE TO ROOT BALLS OR PLANTS TO AOR AND LA.
- ROOT FLARE OF PLANTS TO BE SET MIN AT FINISHED GRADE OR NO MORE THAN 3 INCHES ABOVE FINISHED GRADE.
- ROOT FLARE OF PLANTS INSTALLED IMMEDIATELY ADJACENT TO HARDSCAPE TO BE PLANTED NO MORE THAN 1 INCH ABOVE FINISHED GRADE TO ENSURE SMOOTH TRANSITION TO HARDSCAPE.
- FOR PERENNIALS AND GROUND COVERS, PLANT 1 INCH ABOVE SOIL PROFILE TO ACCOMMODATE MULCH LAYER.
- DURING BACKFILL OF ALL PLANTS (WITH THE EXCEPTION OF SOD AND SEEDED LAWNS) SUPPLEMENT BACKFILL WITH PLANTING SOIL. SUPPLEMENTS AS NOTED IN THE SPECS. REFER TO MANUFACTURER REQUIREMENTS FOR INSTALLATION.
- DO NOT ALLOW AIR POCKETS TO FORM IN SOIL WHILE BACKFILLING.
- TREE STAKING AND GUYING TO BE REVIEWED WITH AOR AND LA WHEN DEEMED NECESSARY PRIOR TO INSTALLATION.
- ALL NEWLY INSTALLED PLANT MATERIAL SHALL BEAR THE SAME RELATIONSHIP TO THE NEW GRADE AS THEY BORE TO NURSERY GRADES UNO.
- WATER AND MAINTAIN PLANT MATERIAL WHILE BEING STORED ON SITE, IMMEDIATELY AFTER PLANTING AND UNTIL FINAL ACCEPTANCE.
- ALL PLANTING BED EDGES TO HAVE A SPADED EDGE UNO, BOTTOM OF SPADED EDGE TO BE 2 INCHES BELOW HATCH OR ADJACENT HARDSCAPE SURFACE.
- PROVIDE 2 INCH (AFTER SETTLEMENT) OF MULCH OVER ALL PLANTING BEDS UPON COMPLETION OF PLANTING OPERATIONS. MULCH TO BE A 50/50 BLEND OF 1/2 INCH SHREDDED HARDWOOD BARK FINES AND 3/4 INCH PINE BARK FINES UNO.
- PRUNING OPERATIONS SHOULD FOLLOW APPLICABLE ANSI STANDARDS UNO. UPON COMPLETION OF WORK UNDER THIS CONTRACT, PRUNE AND REPAIR INJURIES TO ALL PLANTS. LIMIT AMOUNT OF PRUNING TO MINIMUM NECESSARY TO REMOVE INJURED OR DAMAGED BRANCHES, TWIGS AND STEMS AND TO COMPENSATE FOR THE LOSS OF ROOTS AS A RESULT OF TRANSPLANTING OPERATIONS. PRUNE IN SUCH A MANNER AS NOT TO CHANGE NATURAL HABIT OR SHAPE UNO. ALL HEDGE PRUNING TO BE COMPLETED UNDER THE REVIEW BY AOR AND LA.
- DURING PUNCH LIST REVIEW, COORDINATE WITH AOR AND LA TO REVIEW AESTHETIC PRUNING NEEDS FOR ALL PLANTS. AOR AND LA TO MARK (WITH YELLOW RIBBON) OR SELF CONDUCT AESTHETIC PRUNING WITH CONTRACTOR AS PART OF THE SUBSTANTIAL COMPLETION REVIEW.
- WARRANTY ALL PLANT MATERIAL FOR A MIN PERIOD OF 1 YEAR FROM FINAL ACCEPTANCE UNO IN THE SPECIFICATIONS. REMOVE AND REPLACE PLANTS AS DETERMINED BY LA TO BE SIGNIFICANTLY UNHEALTHY OR DEAD PER THE CRITERIA SET FORTH IN THE SPECS. CONDUCT REPLACEMENTS DURING THE APPROPRIATE PLANTING SEASON AS DETERMINED BY THE LOCAL CLIMATE AND INDUSTRY STANDARDS. REPLACEMENTS SHOULD CONFORM TO THE CRITERIA SHOWN AND SPECIFIED. SEE SPECS FOR ALL MAINTENANCE REQUIREMENTS, SIZE, CHARACTER AND FORM OF ALL REPLACEMENTS TO BE SIMILAR TO THE ORIGINAL APPROVED MATERIAL AND APPROVED BY THE AOR AND LA.
- ALL PLANT MATERIAL IS SHOWN AS ANTICIPATED SIZE AT INSTALLATION. PLANT GROWTH VARIES BY SPECIES AND YEARLY CLIMACTIC CONDITIONS.



LEGEND



1 AMENITY DECK
1" = 20'-0"



EXHIBIT F

Ordinance 61-O-76

AS ADOPTED

3. B.A. 2-76 (V.S.U. PDCA)
file
ford
conroy



6-3-76, 6-16-76
6-21-76, 6-25-76
7-14-76

61-0-76

AN ORDINANCE

Granting the Application by the Cook County
Housing Authority for the Establishment of
A Planned Development at 1900-20 Sherman Avenue

WHEREAS, the City of Evanston did enter into a cooperation agreement with the Housing Authority of Cook County (hereinafter referred to as HACC) on August 17, 1970 to provide low-rent housing for the elderly in the City of Evanston; and

WHEREAS, the HACC did submit a request for a program reservation to the Department of Housing and Urban Development to provide federal funds for such housing and HUD did approve a reservation of 100 units of low-rent housing for the elderly in Evanston; and

WHEREAS, on February 3, 1976, March 16, 1976, March 30, 1976, April 20, 1976, and May 18, 1976 the Zoning Board of Appeals conducted a Public Hearing upon the application of the Housing Authority of the County of Cook for various forms of relief including a planned development as provided by the Evanston Zoning Ordinance adopted November 21, 1960 as amended, as they apply to the property located at 1900-20 Sherman Avenue, Evanston, Illinois, an R7 General Residence District, to allow construction of a low-rent elderly housing facility in conformance with the Zoning Board of Appeals application and plans submitted and identified as received by the Appeals Officer on December 24, 1975; said public hearing having been conducted pursuant to notice and publication thereof as provided by law; and

WHEREAS, the Zoning Board of Appeals has recommended that said application for a Planned Development be granted; and

AS ADOPTED



AS ADOPTED

61-0-76

-2-

WHEREAS, THE City Council of the City of Evanston has made the following findings of fact, to wit:

1. The proposed structure is so designed, located and proposed to be operated that the public health, safety, and welfare will be protected, and the public welfare will be advanced by providing low income housing of good quality for elderly persons.

2. The development as proposed is in harmony with the general purposes and intent of the comprehensive zoning ordinance.

3. The City Council finds that the proposed Planned Development is compatible with the development allowed under the basic provisions of the Zoning Ordinance in the area in which it is proposed and is not of such a nature in height, bulk, or scale as to exercise influence contrary to the purpose and intent of the Zoning Ordinance as specifically set forth in Section II of the Zoning Ordinance.

4. The City Council finds that the proposed use and development plans are compatible with and implement the planning goals and objectives of the City as contained in the Comprehensive Plan and other pertinent policy resolutions, particularly in terms of the following specific findings:

a. That the proposal will implement land use policy of the City as contained in the Comprehensive Plan.

b. That a preponderance of the evidence demonstrates that the proposal, while increasing land use intensity with respect to building height and rear yard, still implements the objectives of the City and other pertinent policy resolutions entered into by City and the applicant.

AS ADOPTED



AS ADOPTED

61-0-76

-3-

c. That the proposal advances the housing goals of the City by implementing City Council Resolution 27-R-73 which designated this site for one hundred one (101) units of low rent housing for the elderly and the objective of the Comprehensive Plan to develop within the means of the community an adequate housing supply so that Evanston residents may enjoy housing with a range of geographic choices that is suited to their needs and within their reasonable ability to pay.

d. That the proposal implements population policy by providing low income, elderly housing with good access to public transportation and within the proximity of the downtown business district.

e. That the evidence demonstrates that the proposal will have no adverse impact on traffic or parking since the street system is adequate, mass transportation is readily accessible, and more off-street parking is being provided than is required by the Zoning Ordinance.

f. That the proposal will not increase the number of school-age children within the area and will have no adverse impact on public services and facilities and a portion of the land will be restored to the tax rolls and payments made in lieu of taxes.

g. That the proposal will not alter the essential character of the neighborhood since it is already improved with a number of multiple family dwellings except to the west of the subject property which is located in a C2 District.

h. That the proposal is consistent with planning goals.

i. That the proposal will assist in the conservation of taxable value of land and buildings throughout the City of Evanston and the retention of taxable land on the tax rolls by returning a portion of the land to the tax rolls and the applicant will be making payments in lieu of taxes.

AS ADOPTED



AS ADOPTED

61-0-76

-4-

5. The City Council finds that the proposed development can be completed within two years of obtaining the permit.
6. The City Council finds that the proposed development will not overload the streets adjacent to the project.
7. The City Council finds that the existing utility services are adequate for the proposed development.
8. The City Council finds that the applicant's Exhibit F.2. demonstrates adequate financial resources to assure completion of the development.
9. The City Council finds that the findings and recommendations of the Zoning Board are supported by the evidence and the approval of said planned development is in the best interest of the City of Evanston;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF EVANSTON, COOK COUNTY, ILLINOIS:

SECTION 1: That the Housing Authority of the County of Cook be and it is hereby granted approval for a Planned Development to permit construction of housing for the elderly in an R7 General Residence District on the property located at 1900-20 Sherman Avenue, Evanston, Illinois, and legally described as:

Lots 7, 8, 9, 10, 11, and 12 in Huse and Powers Addition to Evanston, being a Subdivision of Lot 7 of Assessor's Division of the Northeast Quarter of Section 18, Township 41 North, Range 14, East of the Third Principal Meridian pursuant to Plat recorded May 29, 1874 as Document No. 163502 all in Cook County, Illinois

with the following allowances:

- a. A building height of 97 feet; and
- b. A rear yard of 25 feet

subject to compliance with all other provisions of the Zoning Ordinance, other applicable laws, and the following conditions:

1. Unless otherwise approved by the Planning and Development Committee or the City Council, the planned development shall conform to the application as presented to the Zoning Board of Appeals.

AS ADOPTED



AS ADOPTED

61-0-76

-5-

2. That said planned development will be constructed in substantial compliance with the application and plans attached hereto and made part of this ordinance as:

Exhibit C - Applications for variation, planned development, etc.

Exhibit E - Property line map, topography map, utility map, landscape plan, site plan, ground floor plan, typical floor plan, elevation drawings

3. Residents of the City of Evanston shall be given priority for occupancy of the dwelling units.

4. That the owners shall make an annual payment in lieu of taxes at the rate of 10 percent of the gross amount realized annually for the rental of the units in said Planned Development.

SECTION 2: The plat of consolidation attached to this ordinance is hereby approved.

SECTION 3: The Director of Inspections and Permits is hereby ordered and directed to grant such permits as shall be required to effectuate the provisions of this ordinance subsequent to recordation of said ordinance in the office of the Recorder of Deeds of Cook County.

SECTION 4: This ordinance shall be in full force and effect from and after its passage and approval in the manner provided by law.

AS ADOPTED

AS ADOPTED

61-0-76

-6-

Introduced _____, 1976

Adopted July 12, 1976

Approved _____, 1976

Mayor

ATTEST:

City Clerk

Approved as to form: _____
Corporation Counsel

AS ADOPTED

PLANNED DEVELOPMENT AMENDMENT APPLICATION FOR:

The Emerson: A Mixed-Income Senior Building at 1900 Sherman Avenue

CONTENTS:

- 1. Application Form**
- 2. Disclosure Statement**
- 3. Plan of Site PINs**
- 4. Plat of Survey**
- 5. Zoning Analysis Results Sheet**
- 6. Development Plans**
- 7. Inclusionary Housing Ordinance Application**
- 8. Development Schedule**
- 9. Statement on Development Allowances**

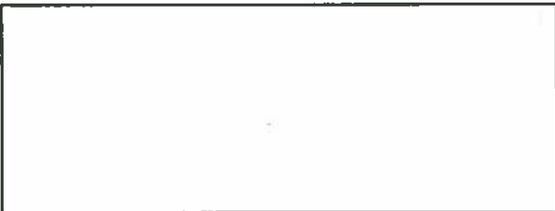


**Project Developer:
The Housing Authority of Cook County**

April 16, 2021



PLANNED DEVELOPMENT APPLICATION



Case Number: _____

1. PROPERTY

Address(es)/Location(s)

1900 Sherman Avenue, Evanston, IL 60201

Brief Narrative Summary of Proposal:

The Petitioner seeks to amend the approved planned development. The Petitioner submitted a revised Zoning Analysis Application to the City of Evanston in August of 2021, which identified the requested changes. The Zoning Analysis Results Sheet identified four (4) major adjustments needed to allow the Petitioner to make the requested changes to the Development Plans. Please refer to the he Zoning Analysis Results Sheet .

2. APPLICANT

Name: The Housing Authority of Cook County Organization: Municipal Corporation: Illinois Housing Authority Act

Address: 175 West Jackson Blvd. City, State, Zip: Chicago, IL 60604

Phone: Work: 312.663.5447 Home: _____ Cell/Other: _____

Fax: Work: none Home: _____

E-mail: ahushaw@thehacc.org

Please circle the primary means of contact.

What is the relationship of the applicant to the property owner?

- same
- builder/contractor
- potential purchaser
- potential lessee
- architect
- attorney
- lessee
- real estate agent
- officer of board of directors
- other: Executive Director

3. SIGNATURE

"I certify that all of the above information and all statements, information and exhibits that I am submitting in conjunction with this application are true and accurate to the best of my knowledge."

Julia P. Duncan, General Counsel
Applicant Signature – REQUIRED

September 16, 2021
Date

4. PRE-SUBMISSION REQUIREMENTS

Prior to actually submitting an application for Planned Development, you must:

A. Complete a Zoning Analysis of the Development Plan

The Zoning Office staff must review the development plan and publish a written determination of the plan's level of compliance with the zoning district regulations. Apply at the Zoning Office.

B. Present the planned development at a pre-application conference

Contact the Zoning Office to schedule a conference with Planning & Zoning Division staff.

5. REQUIRED SUBMISSION DOCUMENTS AND MATERIALS

(This) Completed Application Form

Application Fee (Planned Development: \$6,000; Major Adjustment: \$2,200; Minor Adjustment: \$1,000)

Two (2) Copies of Application Binder

Your application must be in the form of a binder with removable pages for copying.

You must submit two application binders for initial review.

The Application Binder must include:

- Certificate of Disclosure of Ownership Interest Form
- Plan drawing illustrating development boundary and individual parcels and PINs
- Plat of Survey of Entire Development Site
- Zoning Analysis Results Sheet
- Preliminary Plat of Subdivision
- Pre-application Conference Materials
- Development Plan
- Landscape Plan (if changes will be proposed)
- Inclusionary Housing Ordinance Application
- Statement addressing how the planned development approval will further public benefits
- Statement describing the relationship with the Comprehensive Plan and other City land use plans
- Statement describing the development's compliance with any other pertinent city planning and development policies
- Statement addressing the site controls and standards for planned developments
- Statement of proposed development's compatibility with the surrounding neighborhood
- Statement of the proposed development's compatibility with the design guidelines for planned developments
- Statements describing provisions for care and maintenance of open space and recreational facilities and proposed articles of incorporation and bylaws
- Restrictive Covenants
- Schedule of Development
- Market Feasibility Statement
- Traffic Circulation Impact Study
- Statement addressing development allowances for planned developments

Notes:

- **Plats of survey** must be drawn to scale and must accurately and completely reflect the current conditions of the property.
- **Building plans** must be drawn to scale and must include interior floor plans and exterior elevations.
- **Application Fees** may be paid by cash, check, or credit card.
- **Mailing Fees** also apply and will be provided to the applicant from the City's mailing vendor.

6. OTHER PROFESSIONAL REPRESENTATIVE INFORMATION

The professional representatives are the same as in the original planned development application.

Attorney

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Architect

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Surveyor

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Civil Engineer

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Traffic Engineer

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Other Consultant

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Application Procedure

- (A) Pre-Application Conference: Pre-Application Conference: Prior to application submittal, an applicant shall meet with the Zoning Administrator, Planning & Zoning staff, and the Alderman of the ward in which the proposed planned development is located. Where applicable, a representative of the Preservation Commission shall be present. The purpose of the conference is to present the concept of the proposed plan and discuss procedures and standards for approval. No representation made by City staff or the representative of the Preservation Commission during such conference or at any other time shall be binding upon the City with respect to the application subsequently submitted. The pre-application conference shall be scheduled within 15 calendar days after receiving the applicant's request.
- (B) Information Needed for Pre-Application Conference: The applicant shall include the following information at the time of request for the meeting:
1. Narrative summary of proposal.
 2. Conceptual site plan.
 3. Plat of survey (including the location of utilities).
 4. Proposed elevations.
 5. Photographs of the subject and surrounding properties.
 6. Description of adjacent land uses and neighborhood characteristics.
 7. Description of critical historical structures, details or characteristics (if applicable).
- (C) Zoning Analysis Application: Prior to review of the project, the applicant must submit the project for "zoning analysis." The Zoning Division's response to this application is a Zoning Analysis Review Sheet addressing specific regulatory areas, and indicating compliance or deficiency.
- (D) Results of Pre-Application Conference and Zoning Analysis: Following the pre-application conference, the Zoning Administrator shall be available to suggest modifications to the site plan as discussed during the pre-application conference. Within 7 calendar days, minutes of the pre-application conference shall be sent to the applicant and shall be available upon request to interested parties by the Zoning Administrator.

Review Procedure - Decision

- (A) Review Procedure: Upon the review of an application for a planned development the Zoning Administrator shall notify the developer of any deficiencies or modifications necessary to perfect the planned development application.
- (B) Public Hearing: After determining that the application is complete, the Zoning Administrator shall schedule a public hearing to be held by the Plan Commission at which time a formal presentation of the planned development application will be presented. The public hearing shall be held not less than 15 calendar days and no more than 30 calendar days from the date of receipt of the complete application. In addition, a sign shall be posted on the property for a minimum of 10 working days prior to the public hearing indicating the place, time and date of the hearing.
- (C) Mailed Notices Required: Notice shall also be given by first class mail to all owners of property within a 1,000-foot radius of the subject property as provided by the City. The failure of delivery of such notice, however, shall not invalidate any such hearing. The City, through its Geographic Information System, will supply the names and addresses of the owners of property within the 1,000-foot radius. A third party mailing service mails notice of the hearing to the neighboring property owners. The applicant must pay any and all fees and postage associated with mailing this notice. The City publishes a notice of the hearing in a locally circulating newspaper, generally the Evanston Review, no less than 15 days nor more than 30 days prior to hearing.
- (D) Recommendation: The Plan Commission holds a public hearing and makes a recommendation, based on findings of fact, to the City Council within 60 days of the close of the public hearing. The Planning and Development Committee of the City Council considers the Plan Commission's recommendation and forwards it to the full Council with or without a recommendation. The City Council considers the Plan Commission's recommendation and may introduce an ordinance granting the planned development. The City Council may adopt an ordinance granting the planned development at the following or any subsequent City Council meeting. The developer shall record the ordinance granting the planned development and the development plan with the Cook County Recorder.

Submittal Requirements

- 1) Planned Development Application Form.
- 2) Certificate of Disclosure of Ownership Interest Form listing each individual lot contained within the proposed development identified by parcel identification number and each owner having legal or equitable interest in each

individual parcel. Connection to the ownership interest in the property must be documented in the form of a title insurance policy, deed, lease or contract to lease or purchase.

- 3) Exhibit illustrating the boundaries of each individual parcel contained within the property(ies) proposed for development with coincide parcel identification numbers.
- 4) Plat of Survey, drawn to scale, showing dimensions and areas of the parcel(s), lot(s), block(s), or portions thereof, according to the recorded plat of the subject property(ies).
- 5) Preliminary Plat of Subdivision, if necessary, showing the development consists of, and is coterminous with, a single lot legally described in a recorded plat of subdivision or proposed subdivision or consolidation.
- 6) Pre-application Conference Materials:
 - a) Conceptual site plan, showing parking and bicycle facilities where appropriate;
 - b) Plat of survey showing location of utilities;
 - c) Elevations;
 - d) Photographs of the subject and surrounding properties;
 - e) Description of adjacent land uses and neighborhood characteristics; and
 - f) Description of critical historical structures, details or characteristics.
- 7) Zoning Analysis Results Sheet, if available.
- 8) Development Plan showing:
 - a) Location, dimensions and total area of site;
 - b) Location, dimensions, floor area, construction type and use of each structure;
 - c) Number, type and size of dwelling units, and the overall dwelling unit density;
 - d) Number and location of parking spaces and loading docks, with means of ingress and egress;
 - e) Traffic circulation pattern, location and description of public improvements, streets and access easements to be installed or created;
 - f) All existing and proposed dedications and easements;
 - g) Drainage plan;
 - h) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - i) Significant topographical or physical features, including trees;
 - j) Soil or subsurface conditions; and
 - k) Historical structures or features.
- 9) Landscape Plan, including:
 - a) Location, dimensions and total area of site;
 - b) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - c) Landscaping location and treatment, plant material types, size and quantity, open spaces, and exterior surfaces of all structures with sketches of proposed landscaping;
 - d) Topographic and physical site features including soils and existing trees and vegetation;
 - e) Location, type and size of trees to be removed, and preservation plan for existing trees to remain through construction; and
 - f) Location, type, height and material of all fences and walls.
- 10) Inclusionary Housing Ordinance Application.
- 11) Statement addressing how the planned development approval will further public benefits including:
 - a) Preservation and enhancement of desirable site characteristics, open space, topographic and geologic features, and historic and natural resources;
 - b) Use of design, landscape, and architectural features to create a pleasing environment;
 - c) Provide a variety of housing types in accordance with the City's housing goals;
 - d) Eliminate blighted structures or incompatible uses through redevelopment or rehabilitation;
 - e) Business, commercial, and manufacturing development to enhance the local economy and strengthen the tax base;
 - f) Efficiently use land resulting in more economic networks of utilities, streets, schools, public grounds, and other facilities; and
 - g) Incorporate recognized sustainable design practices and building materials to promote energy conservation and improve environmental quality.
 - h) Additional benefits related to transit alternatives, public art, public space improvements, etc.

- 12) Statement describing the relationship between the proposed development and the Comprehensive General Plan and other City land use plans.
- 13) Statement describing the developments compliance with the Zoning Ordinance and any other pertinent city planning and development policies.
- 14) Statement addressing the site controls and standards for planned developments in the subject property's zoning district regarding the following:
 - a) Minimum area
 - b) Tree preservation
 - c) Landscaped strip
 - d) Open space
 - e) Walkways
 - f) Parking and loading
 - g) Utilities
 - h) Stormwater treatment
- 15) Statement of proposed developments compatibility with the surrounding neighborhood.
- 16) Statement of proposed developments compatibility with the design guidelines for planned developments.
- 17) Statement describing provisions for care and maintenance of open space and recreational facilities and, if owned by an entity other than a government authority, proposed articles of incorporation and bylaws.
- 18) Restrictive Covenants to be recorded against proposed development.
- 19) Schedule of Development phases or stages stating beginning and completion time for each phase.
- 20) Market Feasibility Statement indicating the consumer market areas for all proposed uses in the development, the population potential of the area to be served by the proposed uses and other pertinent information concerning the demand for such uses of land.
- 21) Traffic Circulation Impact Study showing the effect of the development upon adjacent roadways, anticipated vehicular trips and traffic flow, and what road improvements and traffic control upgrading might be necessary.
- 22) Statement addressing the development allowances for planned developments in the subject property's zoning district regarding the following:
 - a) Height increases
 - b) Density increases
 - c) The location and placement of buildings varying from that otherwise permitted in the district
 - d) Floor area ratio increases

Standing

The applicant must own, lease, or have legal or equitable interest in the subject property. The Planning and Zoning Division requires the applicant to demonstrate his or her connection to the ownership interest in the property. Documentation can be in the form of a title insurance policy or a deed, and a lease or contract to lease or purchase.



City of Evanston

2. DISCLOSURE STATEMENT FOR ZONING HEARINGS

OFFICE USE	
	CASE #
DATE RECEIVED	

The Evanston City Code, Title 1, Chapter 18, requires any persons or entities who request the City Council to grant zoning amendments, variations, or special uses, including planned developments, to make the following disclosures of information. The applicant is responsible for keeping the disclosure information current until the City Council has taken action on the application. For all hearings, this information is used to avoid conflicts of interest on the part of decision-makers.

1. Address or location of property for which zoning relief is sought, if applicable: 1900 Sherman Avenue Zip: 60201

2. Name of applicant for zoning relief: Housing Authority of Cook County

3. If applicant is an agent or designee, list the name, address, phone, fax, and any other contact information of the proposed user of the land for which this application for zoning relief is made: Does not apply.

4. If a person or organization owns or controls the proposed land user, name, address, phone, fax, and any other contact information of person or entity having constructive control of the proposed land user. Same as number _____ above, or indicated below. (An example of this situation is if the land user is a division or subsidiary of another person or organization.)

5. Name, address, phone, fax, and any other contact information of person or entity holding title to the subject property. Same as number _____ above, or indicated below.
Evanston Senior Redevelopment LLC, 175 West Jackson Street, Suite 350, Chicago, Illinois 60604
Richard J. Monocchio, Chief Executive and President of OneVision Housing LLC, its Managing Member, 312-663-5447
See attached Exhibit A: Disclosure of Members

6. Name, address, phone, fax, and any other contact information of person or entity having constructive control of the subject property. Same as number 1 above, or indicated below.
Housing Authority of Cook County, 175 West Jackson Street, Suite 350, Chicago, Illinois 60604
Richard J. Monocchio, Executive Director, 312-663-5447
Additional persons listed on attached Exhibit B: Board of Directors of Housing Authority of Cook County
(contact information same as above)

If Applicant or Proposed Land User is a Corporation

7.

Any corporation required by law to file a statement with any other governmental agency providing substantially the information required below may submit a copy of this statement in lieu of completing a and b below.

a. Names and addresses of all officers and directors.

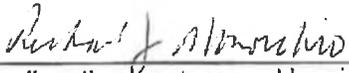
b. Names, addresses, and percentage of interest of all shareholders, if there are fewer than 33 shareholders, or shareholders holding 3% or more of the ownership interest in the corporation, if there are more than 33 shareholders.

If Applicant or Proposed Land User is not a Corporation

8.

Name, address, percentage of interest, and relationship to applicant, of each partner, associate, person holding a beneficial interest, or other person having an interest in the entity applying, or in whose interest one is applying, for the zoning relief.

I certify that all of the above statements and all statements, information and exhibits that I am submitting in conjunction with this application for relief from the requirements of the Zoning Ordinance or for an appeal from the Zoning Administrator's decision are true to the best of my knowledge.

 5/11/2020

Applicant's signature Housing Authority of Cook County Date

Applicant's signature Date

Applicant's signature Date

Exhibit A
Disclosure Statement for Zoning Hearings
1900 Sherman Avenue
May 7, 2020

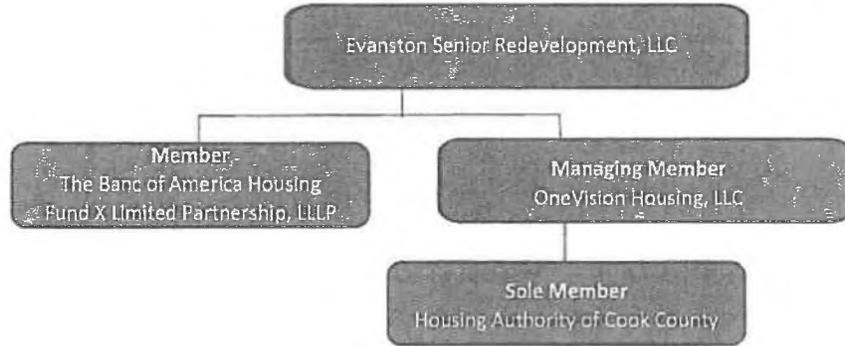


Exhibit B
Disclosure Statement for Zoning Hearings
1900 Sherman Avenue
May 7, 2020

Richard J. Monocchio	Executive Director
Wendy Walker Williams	Acting Chair
Polly Kuehl	Commissioner
Denise Jordan Walker	Commissioner
Saul Klibanow	Commissioner
Nilda Soler	Commissioner
Vicki Reed	Commissioner



175 West Jackson Boulevard, Suite 350 • Chicago, Illinois 60604 • 312.663.5447

May 7, 2020

Richard J. Monocchio
President

Karl M. Becker
Vice President

Scott Mangum
Planning and Zoning Manager
City of Evanston
2100 Ridge Avenue
Evanston, Illinois 60201

Re: Consent from Evanston Senior Redevelopment, LLC
1900 Sherman Avenue, Evanston ("Property")

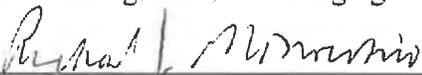
Dear Mr. Mangum:

On behalf Evanston Senior Redevelopment, LLC, owner of the captioned Property, I hereby consent to Housing Authority of Cook County applying for and securing the following land use approvals: a) rezoning; b) amendment of the existing planned development; and c) all other necessary zoning and land use allowances required to effectuate development of a new residential tower.

Kind regards,

Evanston Senior Redevelopment, LLC

By: OneVision Housing, LLC, its Managing Member

By: 
Richard J. Monocchio
Chief Executive Officer and President



Building Communities and Changing Lives.....
175 W. Jackson Blvd., Suite 350 • Chicago, IL 60604 • (312) 663-5447

May 7, 2020

Wendy Walker Williams
Vice Chair, Acting Chair

Polly Kuehl
Commissioner

Deniece Jordan-Walker
Commissioner

Nilda Soler
Commissioner

Saul H. Klibanow
Commissioner

Vicki Reed
Commissioner

Richard J. Monocchio
Executive Director

Scott Mangum
Planning and Zoning Manager
City of Evanston
2100 Ridge Avenue
Evanston, Illinois 60201

Re: 1900 Shermer Avenue, Evanston

Dear Mr. Mangum:

The attached City of Evanston Disclosure Statement for Zoning Hearings is true and correct as of the date hereof.

On behalf of the Applicant, Housing Authority of Cook County, I acknowledge that Evanston City Code Title 1 Chapter 18-2 requires the Applicant to update the Disclosure Statement to keep it current. Accordingly, I confirm that Applicant will amend the Disclosure if the development transaction includes any partners, associates, persons holding a beneficial interest in the Property, or any other persons in whose interest the Applicant is applying for the requested zoning relief.

Kind regards,


Richard J. Monocchio
Housing Authority of Cook County

PROOF OF OWNERSHIP

14

**QUITCLAIM DEED
Statutory (Illinois)**

Recording Requested By and
and Upon Recordation Return To:
Stephen R. Schuster
Burke, Warren, MacKay & Serritella, P.C.
330 N. Wabash Avenue
21st Floor
Chicago, IL 60611



Doc#: 1503045044 Fee: \$46.00
RHSP Fee: \$9.00 RPRF Fee: \$1.00
Affidavit Fee: \$2.00
Karen A. Yarbrough
Cook County Recorder of Deeds
Date: 01/30/2015 03:12 PM Pg: 1 of 5

QUITCLAIM DEED

THE GRANTOR, New Foundation Center, Inc., a not-for-profit corporation existing under the laws of the State of Illinois, for the consideration of Ten and no/100 (\$10.00) in hand paid, conveys and quit claims to Evanston Senior Redevelopment, LLC, the Grantee, an Illinois limited liability company organized and existing under the laws of the State of Illinois having its principal office at 175 W. Jackson Blvd., Suite 350, Chicago, Cook County, Illinois, all of Grantor's rights, title and interest in all buildings, improvements and appurtenances located on the following described Land situated in the County of Cook and State of Illinois, which Land is demised by Grantor to Grantee pursuant to a certain Assignment and Assumption of Lease and Landlord's Consent dated as of January 29, 2015, with respect to which a Memorandum of Assignment was recorded on January 29, 2015 as Document No. _____

*recorded simultaneously
herewith*

Lot 1 in Housing Authority of Cook County's consolidation of lots 7 to 12 in Huse and Power's addition to Evanston, being a Subdivision of Lot 7 of the Assessor's Division of the North East Quarter of the North West Quarter of Section 18, Township 41 North, Range 14 East of the Third Principal Meridian, pursuant to Plat Recorded May 29, 1874 as Document No. 163502, in Cook County, Illinois.

Common Address: 1900 Sherman Avenue, Evanston, Illinois 60201

PIN: 11-18-109-059



THIS INSTRUMENT CONVEYS ONLY THE INTEREST NOTED ABOVE AND DOES NOT CONVEY TITLE TO THE DEMISED LAND.

[Signature Page Follows]

IN WITNESS WHEREOF, said Grantor has caused its name to be signed to these presents by its duly authorized attorney-in-fact this 29th day of January, 2015.

GRANTOR:

NEW FOUNDATION CENTER, INC., an Illinois not-for-profit corporation

By: Susan M Shimon
Susan Shimon, Executive Director

Exempt under provisions of 35 ILCS 200/31-45(b),
Real Estate Transfer Tax Act

Date: January 29, 2015
By: Stephen Schultz

Exempt under provisions of Paragraph B of Section 93-0-27 of the Cook County Real Estate Transfer Tax Ordinance

Date: January 29, 2015
By: Stephen Schultz

Send Subsequent Tax Bills To:

Evanston Senior Redevelopment, LLC
175 W. Jackson Blvd., Suite 350
Chicago, IL 60604
Attention: Chief Executive Officer &
President

STATE OF ILLINOIS)
) SS.
COUNTY OF COOK)

On January 29 2015, before me, Mary E. Considine, a Notary Public, in and for said County, in the State aforesaid, personally appeared Susan Shimon, personally known to me (or proved to me on the basis of satisfactory evidence) to be Susan Shimon, the Executive Director of New Foundation Center, Inc., an Illinois not-for-profit corporation, the person whose name is subscribed to the within instrument and acknowledged to me that he executed the same in his authorized capacity, and that by his signature on the instrument, the person, or the entity upon behalf of which the person acted, executed the instrument.

WITNESS my hand and official seal this 29th day of January, 2015.

Mary E. Considine
Notary Public



STATEMENT BY GRANTOR AND GRANTEE

The **grantor** or his agent affirms that, to the best of his knowledge, the name of the **grantee** shown on the deed or assignment of beneficial interest in a land trust is either a natural person, an Illinois corporation or foreign corporation authorized to do business or acquire and hold title to real estate in Illinois, a partnership authorized to do business or acquire and hold title to real estate in Illinois, or other entity recognized as a person and authorized to do business or acquire title to real estate under the laws of the State of Illinois.

Dated January 29, 2015

GRANTOR:

NEW FOUNDATION CENTER, INC., an Illinois not-for-profit corporation

By: Susan M Shimon
Susan Shimon, Executive Director

Subscribed and sworn to before me

By the said Grantor
This 29th, day of January, 2015
Notary Public Mary E Conidine



The **grantee** or his agent affirms and verifies that the name of the **grantor** shown on the deed or assignment of beneficial interest in a land trust is either a natural person, an Illinois corporation or foreign corporation authorized to do business or acquire and hold title to real estate in Illinois, a partnership authorized to do business or acquire and hold title to real estate in Illinois or other entity recognized as a person and authorized to do business or acquire title to real estate under the laws of the State of Illinois.

Dated January 29, 2015

GRANTEE:

EVANSTON SENIOR REDEVELOPMENT, LLC, an Illinois limited liability company

By: OneVision Housing, LLC, an Illinois limited liability company, as its Managing Member

By: Richard J Monocchio
Richard J. Monocchio, President

Subscribed and sworn to before me

By the said Grantee
This 29th, day of January, 2015
Notary Public Deborah A Kyker



1689523.4

Note: Any person who knowingly submits a false statement concerning the identity of a Grantee shall be guilty of a Class C misdemeanor for the first offense and of a Class A misdemeanor for subsequent offenses.

(Attach to **deed** or **ABI** to be recorded in Cook County, Illinois if exempt under provisions of Section 4 of the Illinois Real Estate Transfer Tax Act.)



September 16, 2021

The City of Evanston
Attn: Planning and Zoning
2100 Ridge Avenue
Evanston, IL 60201

Re: 1900 Sherman Avenue

To Whom It May Concern:

The Disclosure Statement included within this application for major adjustments to the approved planned development for the 1900 Sherman Avenue property is accurate and true as of the date of this letter. However, the Housing Authority of Cook County (HACC) has made it known that it intends to form a partnership with private-sector developer Related Midwest to develop, own and manage the new building, to be called The Emerson. The details of this partnership have not been finalized, thus the ownership of the property is as represented in the Disclosure Statement. However, HACC would like to share with the City of Evanston the basis of its intended partnership with Related Midwest, which is that Related Midwest will serve as a co-General Partner to the HACC in the ownership structure, with HACC serving as the majority member.

Very Truly Yours,

de c W

Jon A. Duncan
General Counsel

Wendy Walker Williams
Chair

Polly Kuehl
Vice Chair

Nilda Soler
Commissioner

Saul H. Klibanow
Commissioner

Dr. Normah Salleh-Barone
Commissioner

Elaine Kroll
Commissioner

Richard J. Monocchio
Executive Director

HILL:ICC

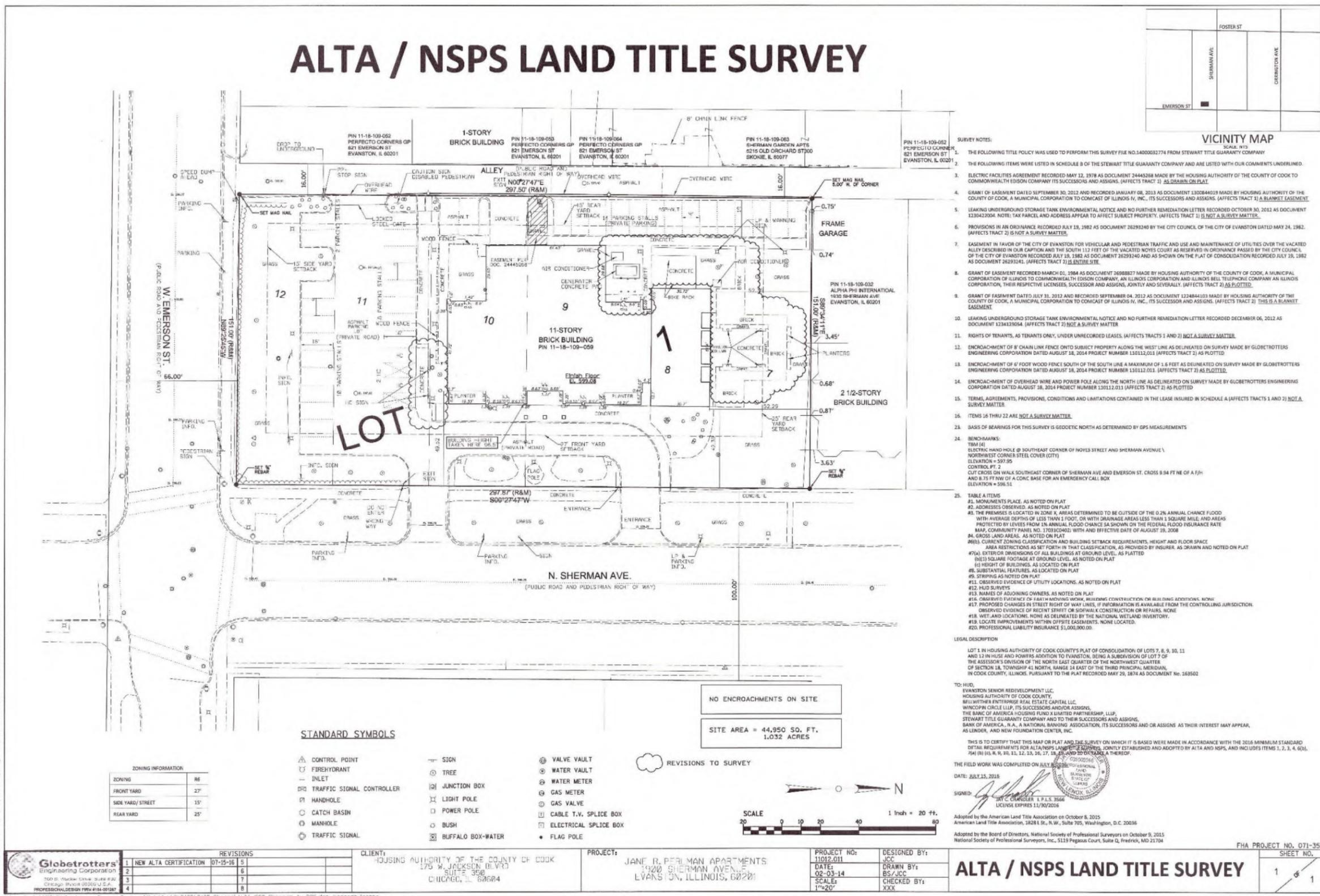
tit
EQUAL HOUSING
OPPORTUNITY ACT

C

DOCUMENT #4

Plat of Survey

ALTA / NSPS LAND TITLE SURVEY



VICINITY MAP
SCALE: N.T.S.

SURVEY NOTES:

- THE FOLLOWING TITLE POLICY WAS USED TO PERFORM THIS SURVEY FILE NO. 14000032774 FROM STEWART TITLE GUARANTY COMPANY
- THE FOLLOWING ITEMS WERE LISTED IN SCHEDULE B OF THE STEWART TITLE GUARANTY COMPANY AND ARE LISTED WITH OUR COMMENTS UNDERLINED.
- ELECTRIC FACILITIES AGREEMENT RECORDED MAY 12, 1978 AS DOCUMENT 2444338 MADE BY THE HOUSING AUTHORITY OF THE COUNTY OF COOK TO COMMONWEALTH EDISON COMPANY ITS SUCCESSORS AND ASSIGNS. (AFFECTS TRACT 1) AS DRAWN ON PLAT.
- GRANT OF EASEMENT DATED SEPTEMBER 30, 2012 AND RECORDED JANUARY 08, 2013 AS DOCUMENT 130284019 MADE BY HOUSING AUTHORITY OF THE COUNTY OF COOK, A MUNICIPAL CORPORATION TO COMCAST OF ILLINOIS IV, INC., ITS SUCCESSORS AND ASSIGNS. (AFFECTS TRACT 1) AS BLANKET EASEMENT.
- LEAKING UNDERGROUND STORAGE TANK ENVIRONMENTAL NOTICE AND NO FURTHER REMEDIATION LETTER RECORDED OCTOBER 30, 2012 AS DOCUMENT 123042204. NOTE: TAX PARCEL AND ADDRESS APPEAR TO AFFECT SUBJECT PROPERTY. (AFFECTS TRACT 1) IS NOT A SURVEY MATTER.
- PROVISIONS IN AN ORDINANCE RECORDED JULY 13, 1982 AS DOCUMENT 26293240 BY THE CITY COUNCIL OF THE CITY OF EVANSTON DATED MAY 24, 1982. (AFFECTS TRACT 2) IS NOT A SURVEY MATTER.
- EASEMENT IN FAVOR OF THE CITY OF EVANSTON FOR VEHICULAR AND PEDESTRIAN TRAFFIC AND USE AND MAINTENANCE OF UTILITIES OVER THE VACATED ALLEY DESCRIBED IN OUR CAPTION AND THE SOUTH 112 FEET OF THE VACATED NOYES COURT AS RESERVED IN ORDINANCE PASSED BY THE CITY COUNCIL OF THE CITY OF EVANSTON RECORDED JULY 18, 1982 AS DOCUMENT 26293240 AND AS SHOWN ON THE PLAT OF CONSOLIDATION RECORDED JULY 15, 1982 AS DOCUMENT 26293241. (AFFECTS TRACT 2) IS ENTIRE SITE.
- GRANT OF EASEMENT RECORDED MARCH 01, 1984 AS DOCUMENT 3698827 MADE BY HOUSING AUTHORITY OF THE COUNTY OF COOK, A MUNICIPAL CORPORATION OF ILLINOIS TO COMMONWEALTH EDISON COMPANY, AN ILLINOIS CORPORATION AND ILLINOIS BELL TELEPHONE COMPANY AN ILLINOIS CORPORATION, THEIR RESPECTIVE LICENSEES, SUCCESSOR AND ASSIGNS, JOINTLY AND SEVERALLY. (AFFECTS TRACT 2) AS PLOTTED.
- GRANT OF EASEMENT DATED JULY 31, 2012 AND RECORDED SEPTEMBER 04, 2012 AS DOCUMENT 1224844103 MADE BY HOUSING AUTHORITY OF THE COUNTY OF COOK, A MUNICIPAL CORPORATION TO COMCAST OF ILLINOIS IV, INC., ITS SUCCESSOR AND ASSIGNS. (AFFECTS TRACT 2) THIS IS A BLANKET EASEMENT.
- LEAKING UNDERGROUND STORAGE TANK ENVIRONMENTAL NOTICE AND NO FURTHER REMEDIATION LETTER RECORDED DECEMBER 06, 2012 AS DOCUMENT 123413904. (AFFECTS TRACT 2) IS NOT A SURVEY MATTER.
- RIGHTS OF TENANTS, AS TENANTS ONLY, UNDER UNRECORDED LEASES. (AFFECTS TRACTS 1 AND 2) NOT A SURVEY MATTER.
- ENCROACHMENT OF 8' CHAIN LINK FENCE ONTO SUBJECT PROPERTY ALONG THE WEST LINE AS DELINEATED ON SURVEY MADE BY GLOBETROTTERS ENGINEERING CORPORATION DATED AUGUST 18, 2014 PROJECT NUMBER 130112.021 (AFFECTS TRACT 2) AS PLOTTED.
- ENCROACHMENT OF 6' CHAIN LINK FENCE SOUTH OF THE SOUTH LINE A MAXIMUM OF 1.6 FEET AS DELINEATED ON SURVEY MADE BY GLOBETROTTERS ENGINEERING CORPORATION DATED AUGUST 18, 2014 PROJECT NUMBER 130112.021. (AFFECTS TRACT 2) AS PLOTTED.
- ENCROACHMENT OF OVERHEAD WIRE AND POWER POLE ALONG THE NORTH LINE AS DELINEATED ON SURVEY MADE BY GLOBETROTTERS ENGINEERING CORPORATION DATED AUGUST 18, 2014 PROJECT NUMBER 130112.021 (AFFECTS TRACT 2) AS PLOTTED.
- TERMS, AGREEMENTS, PROVISIONS, CONDITIONS AND LIMITATIONS CONTAINED IN THE LEASE INSURED IN SCHEDULE A (AFFECTS TRACTS 1 AND 2) NOT A SURVEY MATTER.
- ITEMS 16 THRU 22 ARE NOT A SURVEY MATTER.
- BASES OF BEARINGS FOR THIS SURVEY IS GEODETIC NORTH AS DETERMINED BY GPS MEASUREMENTS
- MONUMENTS:
#1 (4) ELECTRIC HAND HOLE @ SOUTHEAST CORNER OF NOYES STREET AND SHERMAN AVENUE (NORTHWEST CORNER STEEL COVER (CITY) ELEVATION = 597.95 CONTROL PT. 2 CUT CROSS ON WALK SOUTHWEST CORNER OF SHERMAN AVE AND EMERSON ST. CROSS IS 54 FT NE OF A T/J AND 8.75 FT NW OF A CONC BASE FOR AN EMERGENCY CALL BOX ELEVATION = 596.51
- TABLE A ITEMS
#1. MONUMENTS PLACE, AS NOTED ON PLAT
#2. ADDRESSES OBSERVED, AS NOTED ON PLAT
#3. THE PREMISES IS LOCATED IN ZONE X, AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT, OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE, AND AREAS PROTECTED BY LEVES FROM 1% ANNUAL FLOOD CHANCE AS SHOWN ON THE FEDERAL FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 1703IC0402 WITH AN EFFECTIVE DATE OF AUGUST 19, 2008
#4. GROSS LAND AREAS, AS NOTED ON PLAT
#5(a). CURRENT ZONING CLASSIFICATION AND BUILDING SETBACK REQUIREMENTS, HEIGHT AND FLOOR SPACE AREA RESTRICTIONS AS SET FORTH IN THAT CLASSIFICATION, AS PROVIDED BY INSURER, AS DRAWN AND NOTED ON PLAT
#5(b). EXTERIOR DIMENSIONS OF ALL BUILDINGS AT GROUND LEVEL, AS NOTED ON PLAT
#5(c). SQUARE FOOTAGE AT GROUND LEVEL, AS NOTED ON PLAT
#6. HEIGHT OF BUILDINGS, AS LOCATED ON PLAT
#7. SUBSTANTIAL FEATURES, AS LOCATED ON PLAT
#8. STRIPING AS NOTED ON PLAT
#9. OBSERVED EVIDENCE OF UTILITY LOCATIONS, AS NOTED ON PLAT
#10. HUD SURVEYS
#11. NAMES OF ADJOINING OWNERS, AS NOTED ON PLAT
#12. OBSERVED EVIDENCE OF EXISTING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS, NONE
#13. PROPOSED CHANGES IN STREET RIGHT OF WAY LINES, IF INFORMATION IS AVAILABLE FROM THE CONTROLLING JURISDICTION
#14. OBSERVED EVIDENCE OF RECENT STREET OR SIGNAL CONSTRUCTION OR REPAIRS, NONE
#15. WET AND LOCATIONS, NONE AS DELINEATED BY THE NATIONAL WETLAND INVENTORY
#16. LOCATE IMPROVEMENTS WITHIN OFFSITE EASEMENTS, NONE LOCATED.
#17. PROFESSIONAL LIABILITY INSURANCE \$1,000,000.00.

LEGAL DESCRIPTION
LOT 1 IN HOUSING AUTHORITY OF COOK COUNTY'S PLAT OF CONSOLIDATION OF LOTS 7, 8, 9, 10, 11 AND 12 IN HOUSE AND POWERS ADDITION TO EVANSTON, BEING A SUBDIVISION OF LOT 7 OF THE ASSASSOR'S DIVISION OF THE NORTH EAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 18, TOWNSHIP 41 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS, PURSUANT TO THE PLAT RECORDED MAY 29, 1878 AS DOCUMENT NO. 168502

TO: HUD, EVANSTON SENIOR REDEVELOPMENT LLC, HOUSING AUTHORITY OF COOK COUNTY, BFL WITNER ENTERPRISE REAL ESTATE CAPITAL LLC, WINCOPIN CIRCLE L.L.P., ITS SUCCESSORS AND/OR ASSIGNS, THE BANK OF AMERICA HOUSING FUND X LIMITED PARTNERSHIP, L.L.P., STEWART TITLE GUARANTY COMPANY AND TO THEIR SUCCESSORS AND ASSIGNS, BANK OF AMERICA, N.A., A NATIONAL BANKING ASSOCIATION, ITS SUCCESSORS AND OR ASSIGNS AS THEIR INTEREST MAY APPEAR, AS LENDER, AND NEW FOUNDATION CENTER, INC.

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 6(d), 7(a) (b) (c) (d) (e) (f) (g) (h) (i) (j) (k) (l) (m) (n) (o) (p) (q) (r) (s) (t) (u) (v) (w) (x) (y) (z) (aa) (ab) (ac) (ad) (ae) (af) (ag) (ah) (ai) (aj) (ak) (al) (am) (an) (ao) (ap) (aq) (ar) (as) (at) (au) (av) (aw) (ax) (ay) (az) (ba) (bb) (bc) (bd) (be) (bf) (bg) (bh) (bi) (bj) (bk) (bl) (bm) (bn) (bo) (bp) (bq) (br) (bs) (bt) (bu) (bv) (bw) (bx) (by) (bz) (ca) (cb) (cc) (cd) (ce) (cf) (cg) (ch) (ci) (cj) (ck) (cl) (cm) (cn) (co) (cp) (cq) (cr) (cs) (ct) (cu) (cv) (cw) (cx) (cy) (cz) (da) (db) (dc) (dd) (de) (df) (dg) (dh) (di) (dj) (dk) (dl) (dm) (dn) (do) (dp) (dq) (dr) (ds) (dt) (du) (dv) (dw) (dx) (dy) (dz) (ea) (eb) (ec) (ed) (ee) (ef) (eg) (eh) (ei) (ej) (ek) (el) (em) (en) (eo) (ep) (eq) (er) (es) (et) (eu) (ev) (ew) (ex) (ey) (ez) (fa) (fb) (fc) (fd) (fe) (ff) (fg) (fh) (fi) (fj) (fk) (fl) (fm) (fn) (fo) (fp) (fq) (fr) (fs) (ft) (fu) (fv) (fw) (fx) (fy) (fz) (ga) (gb) (gc) (gd) (ge) (gf) (gg) (gh) (gi) (gj) (gk) (gl) (gm) (gn) (go) (gp) (gq) (gr) (gs) (gt) (gu) (gv) (gw) (gx) (gy) (gz) (ha) (hb) (hc) (hd) (he) (hf) (hg) (hh) (hi) (hj) (hk) (hl) (hm) (hn) (ho) (hp) (hq) (hr) (hs) (ht) (hu) (hv) (hw) (hx) (hy) (hz) (ia) (ib) (ic) (id) (ie) (if) (ig) (ih) (ii) (ij) (ik) (il) (im) (in) (io) (ip) (iq) (ir) (is) (it) (iu) (iv) (iw) (ix) (iy) (iz) (ja) (jb) (jc) (jd) (je) (jf) (jg) (jh) (ji) (jj) (jk) (jl) (jm) (jn) (jo) (jp) (jq) (jr) (js) (jt) (ju) (jv) (jw) (jx) (jy) (jz) (ka) (kb) (kc) (kd) (ke) (kf) (kg) (kh) (ki) (kj) (kk) (kl) (km) (kn) (ko) (kp) (kq) (kr) (ks) (kt) (ku) (kv) (kw) (kx) (ky) (kz) (la) (lb) (lc) (ld) (le) (lf) (lg) (lh) (li) (lj) (lk) (ll) (lm) (ln) (lo) (lp) (lq) (lr) (ls) (lt) (lu) (lv) (lw) (lx) (ly) (lz) (ma) (mb) (mc) (md) (me) (mf) (mg) (mh) (mi) (mj) (mk) (ml) (mm) (mn) (mo) (mp) (mq) (mr) (ms) (mt) (mu) (mv) (mw) (mx) (my) (mz) (na) (nb) (nc) (nd) (ne) (nf) (ng) (nh) (ni) (nj) (nk) (nl) (nm) (nn) (no) (np) (nq) (nr) (ns) (nt) (nu) (nv) (nw) (nx) (ny) (nz) (oa) (ob) (oc) (od) (oe) (of) (og) (oh) (oi) (oj) (ok) (ol) (om) (on) (oo) (op) (oq) (or) (os) (ot) (ou) (ov) (ow) (ox) (oy) (oz) (pa) (pb) (pc) (pd) (pe) (pf) (pg) (ph) (pi) (pj) (pk) (pl) (pm) (pn) (po) (pp) (pq) (pr) (ps) (pt) (pu) (pv) (pw) (px) (py) (pz) (qa) (qb) (qc) (qd) (qe) (qf) (qg) (qh) (qi) (qj) (qk) (ql) (qm) (qn) (qo) (qp) (qq) (qr) (qs) (qt) (qu) (qv) (qw) (qx) (qy) (qz) (ra) (rb) (rc) (rd) (re) (rf) (rg) (rh) (ri) (rj) (rk) (rl) (rm) (rn) (ro) (rp) (rq) (rr) (rs) (rt) (ru) (rv) (rw) (rx) (ry) (rz) (sa) (sb) (sc) (sd) (se) (sf) (sg) (sh) (si) (sj) (sk) (sl) (sm) (sn) (so) (sp) (sq) (sr) (ss) (st) (su) (sv) (sw) (sx) (sy) (sz) (ta) (tb) (tc) (td) (te) (tf) (tg) (th) (ti) (tj) (tk) (tl) (tm) (tn) (to) (tp) (tq) (tr) (ts) (tt) (tu) (tv) (tw) (tx) (ty) (tz) (ua) (ub) (uc) (ud) (ue) (uf) (ug) (uh) (ui) (uj) (uk) (ul) (um) (un) (uo) (up) (uq) (ur) (us) (ut) (uu) (uv) (uw) (ux) (uy) (uz) (va) (vb) (vc) (vd) (ve) (vf) (vg) (vh) (vi) (vj) (vk) (vl) (vm) (vn) (vo) (vp) (vq) (vr) (vs) (vt) (vu) (vv) (vw) (vx) (vy) (vz) (wa) (wb) (wc) (wd) (we) (wf) (wg) (wh) (wi) (wj) (wk) (wl) (wm) (wn) (wo) (wp) (wq) (wr) (ws) (wt) (wu) (wv) (ww) (wx) (wy) (wz) (xa) (xb) (xc) (xd) (xe) (xf) (xg) (xh) (xi) (xj) (xk) (xl) (xm) (xn) (xo) (xp) (xq) (xr) (xs) (xt) (xu) (xv) (xw) (xx) (xy) (xz) (ya) (yb) (yc) (yd) (ye) (yf) (yg) (yh) (yi) (yj) (yk) (yl) (ym) (yn) (yo) (yp) (yq) (yr) (ys) (yt) (yu) (yv) (yw) (yx) (yz) (za) (zb) (zc) (zd) (ze) (zf) (zg) (zh) (zi) (zj) (zk) (zl) (zm) (zn) (zo) (zp) (zq) (zr) (zs) (zt) (zu) (zv) (zw) (zx) (zy) (zz)

STANDARD SYMBOLS

- ▲ CONTROL POINT
- ⊕ FIREHYDRANT
- INLET
- ⊠ TRAFFIC SIGNAL CONTROLLER
- ⊕ HANDHOLE
- ⊕ CATCH BASIN
- ⊕ MANHOLE
- ⊕ TRAFFIC SIGNAL
- SIGN
- ⊕ TREE
- ⊕ JUNCTION BOX
- ⊕ LIGHT POLE
- ⊕ POWER POLE
- ⊕ BUSH
- ⊕ BUFFALO BOX-WATER
- ⊕ VALVE VAULT
- ⊕ WATER VAULT
- ⊕ WATER METER
- ⊕ GAS METER
- ⊕ GAS VALVE
- ⊕ CABLE T.V. SPLICE BOX
- ⊕ ELECTRICAL SPLICE BOX
- FLAG POLE

REVISIONS TO SURVEY



ZONING INFORMATION

ZONING	R6
FRONT YARD	27'
SIDE YARD/STREET	15'
REAR YARD	25'

NO ENCROACHMENTS ON SITE

SITE AREA = 44,950 SQ. FT.
1.032 ACRES

<p>Globetrotters Engineering Corporation 1500 S. Wacker Drive, Suite 4100 Chicago, Illinois 60606 U.S.A. PROFESSIONAL LICENSE #191434-010047</p>	<p>REVISIONS</p> <table border="1"> <tr><td>1</td><td>NEW ALTA CERTIFICATION</td><td>07-15-16</td><td>5</td></tr> <tr><td>2</td><td></td><td></td><td>6</td></tr> <tr><td>3</td><td></td><td></td><td>7</td></tr> <tr><td>4</td><td></td><td></td><td>8</td></tr> </table>	1	NEW ALTA CERTIFICATION	07-15-16	5	2			6	3			7	4			8	<p>CLIENT: HOUSING AUTHORITY OF THE COUNTY OF COOK 175 W. JACKSON BLVD SUITE 350 CHICAGO, IL 60604</p>	<p>PROJECT: JANE R. PERLMAN APARTMENTS 1920 SHERMAN AVENUE EVANSTON, ILLINOIS, 60201</p>	<p>PROJECT NO: 11012.011</p> <p>DATE: 02-03-14</p> <p>SCALE: 1"=20'</p>	<p>DESIGNED BY: JCC</p> <p>DRAWN BY: BS/JCC</p> <p>CHECKED BY: XXX</p>	<p>FHA PROJECT NO. 071-35871 SHEET NO. 1</p>
	1	NEW ALTA CERTIFICATION	07-15-16	5																		
2			6																			
3			7																			
4			8																			
<p>ALTA / NSPS LAND TITLE SURVEY</p>																						



Zoning Analysis Summary

Date: 8/20/21

Case Number:

Case Status/Determination:

21ZONA-0127 – 1900 Sherman Avenue

NON-COMPLIANT

Proposal:

Zoning Analysis in anticipation of Major Adjustment to a Planned Development approved by ordinance 109-O-20 which granted a Special Use for a Planned Development and a Map Amendment to rezone the property to the C1a District. The applicant proposes to reduce the number of dwelling units in the new building from 168 to 152, reduce the number of below grade parking spaces from 37 to 25, and make minor changes to the exterior design of the building.

Zoning Section:	Comments: Major Adjustment to Planned Development Required
6-11-4-6	One Dwelling Unit per 350 square feet of lot area allowed in C1a District (128 allowed based on 44,922.50 SF Lot Area, plus an additional 68 units allowed as IHO Bonus = 196 allowed. 78 additional units may be requested as a Site Development Allowance (40%). Total of 252 dwelling units requested does not exceed Maximum Eligible Site Development Allowance of 274.
6-10-3-9	67' building height allowed, 97' as a Site Development Allowance. Proposed: 168'-4" to top of roof. Previously approved height was 172'-8". Exceeds maximum site development allowance. Super-majority (2/3) vote of City Council not required because IHO is met.
6-16-3-5 Table 16B	0.55 spaces per bedroom required in TOD area. 192 new bedrooms proposed = 106.7 spaces – 18.7 spaces per IHO Bonus (no parking for 34 IHO 1 BR units). A total of 88 additional spaces are required. New Proposed: Removal of 22 existing surface parking spaces to be replaced with 25 below-grade parking spaces. A net addition of 3 spaces where 88 additional parking spaces are required. Eligible site development allowance. <i>Recommend exploring long-term lease of off-site parking.</i>
6-10-3-7 6-10-1-10	Reconfiguration of new building leads to increase in overall FAR from 4.33 to 4.37. Max. Base FAR in C1a is 4.0. With IHO Bonus an, additional 1.0 is provided for a total of 5.0. FAR is therefore, still is compliant despite the increase.

Additional Comments:

Please ensure all measurements of parking spaces and loading area clearance heights are labeled

City of Evanston

ZONING ANALYSIS REVIEW SHEET

APPLICATION STATUS: August 10, 2021

RESULTS OF ANALYSIS: Non-Compliant

Z.A. Number: 21ZONA-0127
Address: 1900 Sherman AVE
Applicant: William James
Phone: 3123994063

Purpose: Zoning Analysis without Bld Permit App
District: C1a **Overlay:** Preservation
Reviewer: Meagan Jones **District:**

THIS APPLICATION PROPOSES (select all that apply):

ANALYSIS BASED ON:

- | | | |
|---|--|--|
| <input checked="" type="checkbox"/> New Principal Structure | <input type="checkbox"/> Change of Use | <input type="checkbox"/> Sidewalk Cafe |
| <input type="checkbox"/> New Accessory Structure | <input type="checkbox"/> Retention of Use | <input type="checkbox"/> Other |
| <input checked="" type="checkbox"/> Addition to Structure | <input type="checkbox"/> Plat of Resubdiv./Consol. | |
| <input type="checkbox"/> Alteration to Structure | <input type="checkbox"/> Business License | |
| <input type="checkbox"/> Retention of Structure | <input type="checkbox"/> Home Occupation | |

Plans Dated: 8.2.21
Prepared By: Pappageorge Haymes
Survey Dated:
Existing Improvements: Multi-family building (Perlman)

Proposal Description:

Major Adjustment to a Planned Development approved by ordinance 109-O-20.
 Proposing a reduction in units from 168 to 152, reduction in below grade parking from 37 to 25 spaces, and some exterior changes to the building.

ZONING ANALYSIS

PLANNED DEVELOPMENT THRESHOLDS

Does not apply to I1, I2, I3, OS, U3, or Excluded T1 & T2 Properties. See Section 6-8-1-10(D) for R's; Section 6-9-1-9(D) for B's; Section 6-10-1-9(D) for C's; Section 6-11-1-10(D) for D's; Section 6-12-1-7(D) for RP; Section 6-13-1-10(D) for MU & MUE; Section 6-15-1-9 for O1, T's, U's, oH, oRE, & oRD.

- | | |
|--|-----|
| 1. Is the request for construction of substantially new structures or a substantial rehabilitation or substantial addition as defined by increasing floor area of principal structure by 35% or more? If not, skip to 2 & 4 below. | Yes |
| 2. Does the zoning lot area exceed 30,000 sqft? | Yes |
| 3. Does the proposal entail more than 24 new residential, commercial, business, retail or office units in any combination? | Yes |
| 4. Does the proposal entail the new construction of more than 20,000 sqft of true gross floor area at or above grade including areas otherwise excluded from defined gross floor area? | Yes |

RESIDENTIAL DISTRICT CALCULATIONS The following three sections apply to building lot coverage and impervious surface calculations in Residential Districts.

<u>Front Porch Exception (Subtract 50%)</u>	<u>Pavers/Pervious Paver Exception (Subtract</u>	<u>Open Parking Debit (Add 200sqft/open space</u>
Total Elibigle Front	Total Paver Area	# Open Required Spaces
Front Porch Regulatory Area	Paver Regulatory Area	Addn. to Bldg Lot Cov.

PRINCIPAL USE AND STRUCTURE

	Standard	Existing	Proposed	Determination
USE:		Dwelling - MF	Dwelling - MF	No Change

Comments:

Minimum Lot Width (LF) NA
USE: Multi Family

Comments:

Minimum Lot Area (SF)	350 sf/ d.u.	44922.5	44922.5 sf	No Change
USE:	Multi Family			

Comments:

Dwelling Units:	128 + 68 (IHO Bonus) = 196	100	252	Non-Compliant
Comments:	274 permitted with a site development allowance			

	Standard	Existing	Proposed	Determination
Rooming Units: Comments:				
Building Lot Coverage (SF) (defined, including subtractions& additions): Comments:				
Impervious Surface Coverage (SF, %) Comments:				
Accessory Structure Rear Yard Coverage: Comments:	40% of rear yard			
Gross Floor Area (SF) (or 179690 sf) + 1 (IHO Bonus) = Use: Residential Comments:		1.18	143202.35 4.37	Compliant
Height (FT) Comments: mechanical penthouse excluded from calculation	67 ft (97 ft w/ s.d.a.)		168 ft 4 in	Non-Compliant
Front Yard(1) (FT) Direction: S Street: Emerson Comments:	0 ft		11 ft 10 1/8 in	Compliant
Front Yard(2) (FT) Direction: Street: Comments:				
Street Side Yard (FT) Direction: E Street: Sherman Comments:	0 ft		20 ft 8 5/8 in	Compliant
Interior Side Yard(1) (FT) Direction: W Comments:	5 ft		5 ft	Compliant
Interior Side Yard(2) (FT) Direction: Comments:				
Rear Yard (FT) Direction: Comments:	10			No Change

PARKING REQUIREMENTS

	Standard	Existing	Proposed	Determination
Use(1): Multi-family (Nonres District)	.55/br		39 (14 existing surface + 25 below grade)	Non-Compliant

Comments: 88 new spaces required where a net increase of 3 is proposed

Standard

Existing

Proposed

Determination

Use(2):

Comments:

Use(3):

Comments:

TOTAL REQUIRED:

Comments:

Handicap Parking Spaces	Sec. 6-16-2-6	2	4 (2 existing moved + 2 new)	Compliant
--------------------------------	---------------	---	------------------------------	-----------

Comments:

Access: Sec. 6-16-2-2

Comments:

Vertical Clearance (LF) 7'

Comments:

Surfacing: Sec. 6-16-2-8 (E)

Comments:

Location: Sec. 6-4-6-2

Comments:

Angle(1): 90 Degree	Comments:			
Width(W) (FT)	8.5			Compliant

Comments:

Depth(D) (FT)	18.0			Compliant
----------------------	------	--	--	-----------

Comments:

Aisle(A) (FT)	24.0			Compliant
----------------------	------	--	--	-----------

Comments:

Module (FT)	SL 42.0, DL 60.0			Compliant
--------------------	------------------	--	--	-----------

Comments: Please provide measurements on plans

Angle(2): **Comments:**

Width(W) (FT)

Comments:

Depth(D) (FT)

Comments:

Aisle(A) (FT)

Comments:

Module (FT)

Comments:

Garage Setback from

Alley Access (FT)

Comments:

LOADING REQUIREMENTS

Standard

Existing

Proposed

Determination

Loading Use: Multi-family	1 short 30K to 100K, 1 short each addtl. 200K.		1	Compliant with Variance
-------------------------------------	--	--	---	-------------------------

Comments: number of loading berths approved with ordinance 109-O-20

LF: Linear Feet SF: Square Feet FT: Feet

	Standard	Existing	Proposed	Determination
TOTAL (long):				
TOTAL (short):				
Long Berth Size (FT)	12' wide x 50' deep			
Comments:				
Short Berth Size (FT)	10' wide x 35' deep		10 ft X 25 ft	Compliant with Variance
Comments:	Loading berth size approved with ordinance 109-O-20			
Vertical Clearance (FT)	14'			
Comments:	Please confirm height on plans			
Location:	Sec. 6-16-4-1			Compliant
Comments:				

MISCELLANEOUS REQUIREMENTS

	Standard	Existing	Proposed	Determination
Requirement (1):				
Comments:				
Requirement (2):				
Comments:				
Requirement (3):				
Comments:				

COMMENTS AND/OR NOTES

Analysis Comments

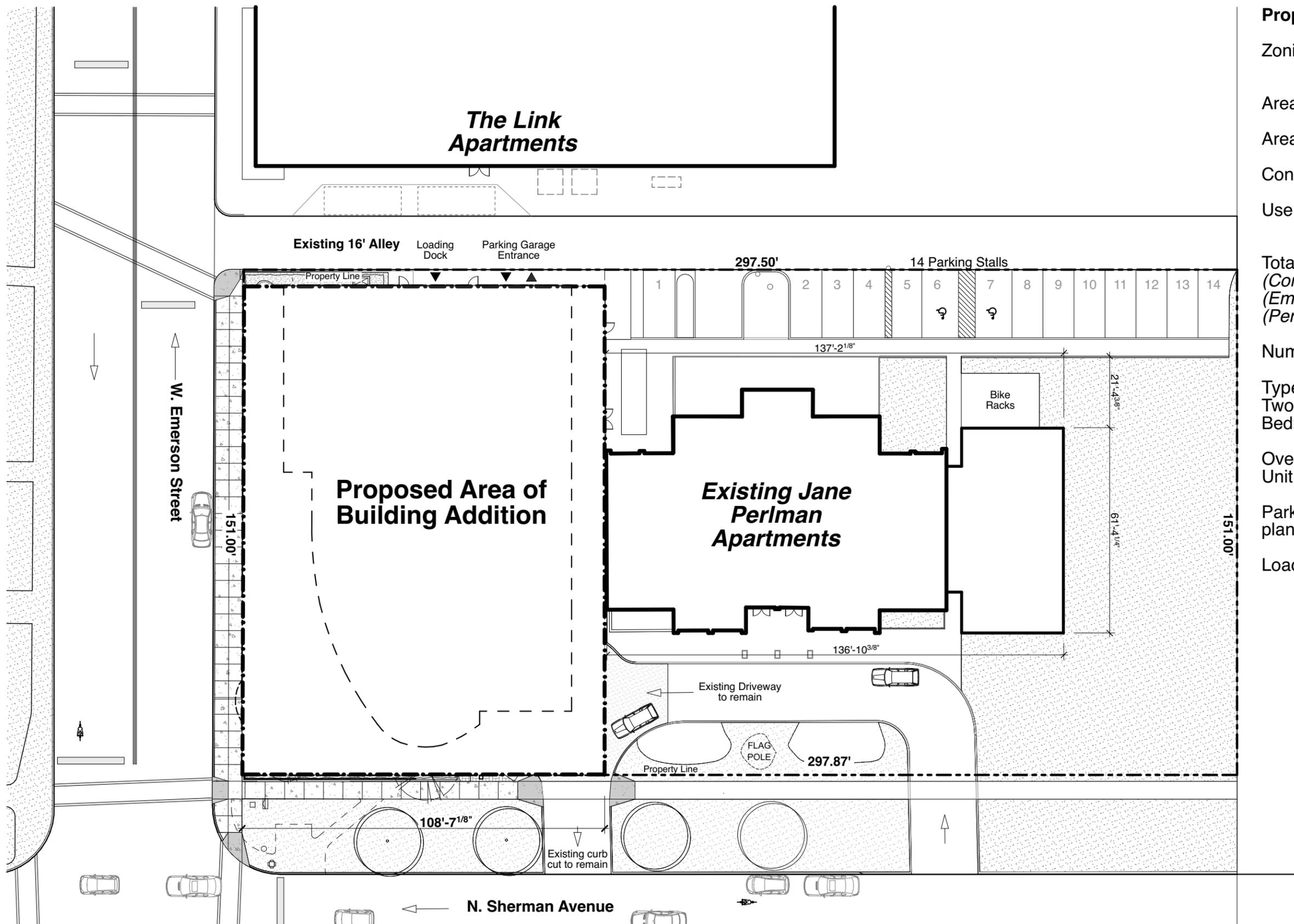
RESULTS OF ANALYSIS

Results of Analysis: This Application is **Non-Compliant**
 Site Plan & Appearance Review Committee approval is: **Required**
 See attached comments and/or notes.

 SIGNATURE DATE

6. DEVELOPMENT PLANS

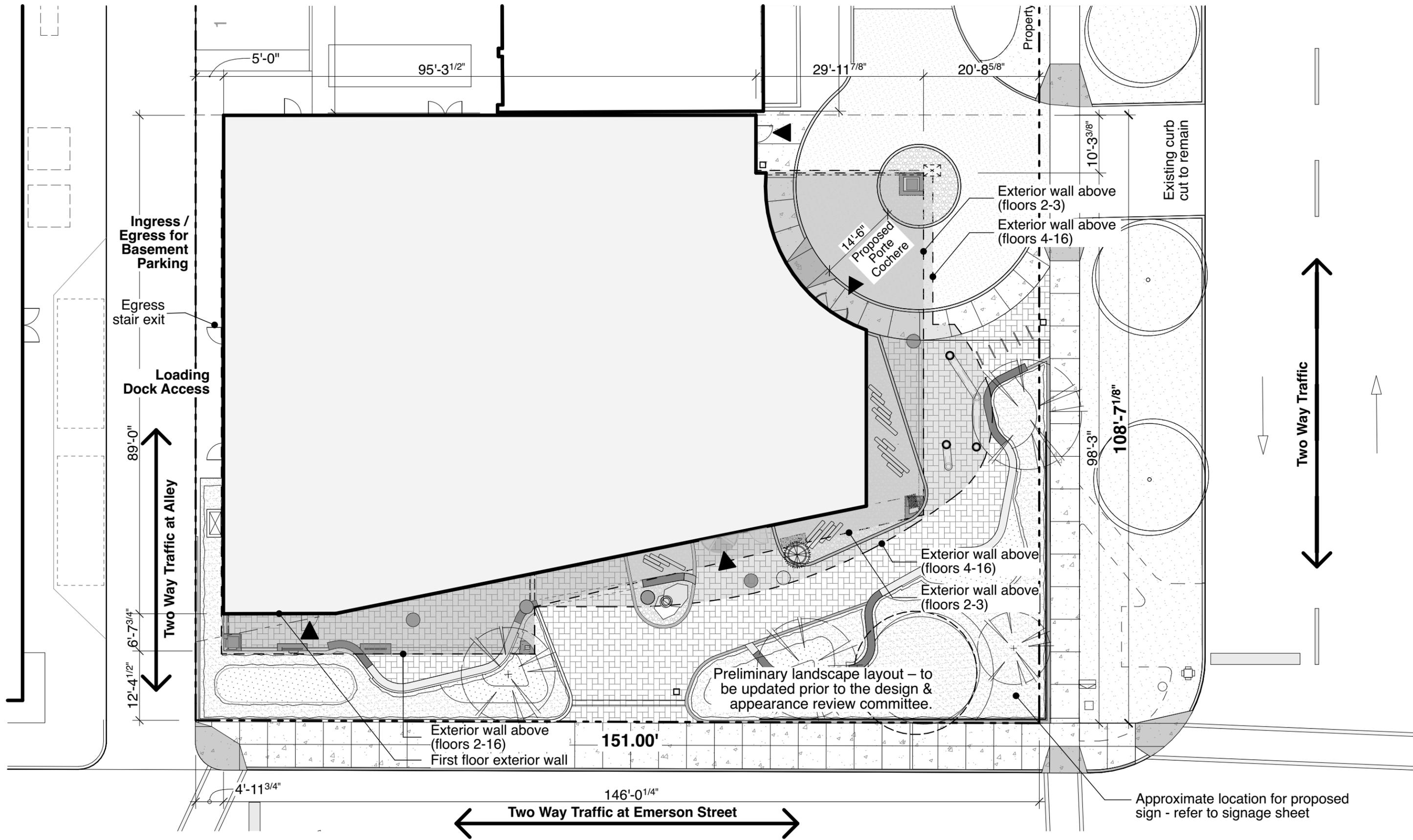
Preliminary Architectural Plan

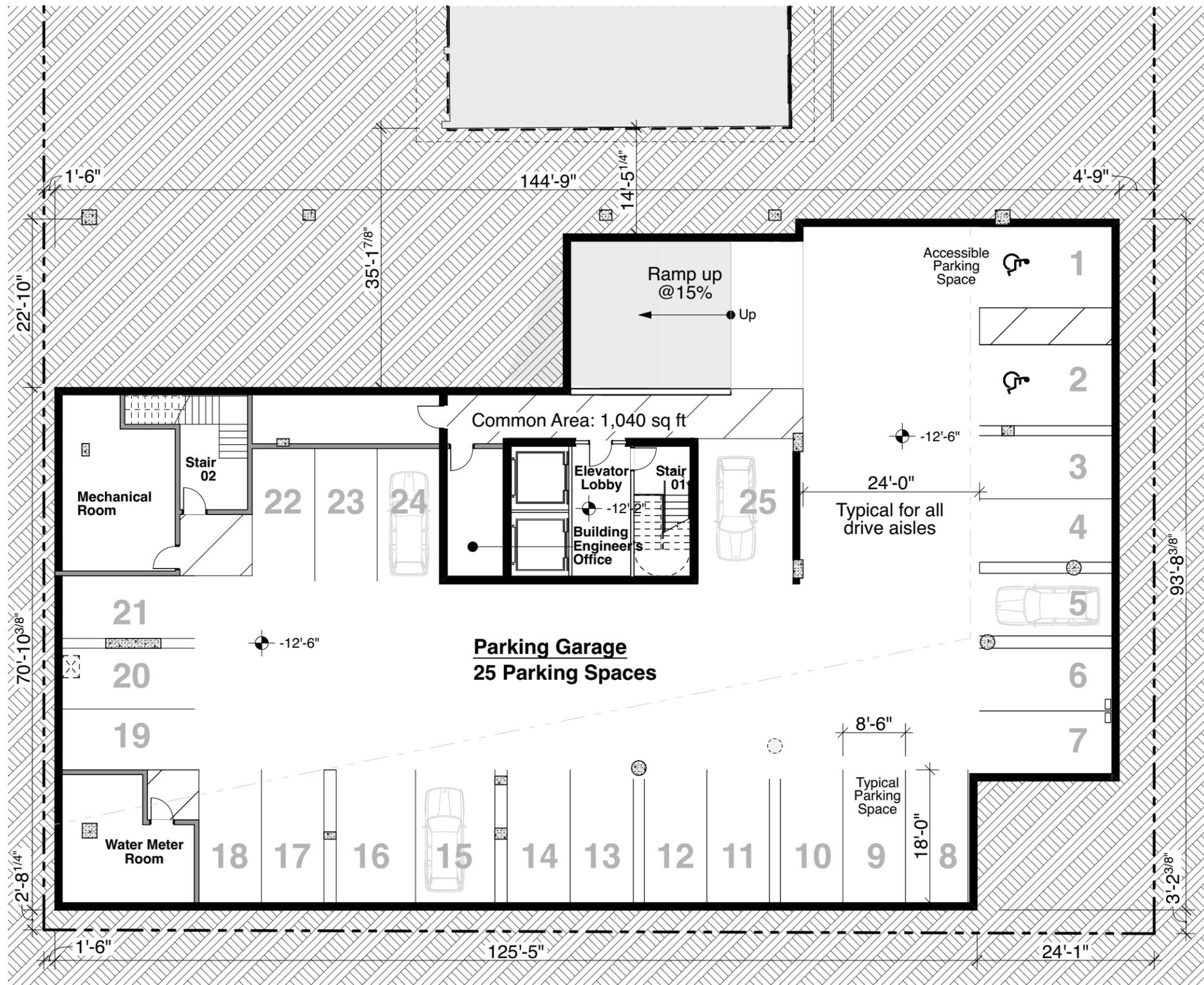


Proposal for 1900 Sherman Ave Evanston IL

Zoning District	Current C1a Proposed C1a
Area (sf)	44,961 SF
Area (acres)	1.03 Acres
Construction Type	IA or IB
Use of Structure	Residential
Total Site Area: (Combined) (Emerson) (Perlman)	244,130 sf 169,591 sf 74539 sf
Number of Dwelling Units	252 (combined)
Type of Dwelling Units Two Bedrooms	Studio, One and Two Bedrooms
Overall Dwelling Unit Density	245 units / acre
Parking Spaces plans)	39 Spaces (see plans)
Loading Dock	2 (see plans)

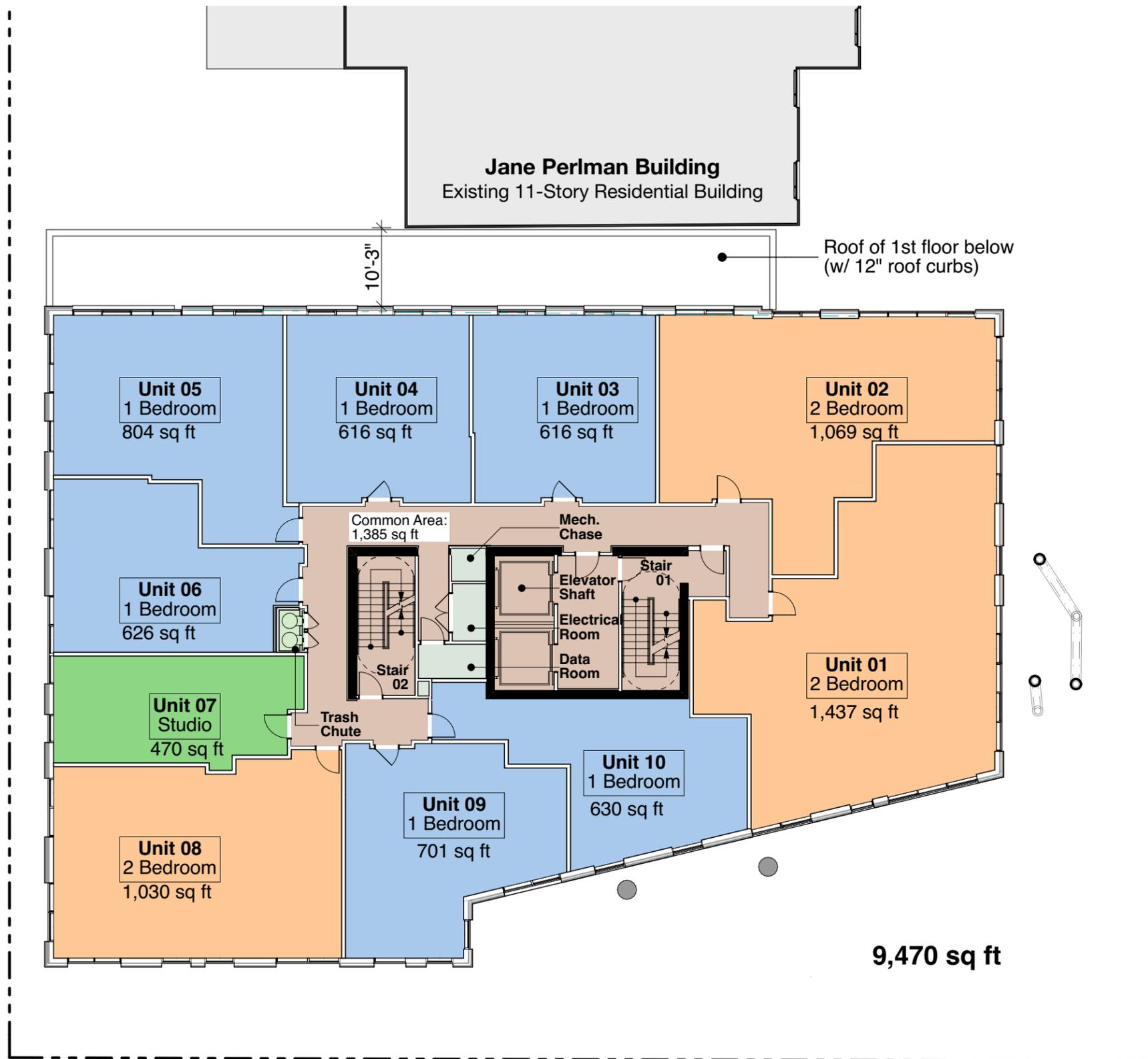






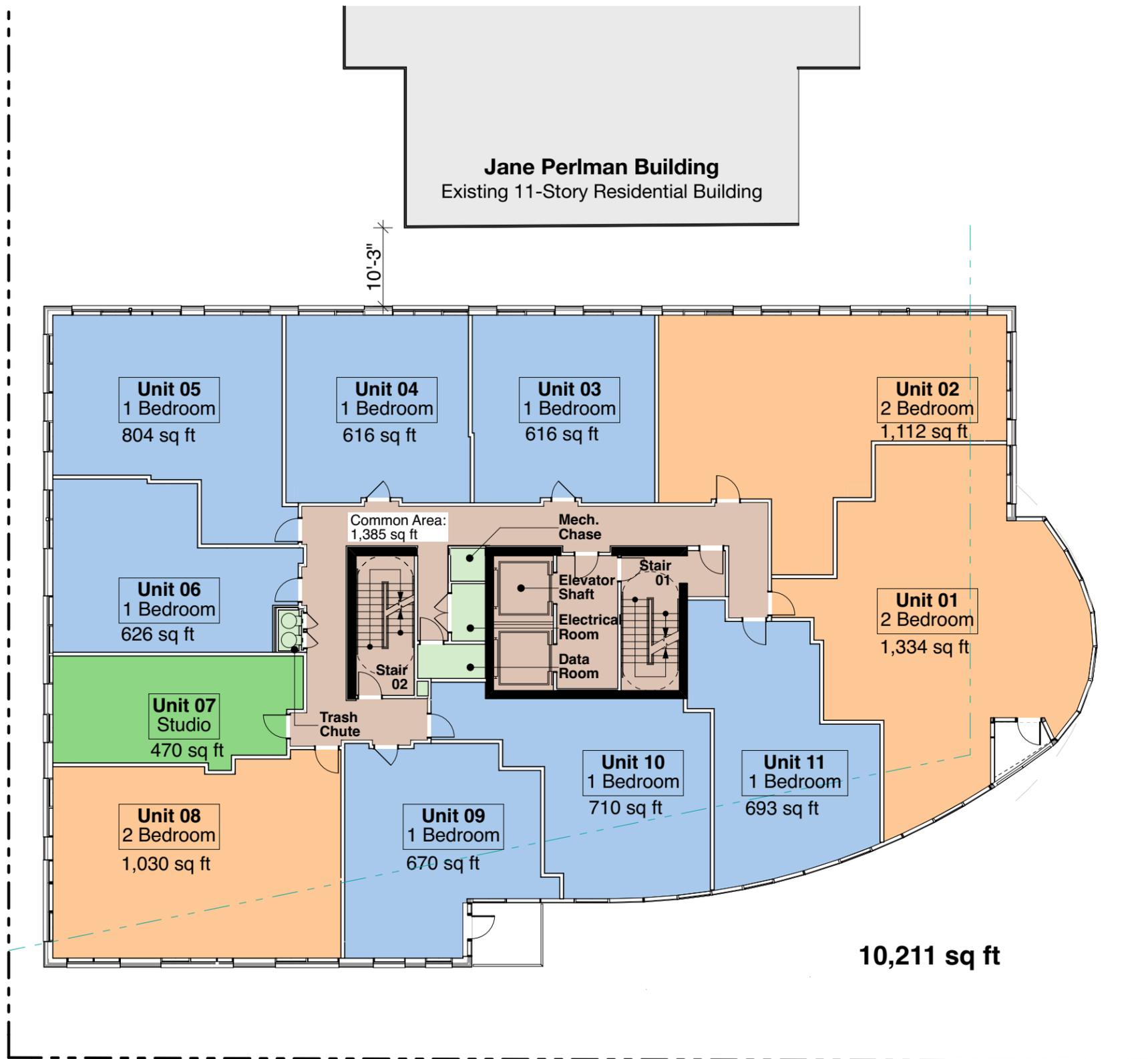
Basement Garage Plan





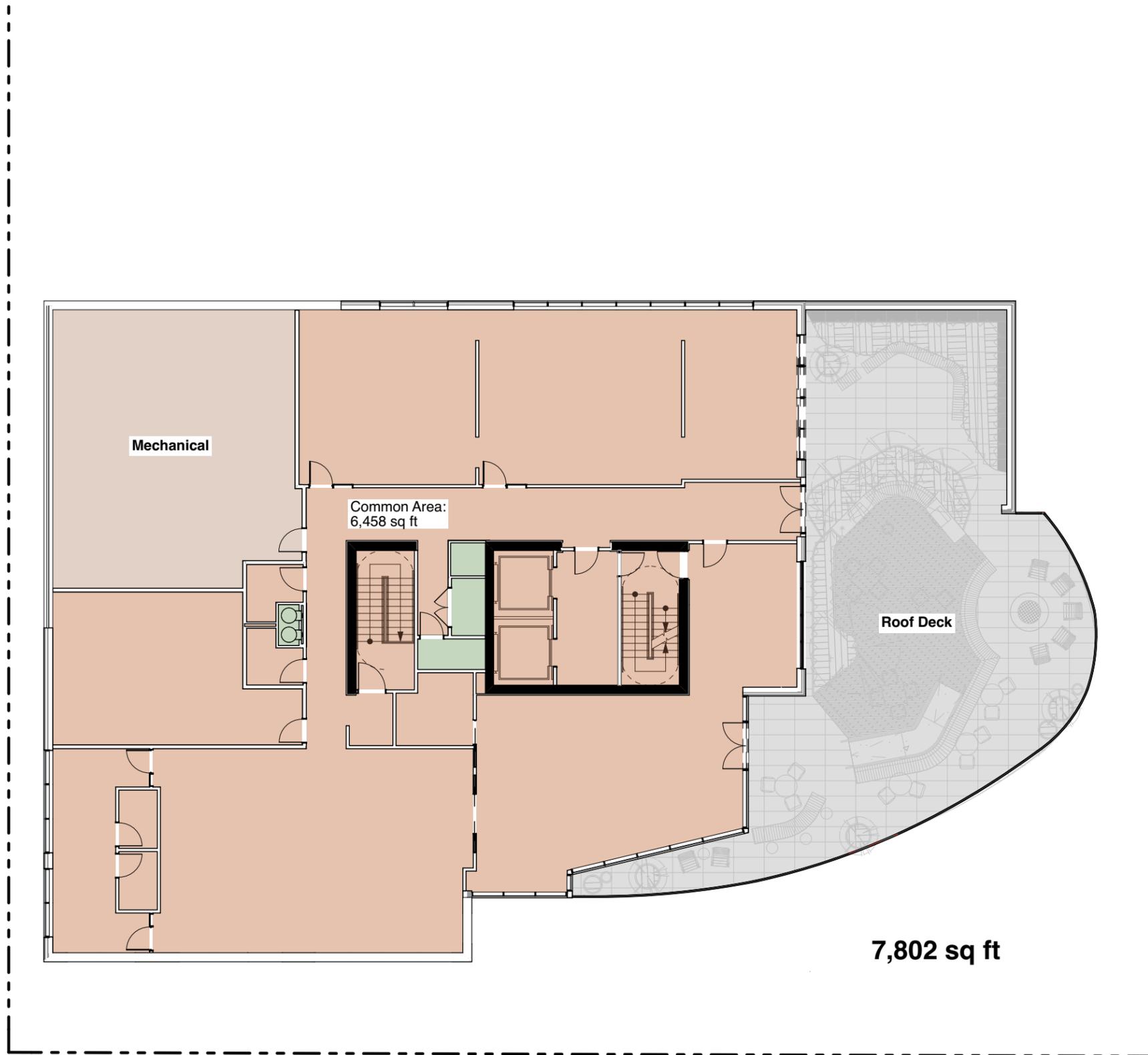
Residential Floor Plan - Level 2 & 3





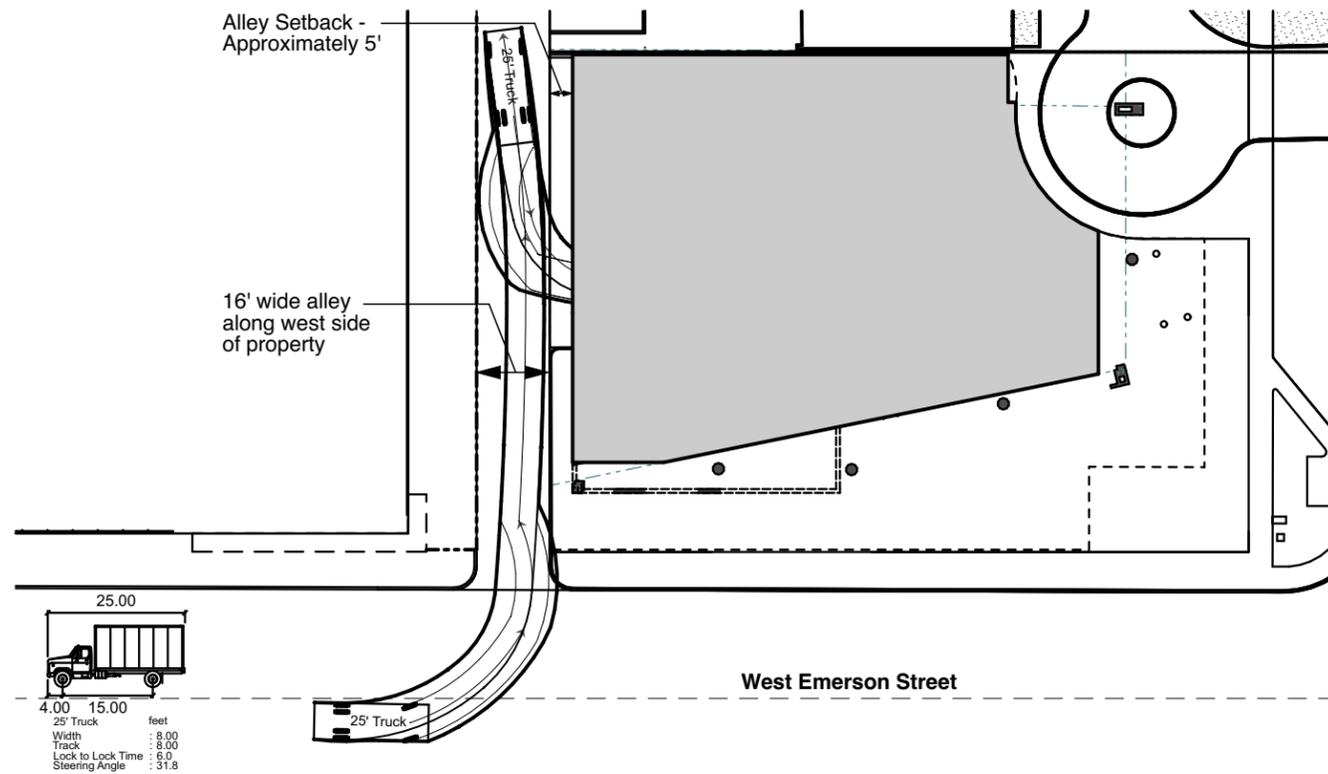
Residential Floor Plan - Level 4-15



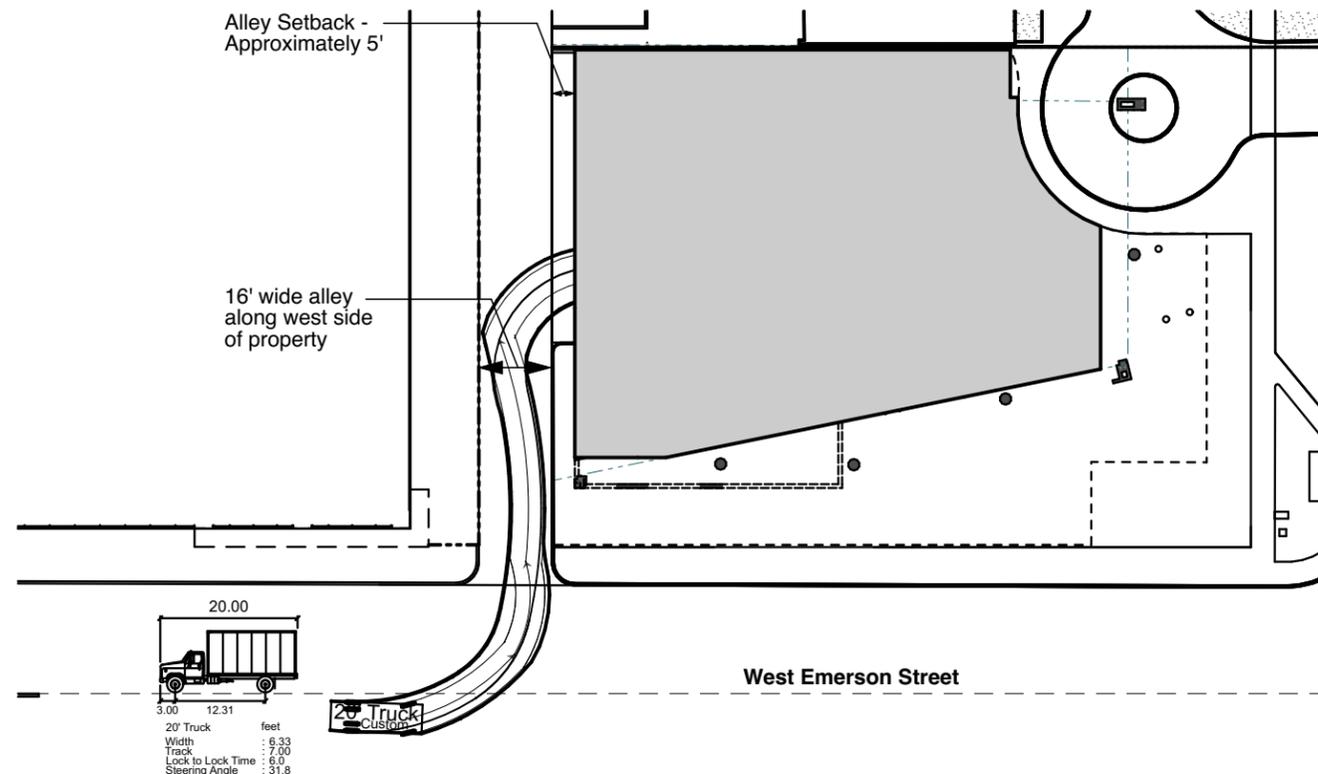


Amenity Floor Plan - Level 16

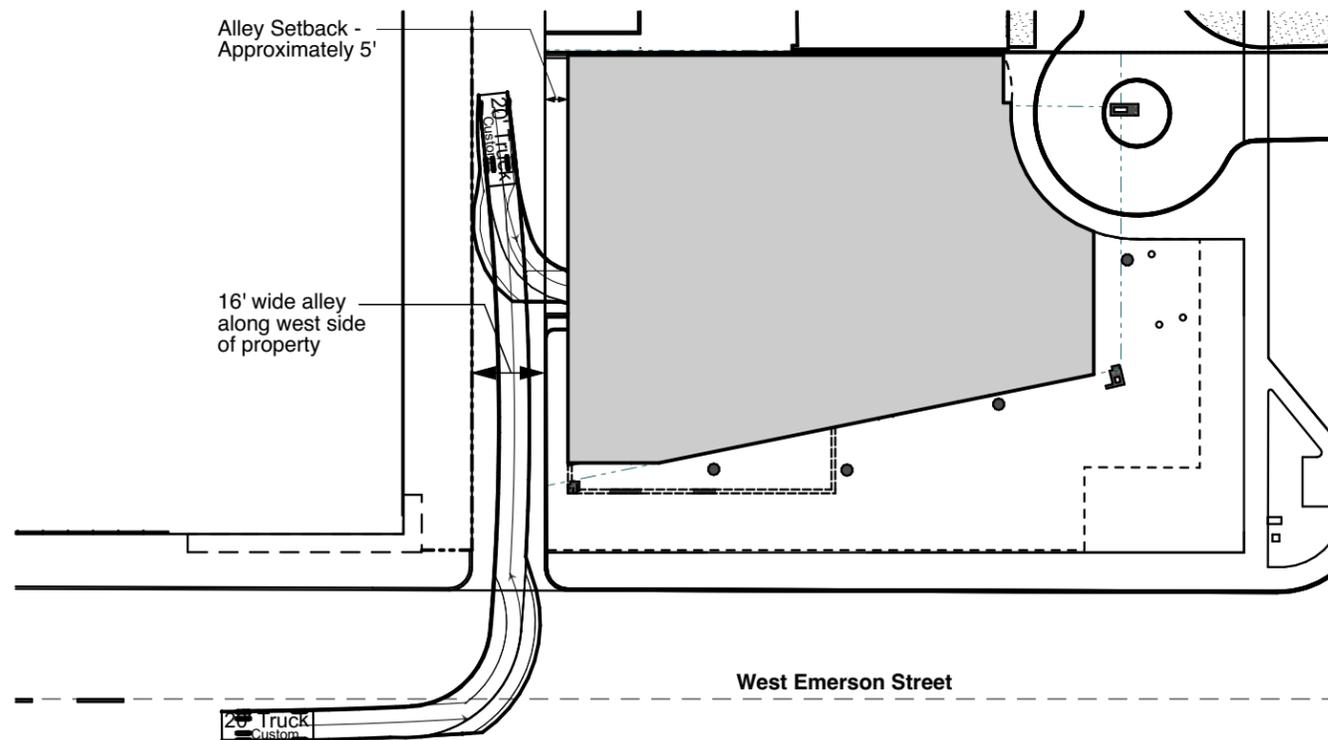




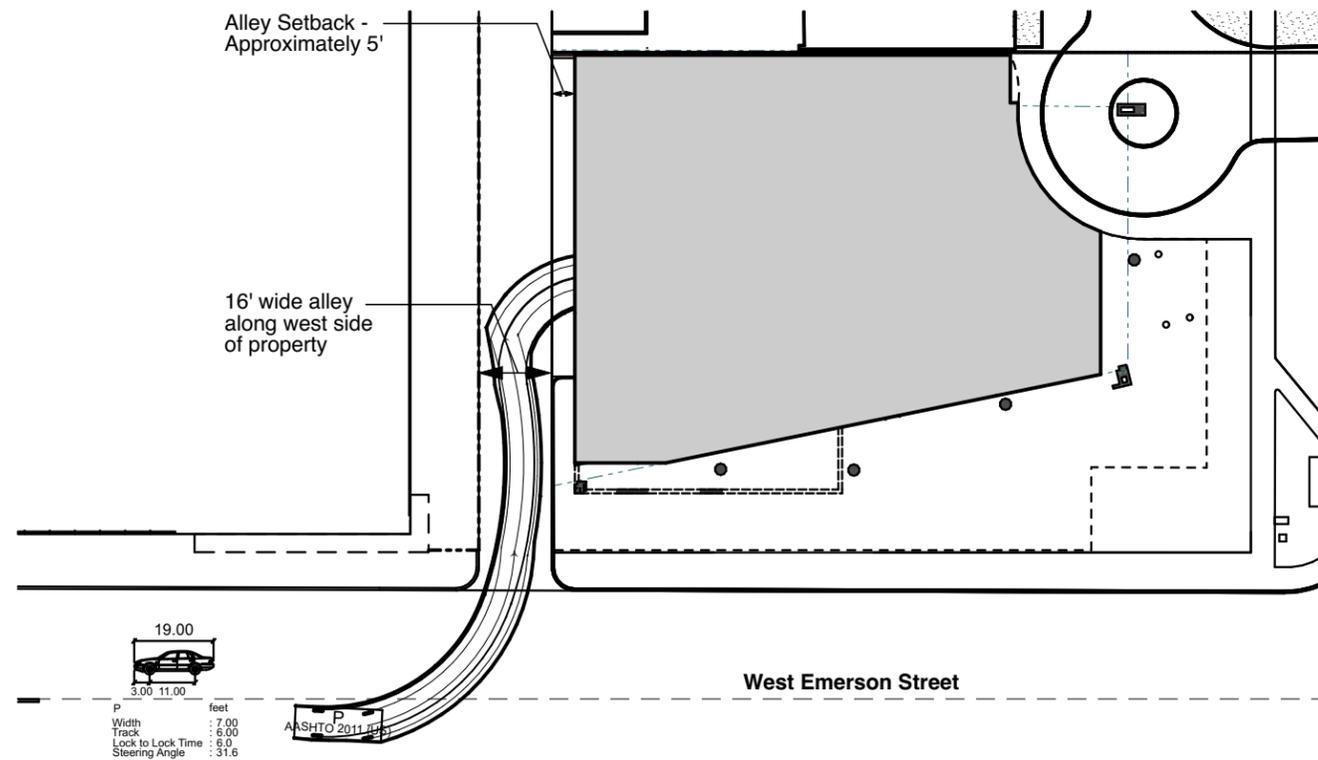
Auto-Turn Diagram - 25' Moving Truck



Auto-Turn Diagram - 20' Moving Truck

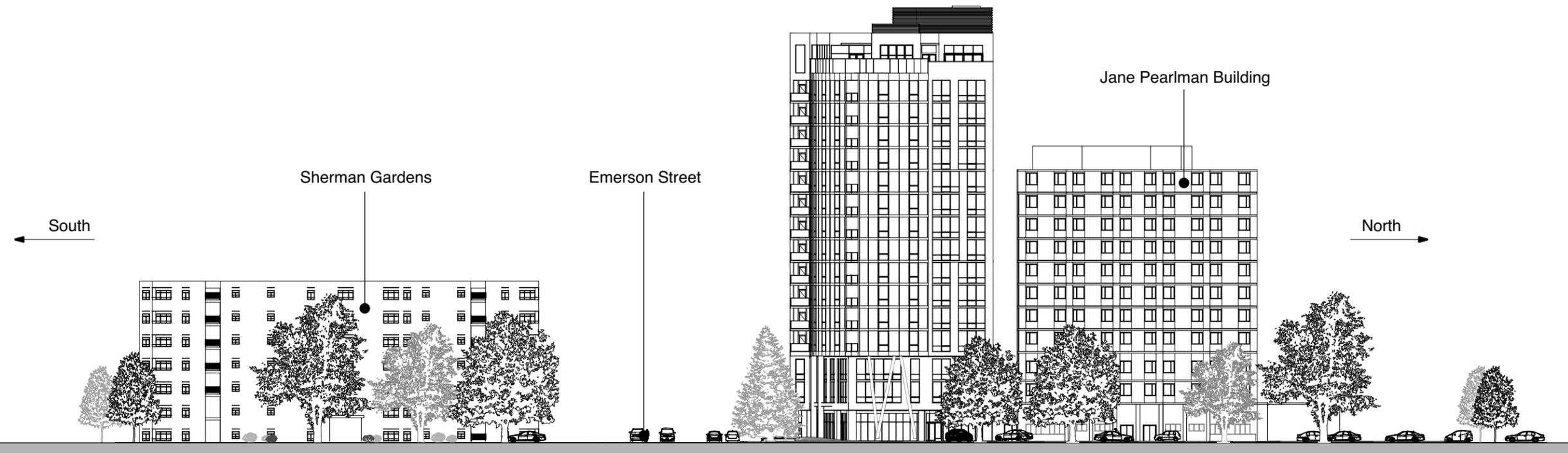


Auto-Turn Diagram - 25' Moving Truck - Alternate Approach



Auto-Turn Diagram - Passenger Vehicle

1900 Sherman (Proposed)



East Street Elevation

SCALE: 1" = 50'

1900 Sherman (Proposed)



South Street Elevation

SCALE: 1" = 50'



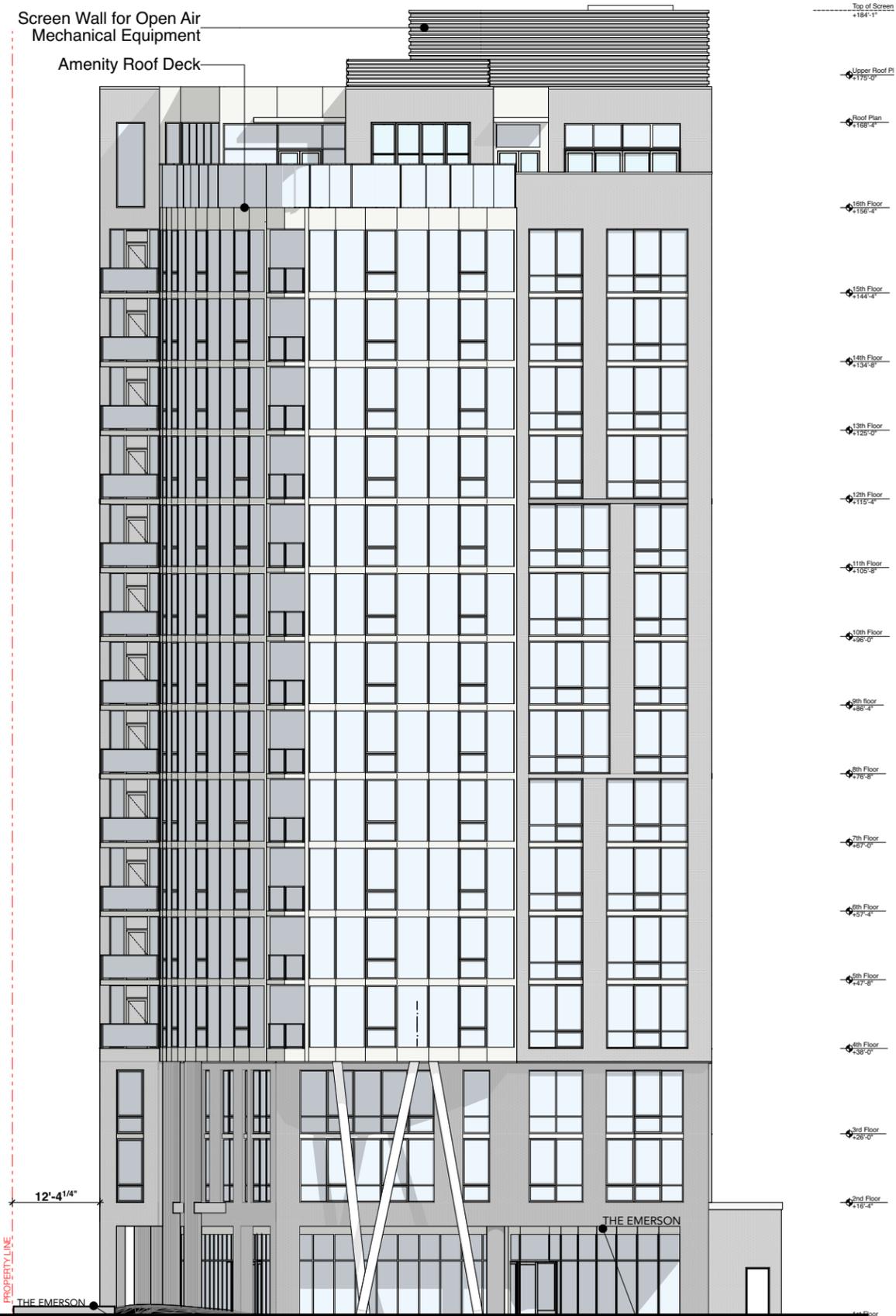
1900 Sherman Ave Evanston IL 60201

Street Elevations

Scale: 1" = 50'
9/16/21

Screen Wall for Open Air Mechanical Equipment

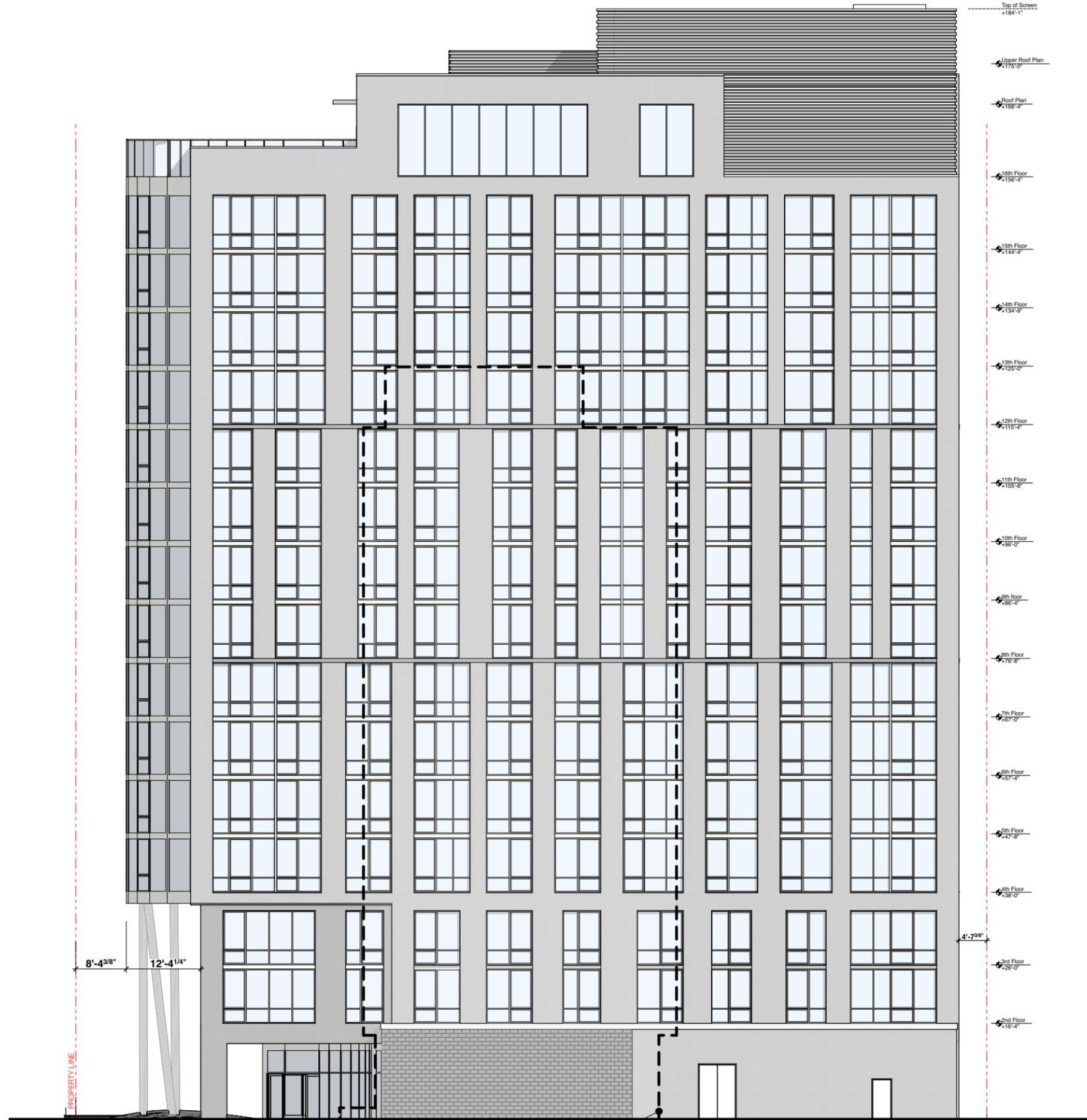
Amenity Roof Deck



East Elevation

Potential low-scale monument signage at corner

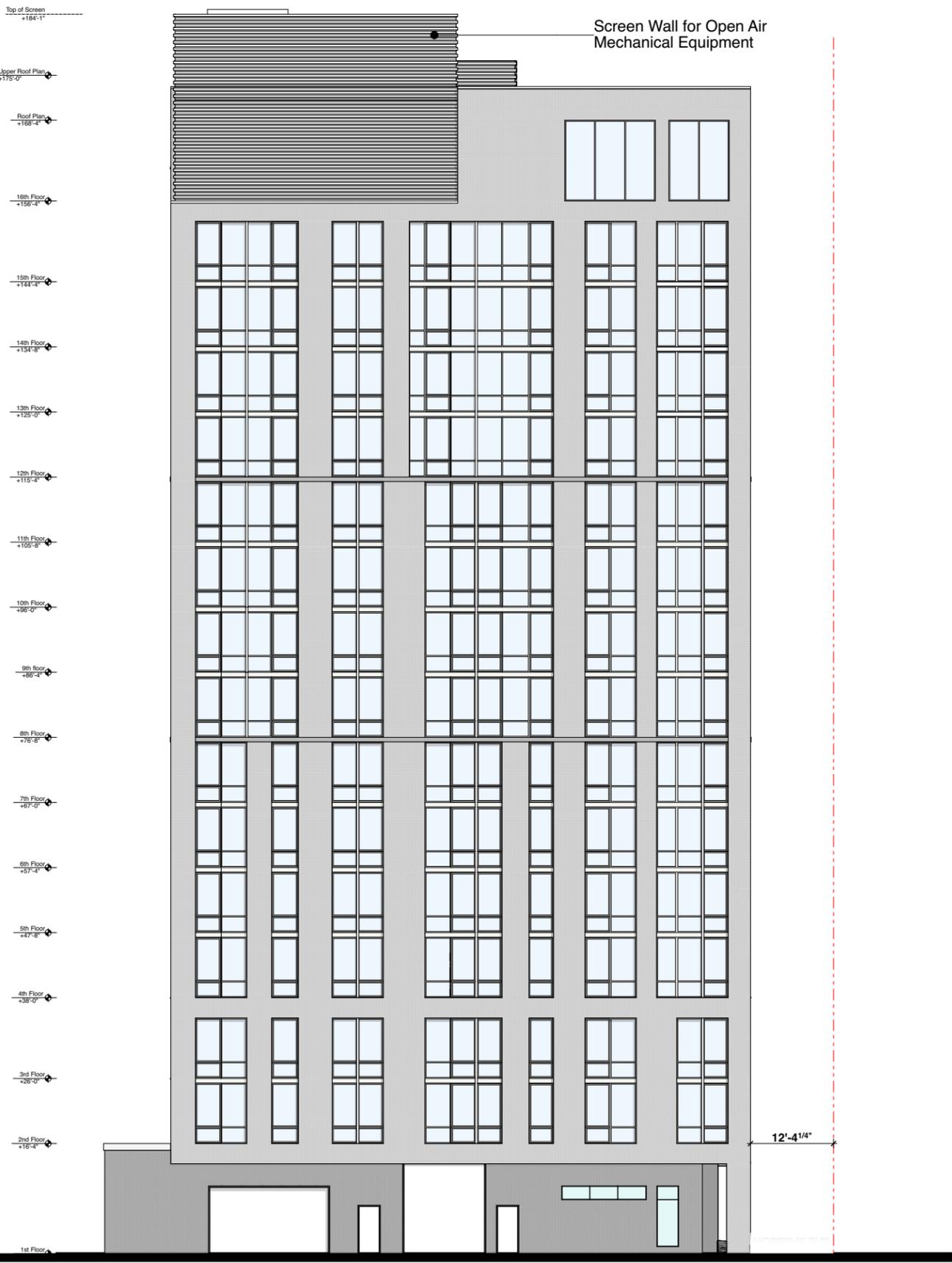
Proposed signage - name and font is a placeholder



North Elevation

Outline of Jane Perlman Apartments





West Elevation



South Elevation

Proposed signage - name and font is a placeholder

Potential low-scale monument signage at corner



1900 Sherman Ave Evanston IL 60201
South & West Elevations

Scale: 1" = 20'
9/16/21



Adjacent apartment building - The Link Evanston

Screen for mechanical equipment and elevator override

Rooftop amenity deck (refer to landscape plans for preliminary design)

Adjacent Jane Perlman building

Sherman Gardens

Porte cochere / drop off

Setback at lower floors (dashed)

Semi-public landscaping at corner

Note: as this is a diagram to identify components of the project, most trees and landscaping are not shown in full

Massing diagram to show primary components of the proposal



Context diagram to compare proposed project with other towers in the vicinity



This is an aerial rendering of the project from the southeast



This is a view of the proposed project from the west taken along Emerson Street. The landscaping within the setback and semi-public space can be seen in the foreground and along Emerson Street



This is a view of the proposed project from the north taken along Sherman Avenue. The Jane Pearlman project can be seen in the foreground.



This is a view of the proposed project from the west taken along Emerson Street. The LINK project can be seen in the foreground on the left and Sherman Gardens is across the street on the right



Community Development Department
 2100 Ridge Avenue
 Evanston, IL 60201-2798
 Ph: 874-448-4311
 Email: IHG@cityofevanston.org
www.cityofevanston.org

INCLUSIONARY HOUSING PROPOSAL

Submission Date: 8/15/15
 Applicant Name: William James, Camiros, Ltd Phone: (312)879-9518
 Applicant Address: 411 South Wells Street, Chicago, IL 60607
 Applicant Phone: _____ Cell Phone: (312) 399-4063
 E-Mail: bjames@camiros.com Website: www.camiros.com

Property Owner Name: Housing Authority of Cook County Phone: m-13544E
 Property Owner Address: 175 Jackson Boulevard, Suite 250, Chicago, IL 60604
 Property Owner Phone: 312-663-5447 Cell Phone: 312-479-2988
 E-Mail: monlo@thehcc.org Website: www.hacc.org

Project Name: Ridgely - Residential Expansion
 Project Address: 1900 Shannon Avenue, Evanston, IL 60201
 Parcel Identification Number (PIN): # 11-09-052
 Project Type: New Construction

Conversion/Addition of Residential Units

Project Located in: TOD Area Please list: Foster Purple Line Station
 Non-TOD Area

Project Description: 17-Story addition to the Jane RPerlman apartments on the existing parking lot fronting Emerson Street with 152 new dwelling units and removal of 22 existing parking spaces and replaced with 25 below ground parking spaces.

Inclusionary Housing Compliance: On-site Units
 Fee in Lieu

Project Funding Type: iBPrivate

iBPublic

If publicly funded (Federal, State, Local), list all sources of governmental assistance, including TIF, low income housing tax credits, bond financing, public grants, land disposition programs and other:

(If applicable) State Donation Tax HOA Funds

Affordable Units: Rental
 For Sale

Market Rate Units: Rental
 For Sale

Residential Units

	Total # of Units	Affordable Units
Studio	16	16
1 Bedroom	94	18
2 Bedroom	42	0
3 Bedroom		
	152	34

Unit Size (Square Footage)

	1,911 sq. ft. Units	Affordable Units
Studio	0	NA
1 Bedroom	676	676
2 Bedroom	1,111 sq. ft.	0
3 Bedroom		

Describe general location of each affordable unit within the development (attach plans including floor plans, specify size and location of affordable units)

Affordable units will be mixed among market rate units on the various floors of the building

Pricing Schedule - Market Rate Units (Estimated Sale Price or Rent Amount)

	Sale Price	Rent Amount
Studio		none
1 Bedroom		1,959
2 Bedroom		\$3,654
3 Bedroom		

For Development in TOD Areas

On-site Affordable Rental Units - Number at each Income Level and Estimated Rents

	Units at 80%AMI		Units at SP/o AMI	
	#of Units	Rent	#of Units	Rent
studio	16	\$1,185	--	--
1 Bedroom	18	\$1,27		
2 Bedroom				
3 Bedroom				

rent will be 10% of tenants income; minimum tenant income 10% of Wfo AMI

On-site Affordable For-Sale Units - Number at each Income Level and Estimated Sale Price

	Units at 80%AMI		Units at 100%AMI	
	#of Units	Sale Price	#of Units	Sale Price
Studio				
1 Bedroom				
2 Bedroom				
3 Bedroom				

Fee in Lieu of On-site Units: \$100,000 x ____ (number of units) = \$_____

For Development in Non-TOD Areas

On-site Affordable Rental Units - Number at each Income Level and Estimated Rents

	Units at 80%AMI		Units at SP/o AMI	
	#of Units	Rent	#of Units	Rent
studio				
1 Bedroom				
2 Bedroom				
3 Bedroom				

On-site Affordable For-Sale Units - Number at each Income Level and Estimated Sale Price

	Units at 80%AMI		Units at 100%AMI	
	#of Units	Sale Price	#of Units	Sale Price
Studio				
1 Bedroom				
2 Bedroom				
3 Bedroom				

Fee in Lieu of On-site Units: \$75,000 x ____ (number of units) = \$_____

If the project construction will be done in phases, provide a construction schedule for market rate and affordable units.

The developer proposes to meet the Inclusionary Housing Ordinance requirements through the attached alternative equivalent action. (The proposal must show that the alternative proposed will increase affordable housing opportunities in the City to an equal or greater extent than compliance with the Inclusionary Housing Ordinance.)

For further information visit: www.cityofevanston.org/IHO

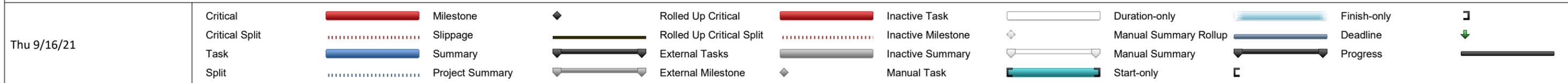
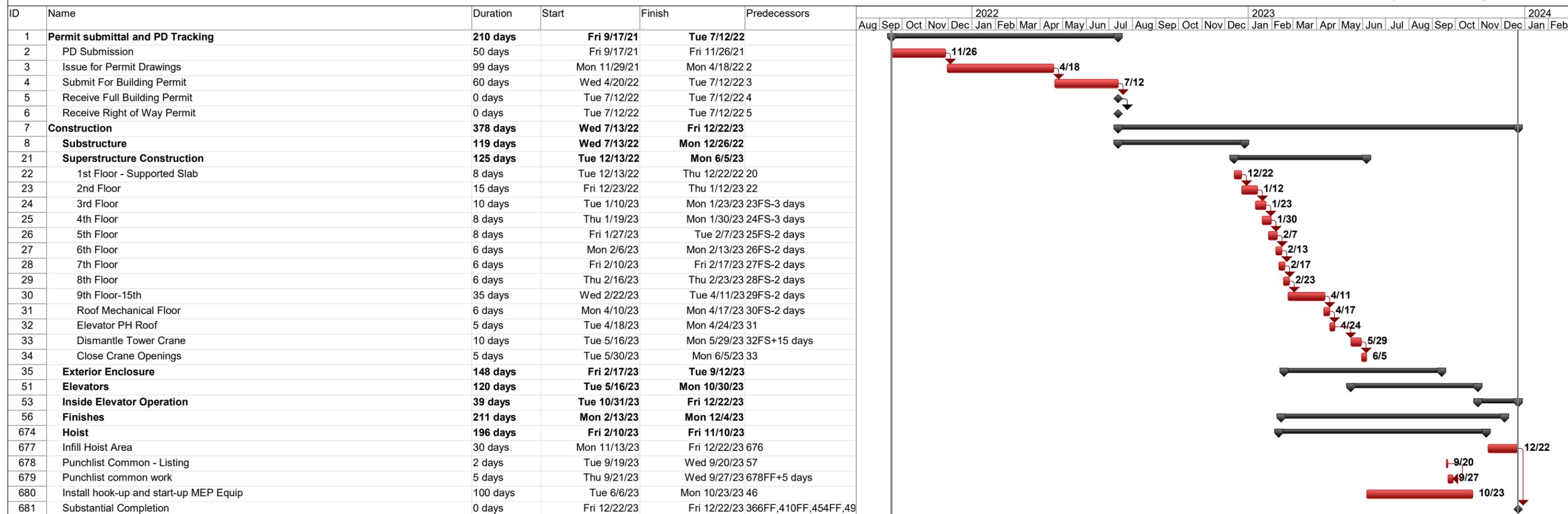
I certify that the above information is true and correct:

Print Name: William R. James Position/Title: Wife

Signature: William R. James Date: 12/1/12

Schedule of Development

Prepared by McHugh Bowa



The Emerson, 1900 Sherman Ave, Evanston IL
 Developed by; The Housing Authority of Evanston

10. Statement on Site Development Allowances

Three of the requested major adjustments require revisiting the Site Development Allowances approved in the original planned development application, as presented below.

1. An allowance for on-site parking is needed because 3 net new parking spaces are provided whereas 88 spaces are required. The City has assisted the Petitioner in identifying surplus parking spaces in nearby buildings that could be leased for use by future residents of The Emerson, which was recognized as part of the original planned development approval. This major adjustment involves a reduction of 12 parking spaces, which is largely offset by the reduction of 16 units. The Petitioner has been in contact with the owners/managers of the other nearby buildings to lease parking spaces and has a letter from the E2 Building offering to lease up to 50 spaces. The Petitioner intends to help all residents of The Emerson who want parking to find parking. Importantly, the Petitioner has made a commitment to residents of the Jane Perlman Building to provide any resident who has a car and needs parking to provide parking free of charge.
2. A major adjustment for building height is required because a building height of 168.34 feet is proposed whereas 67 feet is allowed and 97 feet is allowed with a Site Development Allowance. This application for a major adjustment in building height seeks to reduce the building height 6'-4" from that approved the plan development application. No apparent rationale exists to deny this major adjustment to reduce the building height from 172.66' in the approved planned development to 168.34 feet.
3. A major adjustment for maximum lot density is needed. Since proposed development on the site complies with the Inclusionary Housing Ordinance, a total of 196 dwelling units are allowed as of right. 252 units are requested, a reduction of 16 units from the approved planned development. A Site Development Allowance of 56 units is needed. The maximum Site Development allowance for lot density on this site is 78, thus the request is substantially less than the maximum. All of the public benefits that were part of the original planned development approval remain in the amended application, so there would be no basis for denying a lesser Site Development Allowance for lot density than that allowed in the approved planned development.

Traffic Impact & Parking Study

To: **William R. James**
Camiros

From: Dan Brinkman, PE, PTOE
Assistant Director of Transportation Services

Date: October 6, 2021

Subject: **Proposed Apartments**
1900 Sherman Avenue – Evanston, Illinois

Part I – Project Context and Summary Statement

Gewalt Hamilton Associates, Inc. (GHA) has conducted a Traffic Impact and Parking Study for the above referenced project. As proposed, the at-grade parking lot for the Jane R. Perlman apartments at 1900 Sherman Avenue in Evanston, Illinois would be redeveloped with 152 additional apartments.

The following summarizes our findings and provides various recommendations for your consideration. *Exhibits* and *Appendices* referenced are located at the end of this document. Briefly summarizing, we believe that traffic generated by the proposed apartments can be integrated on the adjacent streets. Reasons include:

- The site is expected to have minimal operational impact on the Sherman Avenue and Emerson Street intersection.
- Per US Census data, the apartments will generate a significant portion of non-auto trips (e.g., walking, ride-share, or public transportation), about 68%.
- The proposed building will add needed accessible parking in the adjacent alley.
- Total parking supply for the development will increase by 1 space.
- The combined development is expected to have a peak demand of 101 spaces, which includes residents, staff, and visitors, that will be met through a combination of on-site parking and offsite arrangements with adjacent and nearby buildings.

Part II - Background Information

Site Location Map, Existing Traffic Operations, and Photo Inventory

Exhibit 1 provides a location map, ***Exhibit 2*** illustrates the existing street traffic operations, and ***Appendix A*** provides a photo inventory of the site vicinity. Pertinent comments regarding land uses in the site vicinity and transportation components, both vehicular and non-auto mobility include:

Land Uses

- The approximate 45,100-square foot site is located on the northwest corner of the Sherman Avenue and Emerson Street intersection in Evanston, Illinois.
- The site currently contains an 11-story residential building with 100 units.
- The surrounding area predominately consists of multi-residential housing.

- Sherman Avenue, south of Emerson Street, is mostly mixed-use development.
- To the immediate west of the site, the 9-story “Link” mixed-use building was recently constructed and opened in the fall of 2019. The building consists of 241 residential apartment units with a 7-Eleven convenience store. The apartments are fully leased.

Street System

Sherman Avenue

- Sherman Avenue is a north-south street that is classified as a “Major Collector” on the IDOT Functional Classification map south of Emerson Street and as a “Local Street” north of Emerson Street.
- Sherman Avenue has one travel lane in each direction.
- On-street parallel parking spaces are provided on both sides of the street. In the site vicinity, two-hour parking is allowed between the hours of 9 AM and 6 PM, excluding Sunday and Holidays.
- At the existing drives of the site, the northern drive serves as enter-only while the southern drive serves as exit-only. At the southern drive, left turns onto Sherman Avenue are permitted.
- Sherman Avenue has a posted speed limit of 25 miles-per-hour (mph).

Emerson Street

- Emerson Street is an east-west street that is classified as a “Minor Arterial” on the IDOT Functional Classification map.
- Emerson Street has one travel lane in each direction.
- East of Sherman Avenue, parking is allowed on the north side of the street. A permit is required between the hours of 9 AM and 8 PM excluding Sunday. West of Sherman Avenue, two-hour parking is allowed on both sides of the street between the hours of 9 AM and 6 PM, excluding Sunday and Holidays.

Public Alley

- Public Alley runs north-south and has its northern terminus at Foster Street and its southern terminus at Emerson Street. At both intersections, no signage or pavement markings are provided.
- The posted speed limit is 15-mph.
- Access to the site’s parking lot is currently restricted via a metal gate.

Non-Auto Mobility

- The CTA Purple Line is to the west with the closest station located on Foster Street, less than 2 blocks from the site. Another station, located on Davis Street, is about half a mile to the south.
- Next to the Davis Street CTA station is a Metra Station, where Metra operates the Union Pacific North line.
- Pace operates route 93 (California Dodge) to the west with stops about 2 blocks west of the site on Emerson Street.
- Northwestern University offers the Intercampus shuttle service that travels north along Sherman Avenue with stops at its intersections with Emerson Street and Foster Street.
- Sidewalks are provided on both sides of Sherman Avenue as well as Emerson Street.
- Crosswalks are provided on all legs of the signalized intersection of Sherman Avenue and Emerson Street. Pedestrian signals are also provided.
- A crosswalk is provided on Emerson Street, south of the site, with signage.

Existing Traffic

GHA conducted weekday morning (6:00-9:00 AM) and evening (4:00-7:00 PM) peak period turning movement counts at the Sherman Avenue intersections with Emerson Street and the site driveways on Wednesday, July 10, 2019 (see **Appendix B**). **Exhibit 3** illustrates the existing weekday morning and evening peak hour traffic volumes and the number of pedestrians crossing the intersection, which occurred from 7:45-8:45 AM and from 4:30-5:30 PM. The 24-hour or Average Daily Traffic (ADT) volumes are also provided as published on the IDOT website. No unusual events occurred during the traffic counts, such as road construction, excessive emergency vehicle activity or severe weather, that would have affected the traffic volumes or travel patterns.

Notes:

1. The multi-use building to the west of the site was under construction at the time of the GHA traffic counts. As such, existing traffic does not reflect traffic from that development.
2. The July 2019 counts were compared with April 2018 counts conducted on behalf of IDOT along both Emerson Street and Sherman Avenue. The July 2019 data was higher than the previous year's April data for both directions of Sherman and Emerson, suggesting no additional adjustments are needed to account for traffic changes associated with nearby Northwestern University.

Historical 2018 data can also be found in **Appendix B**.

Part III – Project Traffic Characteristics

Site Plan

Exhibit 4 depicts the preliminary site plan prepared by Papageorge Haymes Architects, Inc. dated August 2, 2021. As proposed, there will be an addition of 152 apartments. The existing at-grade parking lot would be replaced with an underground parking garage. Access to the new parking garage would be provided on the north-south alleyway west of Sherman Avenue.

Project Traffic Generations and Trip Distribution

Exhibit 5 – Part A tabulates the traffic generation calculations for the proposed residential development. Traffic generations are based on historically observed trip rate data published by the Institute of Transportation Engineers (ITE) in the most recent, 10th Edition of the manual *Trip Generation*. The trips generated by Senior Adult Housing (ITE LUC 252) were compared with trips generated by traditional Multifamily Apartments (ITE LUC 222). As can be seen, the multifamily apartments result in more trips than the senior adult housing. For a conservative approach, the trips for the multifamily apartments were selected.

Discussion Point. **Exhibit 5 – Part A** also shows the anticipated volume reductions associated with published US Census data. As can be seen, there could be a 68% reduction for multi-modal trips (e.g., walking, ride-share, or public transportation). However, this discount was not taken to help ensure that the maximum site traffic impacts are tested.

The proposed development is expected to generate 55 and 60 (combined entering and exiting) trips during the weekday morning and evening peak hours respectively, and 810 trips on a daily or 24-hour basis. The ITE trip generation pages for the residential housing are provided in **Appendix C**.

Exhibit 5 – *Part B* presents the anticipated trip distribution, which is primarily based on the expected vehicle patterns, as well as the proposed site access.

Part IV – Traffic Evaluation

Traffic Assignments

IDOT and other agencies generally require that the existing volumes be increased to reflect other growth in the area for a “Buildout + 5 year” analysis. Assuming a buildout year of 2022, the analysis would be for the Year 2027. The Chicago Metropolitan Agency for Planning (CMAP) was contacted and provided Year 2050 traffic projections (see **Appendix D**). As can be seen, Sherman Avenue and Emerson Street are both projected to have minimal growth. An annual compounded growth rate of $\pm 1\%$ was applied along Sherman Avenue and Emerson Street in order to provide a conservative analysis scenario.

Site traffic was assigned to the adjacent streets based on the project characteristics (see **Exhibit 5**) and is illustrated in **Exhibit 6**. The 2027 No-Build traffic volumes are shown in **Exhibit 7**. *Note: it can be assumed that traffic for the adjacent multi-use building (The Link) was included in CMAP projected future traffic conditions.*

Site traffic volumes were added to the projected traffic growth volumes to produce the Year 2027 total traffic assignment, which is illustrated in **Exhibit 8**. *Note: traffic for the current Jane R. Perlman apartment building was readjusted based on the relocation of the parking lot access from Sherman Avenue to the alleyway.*

Discussion Point. The 68% multi-modal trip discount was not taken for site traffic. Thus, the total traffic volumes (see **Exhibit 8**) are most likely overstated.

Intersection Capacity and Queue Analyses

Capacity analyses are a standard measurement in the transportation industry that identifies how an intersection operates. **Exhibit 9 – Part A** lists the analysis parameters, as published in the Transportation Research Board’s (TRB) Highway Capacity Manual – 6th Edition, 2016 (HCM). They are measured in terms of level of service (LOS). LOS A is the best rating, with LOS F being the worst. LOS C is considered appropriate for “design” purposes and LOS D is usually considered as providing the lower threshold of “acceptable” operations. LOS E and F are usually considered unacceptable.

Exhibit 9 - Part B summarizes the intersection capacity results. The capacity analysis summary printouts are provided in **Appendix E**.

Key Finding. As can be seen from **Exhibit 9**, site traffic will have a minimal impact on operations at the Sherman Avenue and Emerson Street intersection, as the additional intersection delays will be less than 1 second per vehicle. The all individual movements and overall intersection operations are expected to continue to function at or above LOS B.

Traffic Impact Discussion

Site traffic will represent the following volumes traveling through the Sherman Avenue / Emerson Street intersection:

- During the morning peak hour (see **Exhibit 3**), there are currently about 530 vehicles or about 9 vehicles per minute. The apartments would add only 26 trips or less than 1 trip every 2 minutes.
- During the evening peak hour, there are currently about 668 vehicles or about 11 vehicles per minute. The apartments would add only 29 trips or less than 1 trip every 2 minutes.

Key Finding. Based on the above, it can be concluded that no street or intersection improvements would be necessary to specifically accommodate site traffic.

Part V – Parking Analyses

Existing Characteristics

The existing Jane R. Perlman apartment building consists of 100 units. Parking is provided via an at-grade parking lot with 22 parking spaces. 16 additional parking spaces are also available behind the building, along the public alleyway. The total number of existing parking spaces is 38.

Per information provided from the Housing Authority of Cook County (HACC), twelve of the existing residents own vehicles and three parking spaces along the alleyway are used by management. The excess spaces are used for visitors.

Proposed Improvements

As part of the construction of the proposed multi-story senior living apartment building, which would add an additional 152 apartment units, the existing at-grade parking lot would be replaced with an underground parking garage (see **Exhibit 4**). The underground parking garage would provide 25 parking spaces.

The 16 parking spaces located behind the existing building would be reduced by 2 to 14 spaces to provide for needed handicapped accessible spaces. The total number of proposed parking spaces is 39.

Existing Parking Demand Calculations

Using the information provided from HACC, 12-owned vehicles for 100 occupied units, GHA calculated the parking demand for affordable units at 0.12 spaces per unit.

To establish a local parking demand for the proposed market-rate units (101 units) GHA contacted operators of both the adjacent Link building and the E2 apartment building further to the west.

Based on conversation with the Link operator, the building is 99% leased (210 of 211 units) and those tenants currently utilize/ lease only 61 of the 161 spaces available. That equates to a demand of 0.29 spaces per unit.

Based on conversation with the E2 operator, the building is approximately 90% leased (± 318 of 353 units) and those tenants as well as some day parking average a demand of 178 spaces. Conservatively, a calculated demand for 0.56 spaces per unit.

Future Parking Demand Calculations

The proposed building will have 152 total units. Of those 152 units, some will not be market rate. The number of affordable units in the proposed new building will be 34 1 BR and Studio units at 50% AMI or less and 17 units at 80% AMI or less. Using the calculated demand (0.12 spaces per unit) from the Perlman building to establish demand for the new affordable units equates to a demand of 6.12 ~6 spaces (34+17 = 51*0.12).

Using an average of the stated Link and E2 parking demands of 0.42 spaces per unit, the remaining units in the proposed building would require 42 spaces (0.42 *101).

Visitor parking information was calculated based on rate information published by the Urban Land Institute *Shared Parking* – 2nd Edition. Peak visitor parking demand for residential uses occur at a rate of 0.15 spaces per unit. Thus, the peak visitor parking demand would be 23 spaces for the 152-unit development and another 15 for the Perlman building.

Exhibit 10 summarizes the visitor parking space calculations for the new building based on the ULI publication. As can be seen, the peak visitor demands on both weekdays and weekends will tend to occur from 7 to 10 PM. Less than 10 visitor spaces are projected to be needed from 6 AM to 5 PM.

To be conservative, we have also included visitor parking for the existing building at the ULI recommended rate of 0.15 spaces per unit for a total of 15 spaces.

The following table, summarizes the total estimated parking demand for the existing Perlman building and the proposed addition.

Development Component	Units	Parking Demand Spaces per unit	Estimated Peak Demand
<i>HAAC Staff</i>	N/A	N/A	3
<i>Perlman Building Residents</i>	100	0.12	12
<i>Perlman Building Visitors</i>	100	0.15	15
<i>Proposed Affordable Units</i>	51	0.12	6
<i>Proposed Market Rate Units</i>	101	0.42	42
<i>Visitors new building</i>	152	0.15	23
		Total Peak Demand	101

As can be seen, the estimated peak demand for parking for the existing Perlman and proposed residential building is 101 spaces. The site will provide a total of 25 underground and 14 surface spaces for a total of 39 spaces, resulting in a peak deficit of 62 spaces.

Key Finding. The proposed parking supply of 39 spaces will not meet the anticipated peak demands of the building. However, it is our understanding that both Link and E2 are willing to lease additional spaces to tenants of the new building. Based on information provided by Link and E2 there are 100 spaces (Link) and ±175 spaces (E2) generally available within a short walk from the proposed building.

Bicycle Parking

Per the client, bicycle racks will be provided on site with storage for 64 bicycles. The City of Evanston does not have any specific requirements regarding the number of bicycle spaces that should be provided. Accordingly, GHA referenced the City of Chicago Zoning Ordinance to determine how many bicycle spaces should be provided on site. Per § 17-10-0207 of the Chicago Zoning Ordinance, 1 bicycle space should be provided for every 4 auto spaces for elderly housing land uses.

Key Finding. Using the City of Chicago requirements for bicycle parking, a minimum of 23 bicycle spaces would be required ($91 \text{ demand} / 4 = 22.75 \sim 23$). Based on the similarities with Chicago, the proposed bicycle parking supply of 64 spaces should be more than adequate for the proposed apartments.

Part VI - Technical Addendum

The following *Exhibits* and *Appendices* were previously referenced. They provide technical support for our observations, findings, and recommendations discussed in the text.

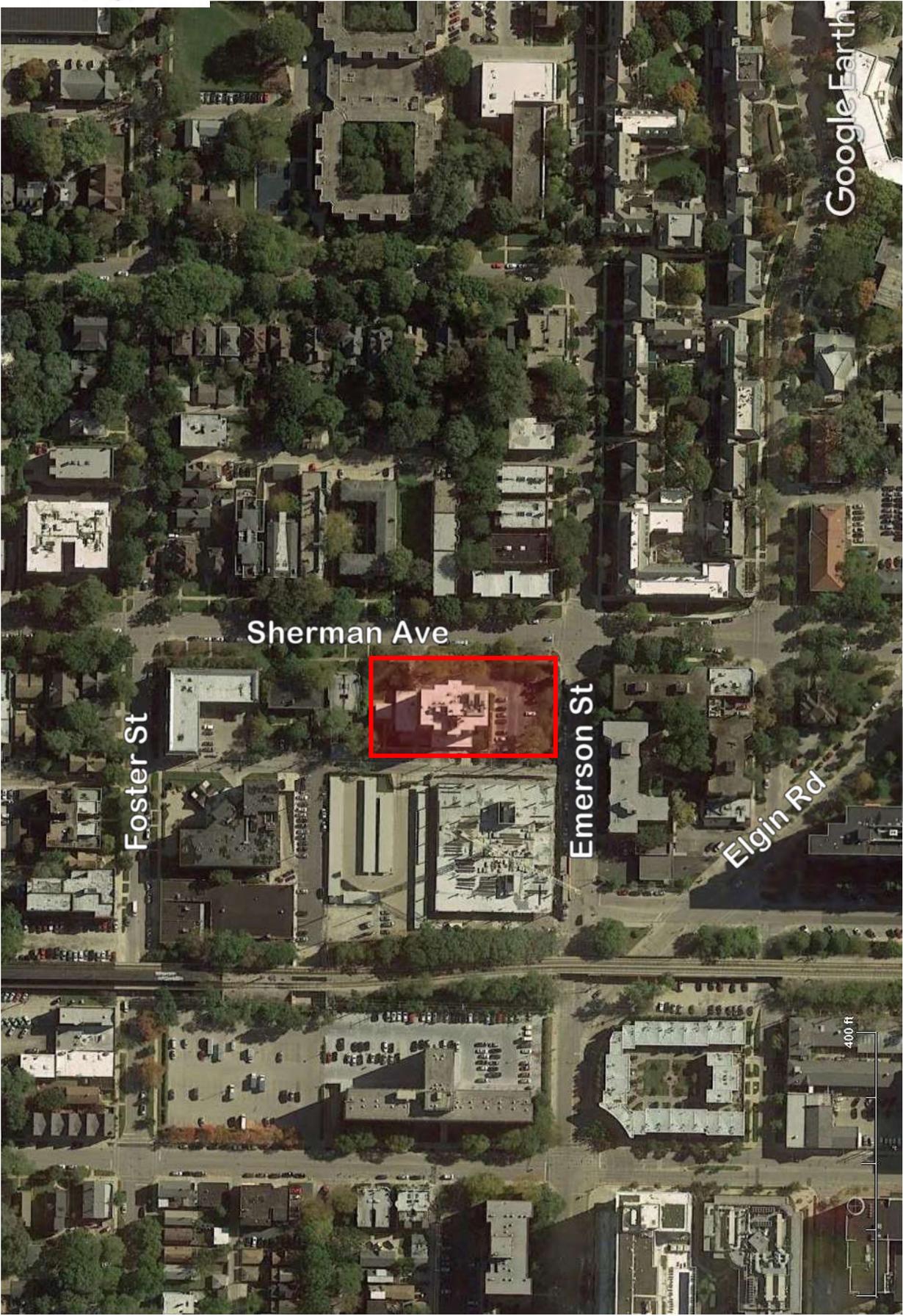
Exhibits

1. Site Location Map
2. Existing Traffic Operations
3. Existing Traffic
4. Site Plan
5. Project Traffic Characteristics
6. Site Traffic
7. No-Build Traffic
8. Total Traffic
9. Intersection Capacity Analyses
10. Visitor Parking Demand Calculations

Appendices

- A. Photo Inventory
- B. Traffic Count Summaries
- C. ITE Trip Generation Pages
- D. CMAP Correspondence
- E. Capacity Analysis Worksheets

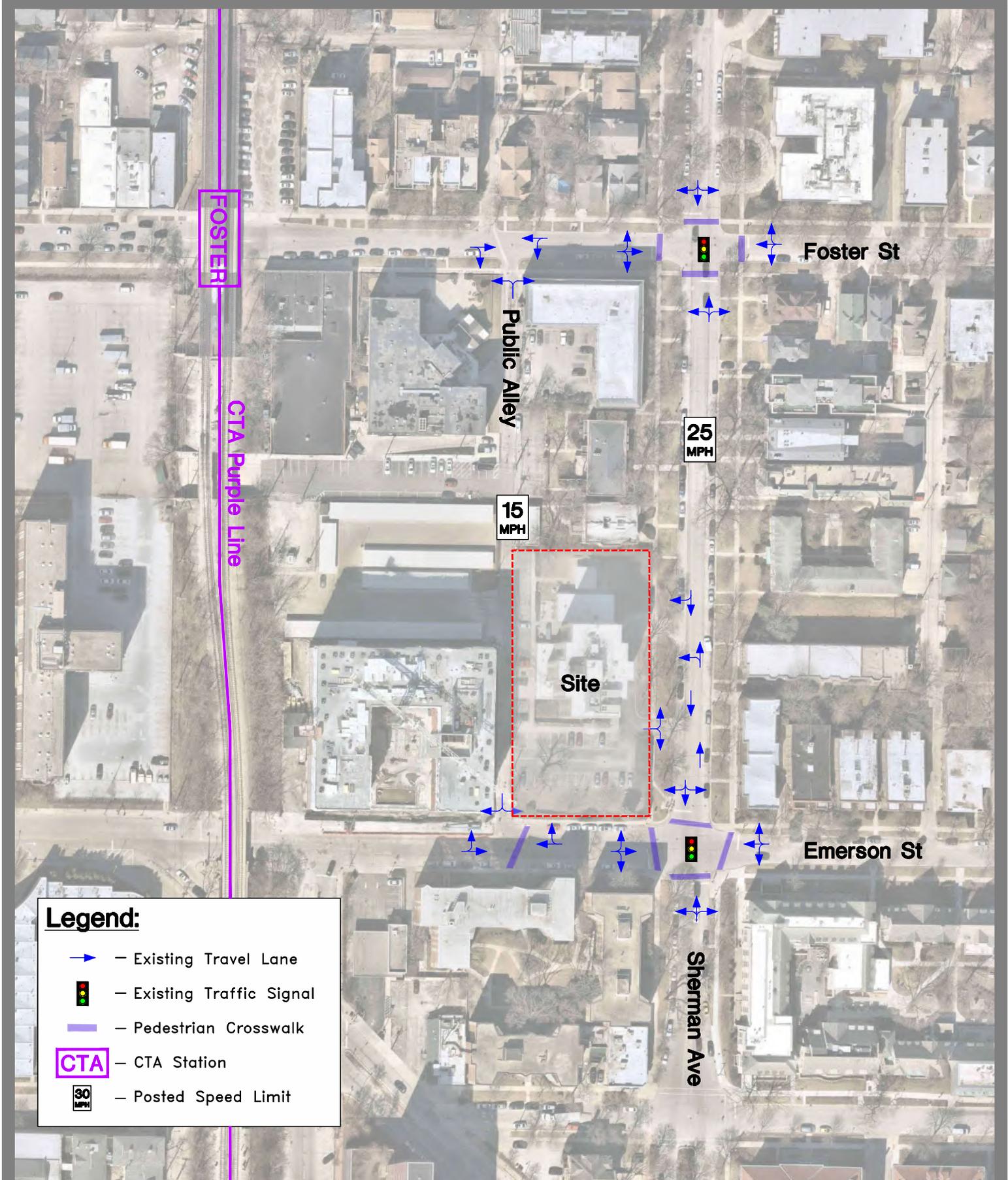
EXHIBITS



Proposed Housing Development – 1900 Sherman Avenue, Evanston, Illinois



Exhibit 1
Site Location Map

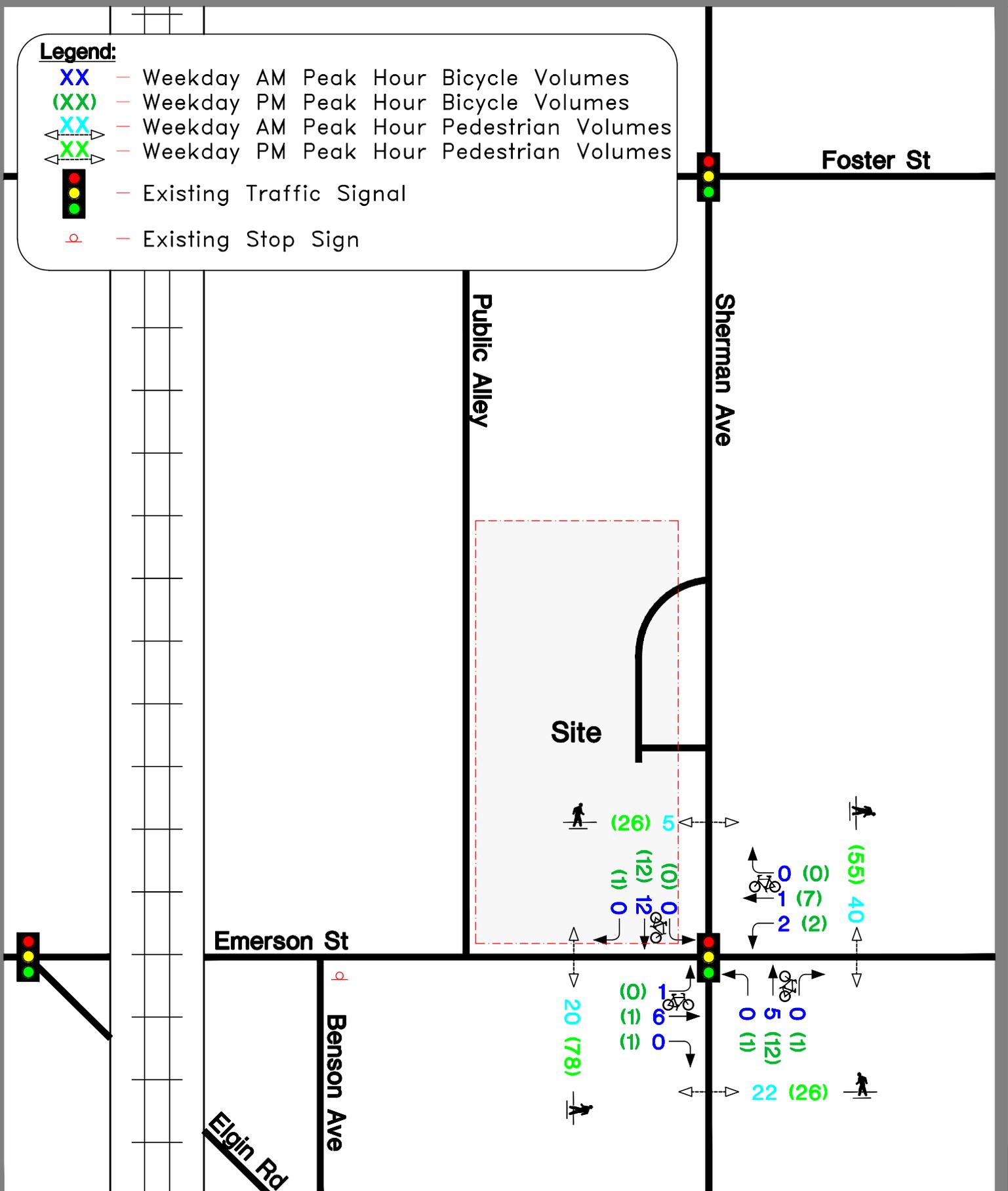


Legend:

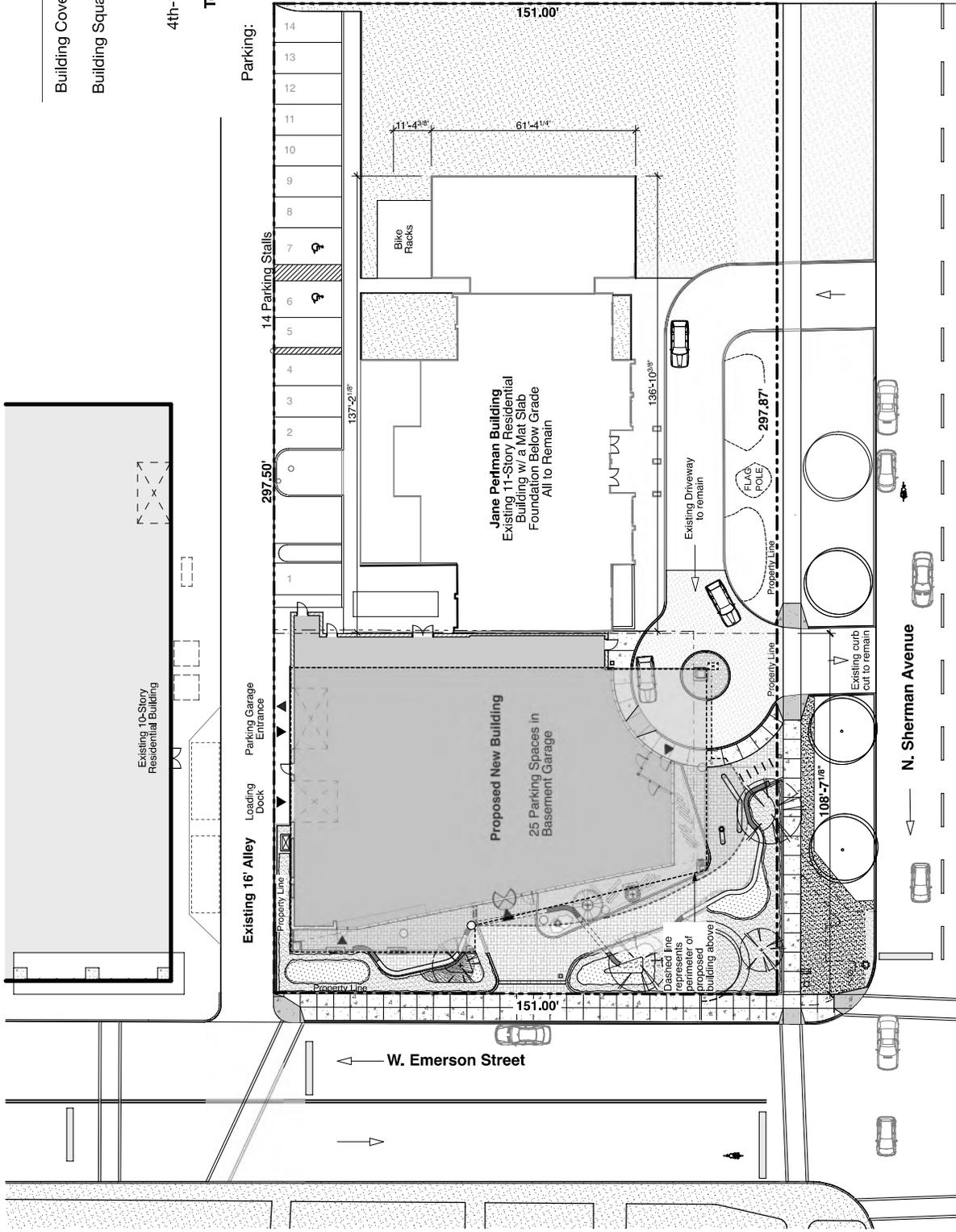
-  - Existing Travel Lane
-  - Existing Traffic Signal
-  - Pedestrian Crosswalk
-  - CTA Station
-  - Posted Speed Limit

Legend:

- XX - Weekday AM Peak Hour Bicycle Volumes
- (XX) - Weekday PM Peak Hour Bicycle Volumes
- XX - Weekday AM Peak Hour Pedestrian Volumes
- XX - Weekday PM Peak Hour Pedestrian Volumes
-  - Existing Traffic Signal
-  - Existing Stop Sign



Site Plan



Site Data Table

Building Coverage:

Building Square Footage: B – 11,273 SF
 1st – 8,813 SF
 2nd – 9,470 SF
 3rd – 9,470 SF
 4th-15th (12 floors) – 10,211 SF
 16th – 7,695 SF
Total GSF Area: 169,253 SF

14 Alley + 25 Garage
Total: 39 Parking Spots



1900 Sherman Ave Evanston IL 60201

Site Plan

Scale: 1" = 30'
8/2/21

Exhibit 4 - Site Plan



Exhibit 5
Project Traffic Characteristics
Proposed Apartments - Evanston, Illinois

Part A. Traffic Generation Calculations

Land Use	Size	ITE Code	Weekday Peak Hours						Daily Sum
			Morning Peak Hour		Evening Peak Hour		Daily Sum		
			In	Out	In	Out	In	Out	
Senior Adult Housing-Attached	152 Dwelling Units	#252	11	19	21	18	39	39	650
Multifamily Housing (High-Rise)	152 Dwelling Units	#222	13	42	37	23	60	60	810
Multi-Modal Reduction @ 68% =			4	13	12	7	19	19	259
Discussion: The discount for non-auto trips was <u>not</u> taken, to help ensure that the maximum site traffic impacts are tested.									
			Projected Trips =	13	42	37	23	60	874

Notes:

- 1) Source: Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition).
- 2) Multifamily Housing (High-Rise) was chosen as a conservative estimate.
- 3) Per US Census for Tract 8087.02, 8093, 8094, and 8095, 68% of residents carpool, take public transportation, or walk.

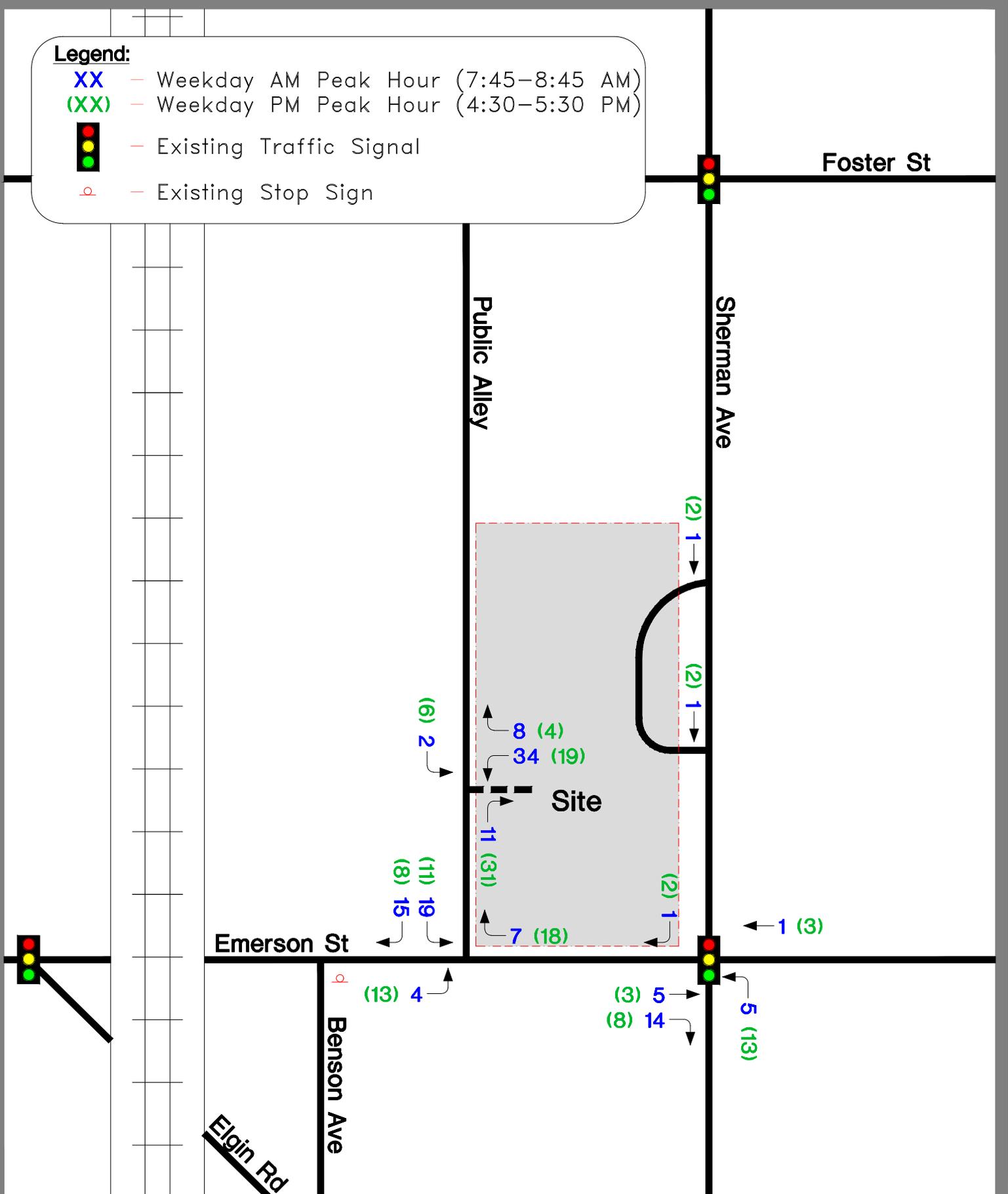
Part B. Trip Distribution

Route & Direction	Percent Use To/From Site
Emerson Street	
- West of Site	35%
- East of Site	10%
Sherman Avenue	
- South of Site	35%
- North of Site	20%
Totals =	100%



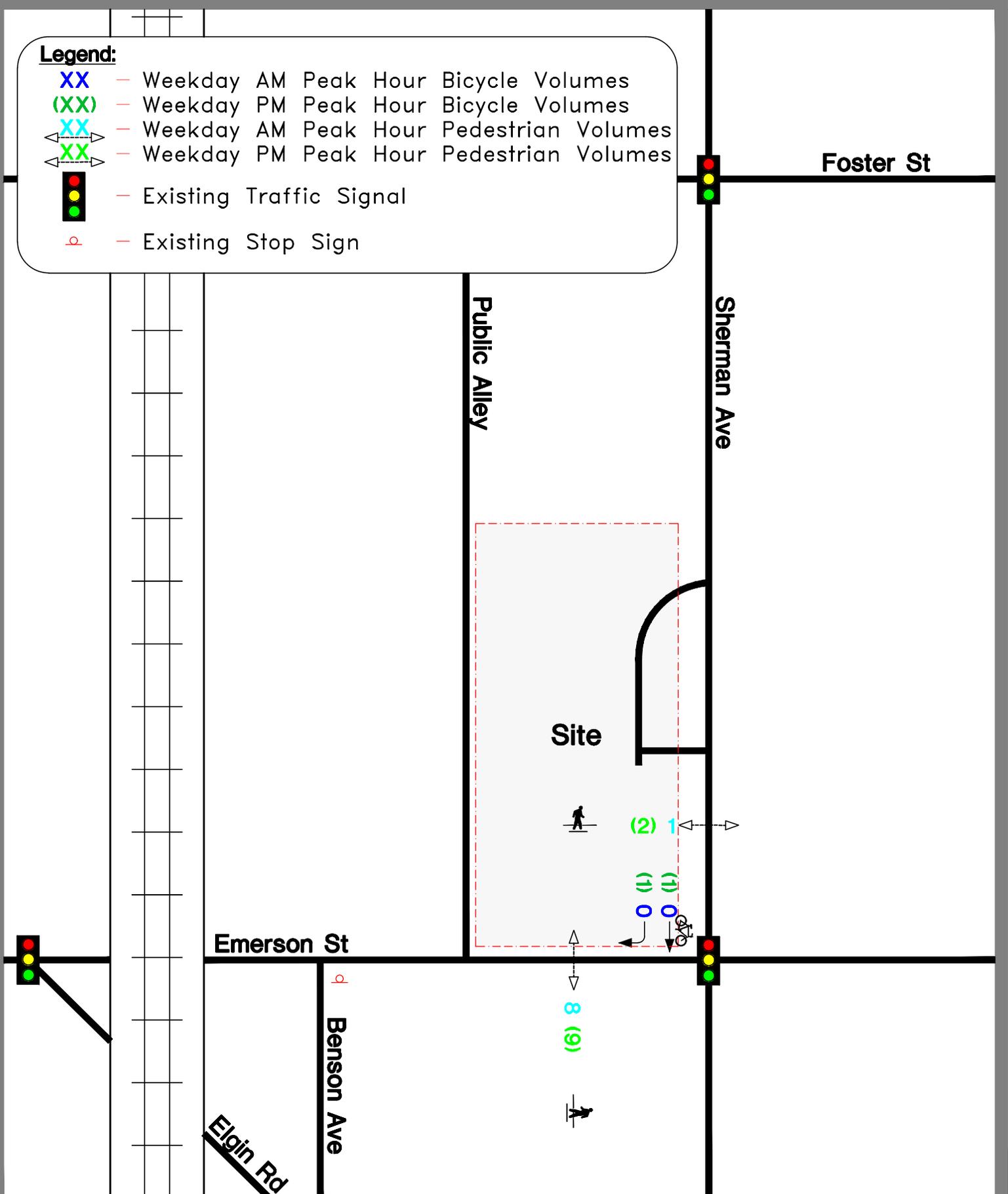
Legend:

- XX — Weekday AM Peak Hour (7:45–8:45 AM)
- (XX) — Weekday PM Peak Hour (4:30–5:30 PM)
-  — Existing Traffic Signal
-  — Existing Stop Sign



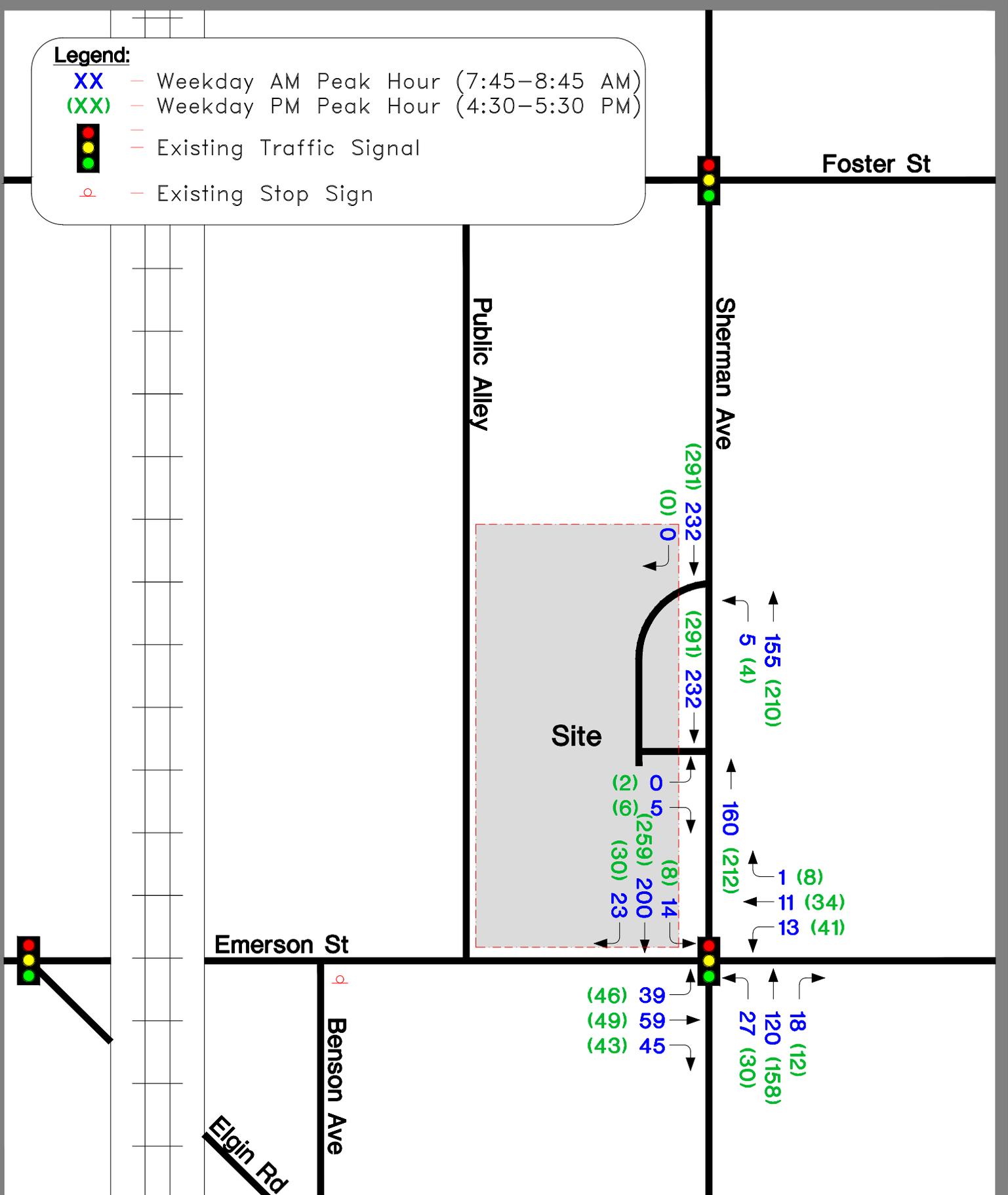
Legend:

- XX — Weekday AM Peak Hour Bicycle Volumes
- (XX) — Weekday PM Peak Hour Bicycle Volumes
- XX — Weekday AM Peak Hour Pedestrian Volumes
- XX — Weekday PM Peak Hour Pedestrian Volumes
-  — Existing Traffic Signal
-  — Existing Stop Sign



Legend:

- XX — Weekday AM Peak Hour (7:45–8:45 AM)
- (XX) — Weekday PM Peak Hour (4:30–5:30 PM)
-  — Existing Traffic Signal
-  — Existing Stop Sign



Legend:

- XX - Weekday AM Peak Hour Bicycle Volumes
- (XX) - Weekday PM Peak Hour Bicycle Volumes
- XX \leftrightarrow - Weekday AM Peak Hour Pedestrian Volumes
- XX \leftrightarrow - Weekday PM Peak Hour Pedestrian Volumes
-  - Existing Traffic Signal
-  - Existing Stop Sign

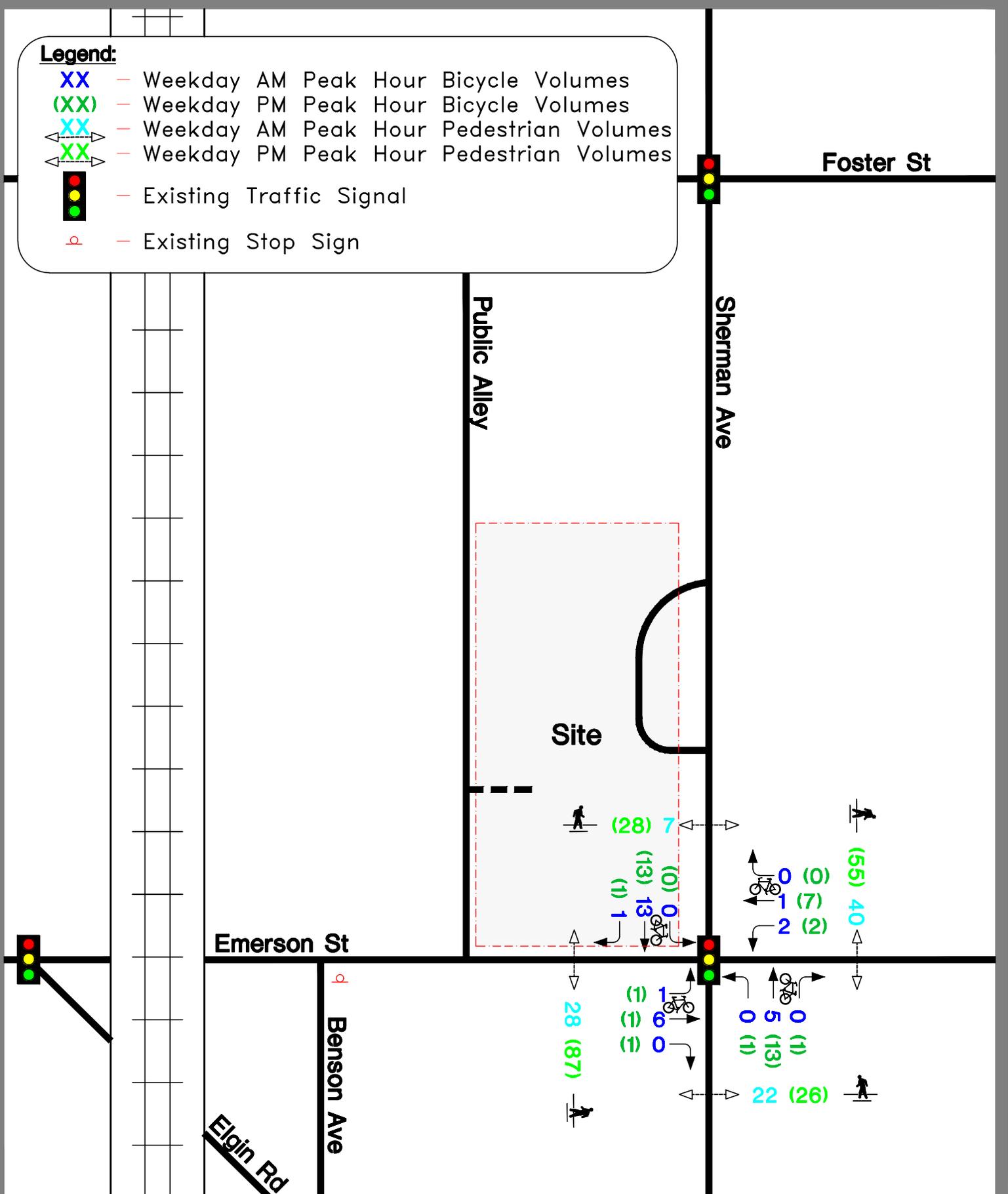


Exhibit 9

Intersection Capacity Analyses

Sherman Avenue & Emerson Street - Evanston, Illinois

Part A. Parameters - Type of Traffic Control (Source: Highway Capacity Manual 6th Edition)

I. Traffic Signals

LOS **Delay (sec./veh)**

A ≤ 10

B >10 and ≤ 20

C >20 and ≤ 35

D >35 and ≤ 55

E >55 and ≤ 80

F >80

Description

All signal phases clear waiting vehicles without delay

Minimal delay experienced on select signal phases

Some delay experienced on several phases; often used as design criteria

Usually considered as the acceptable delay standard

Very long delays experienced during the peak hours

Unacceptable delays experienced throughout the peak hours

Part B. Results

Roadway Conditions	LOS Per Movement Group By Approach												Intersection / Approach				
	> = Shared Lane - = Non Critical or not Allowed Movement																
	Eastbound			Westbound			Northbound			Southbound			Delay (sec / veh)	LOS			
Traffic Signal	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	Intersection Delay				
1. Sherman Ave at Emerson St																	
A. Weekday Morning Peak Hour																	
Existing Traffic (See Exhibit 3A)	>	B	<	>	B	<	>	A	<	>	B	<	>	B	<	10.4	B
2027 No Build Traffic (See Exhibit 7)	-	58	-	-	9	-	-	56	-	-	83	-	-	83	-		
2027 Total Traffic (See Exhibit 8A)	>	B	<	>	B	<	>	A	<	>	B	<	>	B	<	10.6	B
B. Weekday Evening Peak Hour																	
Existing Traffic (See Exhibit 3A)	>	B	<	>	B	<	>	A	<	>	B	<	>	B	<	10.8	B
2027 No Build Traffic (See Exhibit 7)	-	50	-	-	29	-	-	61	-	-	97	-	-	97	-		
2027 Total Traffic (See Exhibit 8A)	>	B	<	>	B	<	>	A	<	>	B	<	>	B	<	10.8	B
2027 Total Traffic (See Exhibit 8A)	-	55	-	-	31	-	-	66	-	-	107	-	-	107	-		
2027 Total Traffic (See Exhibit 8A)	>	B	<	>	B	<	>	A	<	>	B	<	>	B	<	10.9	B
2027 Total Traffic (See Exhibit 8A)	-	59	-	-	32	-	-	72	-	-	107	-	-	107	-		

Exhibit 10
Visitor Parking Demand Calculations
Proposed Apartments
1900 Sherman Avenue - Evanston, IL

Assumptions:

- a) 152 units
- b) Per ULI Table 2.2 maximum visitors generated daily is 23 spaces.

<u>Time</u>	<u>Weekday (Peak Visitors = 23)</u>		<u>Weekend Peak Visitors = 23)</u>	
	<u>Hourly Factor</u>	<u>Demand Spaces</u>	<u>Hourly Factor</u>	<u>Demand Spaces</u>
6:00 AM	0%	0.0	0%	0.0
7:00 AM	10%	2.3	20%	4.6
8:00 AM	20%	4.6	20%	4.6
9:00 AM	20%	4.6	20%	4.6
10:00 AM	20%	4.6	20%	4.6
11:00 AM	20%	4.6	20%	4.6
12:00 PM	20%	4.6	20%	4.6
1:00 PM	20%	4.6	20%	4.6
2:00 PM	20%	4.6	20%	4.6
3:00 PM	20%	4.6	20%	4.6
4:00 PM	20%	4.6	20%	4.6
5:00 PM	40%	9.2	40%	9.2
6:00 PM	60%	13.8	60%	13.8
7:00 PM	100%	23.0	100%	23.0
8:00 PM	100%	23.0	100%	23.0
9:00 PM	100%	23.0	100%	23.0
10:00 PM	100%	23.0	100%	23.0
11:00 PM	80%	18.4	80%	18.4
12:00 AM	50%	11.5	50%	11.5

Source: ULI Shared parking - 2nd Edition; Tables 2.1-2.6

APPENDIX A
Photo Inventory



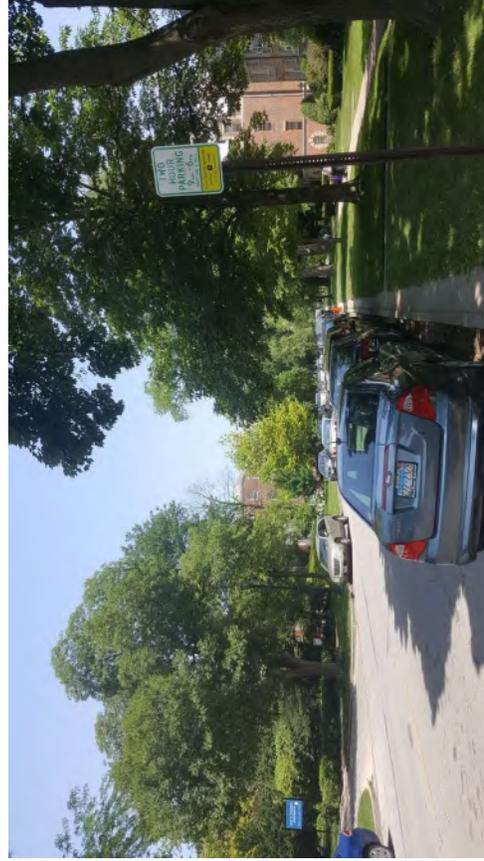
Looking west across Sherman Ave at site's current entrance



Looking west across Sherman Ave at site's current exit



Looking south along Sherman Ave in front of site



Looking north along Sherman Ave in front of site



Looking south along Sherman Ave at the intersection with Emerson St



Looking north along Sherman Ave at the intersection with Emerson St



Looking west along Emerson St at the intersection with Sherman Ave



Looking east along Emerson St at the intersection with Sherman Ave



Looking west along Emerson St south of site



Looking north from Emerson St at alley behind site



Looking north along alley behind site



Looking south along alley behind site



Parking restriction signage on Sherman Ave



Parking restriction signage on Emerson St



Looking west along Foster St at the intersection with Sherman Ave



Looking west along Foster St towards CTA station

APPENDIX B
Traffic Count Summary Sheets

Gewalt Hamilton Associates Inc.
625 Forest Edge Drive

Count Name: Emerson St & Sherman Ave
Site Code:
Start Date: 07/10/2019
Page No: 1

Vernon Hills, Illinois, United States 60061
(847) 478-9700 poster@gha-engineers.com

Turning Movement Data

Start Time	Sherman Southbound				Emerson Westbound				Sherman Northbound				Emerson Eastbound				Int. Total		
	Right	Thru	Left	U-Turn	Peds	App. Total	Right	Thru	Left	U-Turn	Peds	App. Total	Right	Thru	Left	U-Turn		Peds	App. Total
6:00 AM	2	7	0	0	0	9	0	1	1	0	1	2	2	4	3	0	0	1	9
6:15 AM	1	13	1	0	0	15	0	1	1	0	5	2	1	5	2	0	0	1	8
6:30 AM	3	12	1	0	0	16	0	2	2	1	0	5	1	7	5	0	0	13	
6:45 AM	3	8	0	0	1	11	0	1	1	0	4	2	1	15	1	0	2	17	
Hourly Total	9	40	2	0	1	51	0	5	5	1	10	11	5	31	11	0	4	47	
7:00 AM	8	18	2	0	0	28	1	3	1	0	7	5	4	15	1	0	5	20	
7:15 AM	3	21	1	0	6	25	2	3	5	0	5	10	3	17	1	0	1	21	
7:30 AM	2	35	2	0	2	39	3	4	4	0	5	11	3	13	3	0	3	19	
7:45 AM	3	40	2	0	1	45	1	0	3	0	10	4	4	32	9	0	6	45	
Hourly Total	16	114	7	0	9	137	7	10	13	0	27	30	14	77	14	0	15	105	
8:00 AM	9	63	3	0	2	75	0	5	3	0	11	8	3	32	8	0	6	43	
8:15 AM	2	37	4	0	1	43	0	2	3	0	10	5	8	28	5	0	5	41	
8:30 AM	7	46	4	0	1	57	0	3	3	0	9	6	2	20	3	0	5	25	
8:45 AM	5	44	3	0	5	52	2	2	2	0	9	6	4	26	3	0	9	33	
Hourly Total	23	190	14	0	9	227	2	12	11	0	39	25	17	106	19	0	25	142	
BREAK	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	3	49	1	0	1	53	1	13	4	0	18	18	5	25	10	0	3	40	
4:15 PM	9	51	0	0	4	60	1	6	4	0	13	11	0	19	0	0	11	19	
4:30 PM	5	51	1	0	5	57	1	9	7	0	18	17	5	21	6	0	4	32	
4:45 PM	7	62	1	0	6	70	2	5	9	0	16	16	1	42	7	0	9	50	
Hourly Total	24	213	3	0	16	240	5	33	24	0	65	62	11	107	23	0	27	141	
5:00 PM	7	56	1	0	9	64	3	9	16	0	6	28	2	56	9	0	6	67	
5:15 PM	9	72	4	0	6	85	1	9	6	0	15	16	3	28	6	0	7	37	
5:30 PM	4	55	0	0	2	59	0	7	7	0	9	14	9	29	2	0	2	40	
5:45 PM	6	54	1	0	2	61	1	8	7	0	11	16	2	26	3	0	3	31	
Hourly Total	26	237	6	0	19	269	5	33	36	0	41	74	16	139	20	0	18	175	
6:00 PM	6	52	1	0	8	59	0	3	8	0	21	11	4	21	1	0	7	26	
6:15 PM	4	35	0	0	3	39	1	2	3	0	16	6	4	16	6	0	6	26	
6:30 PM	1	49	0	0	6	50	1	5	3	0	12	9	2	22	3	0	4	27	
6:45 PM	4	41	2	0	12	47	0	5	4	0	25	9	2	28	2	0	14	32	
Hourly Total	15	177	3	0	29	195	2	15	18	0	74	35	12	87	12	0	31	111	
Grand Total	113	971	35	0	83	1119	21	108	107	1	256	237	75	547	99	0	120	721	
Approach %	10.1	86.8	3.1	0.0	-	-	8.9	45.6	45.1	0.4	-	-	10.4	75.9	13.7	0.0	-	-	
Total %	4.2	35.9	1.3	0.0	-	41.4	0.8	4.0	4.0	0.0	-	8.8	2.8	20.2	3.7	0.0	-	26.7	
Lights	102	964	32	0	-	1098	19	102	105	1	-	227	73	524	97	0	-	694	
% Lights	90.3	99.3	91.4	-	-	98.1	90.5	94.4	98.1	100.0	-	95.8	97.3	95.8	98.0	-	-	96.3	
Mediums	10	6	3	0	-	19	2	6	2	0	-	10	2	23	2	0	-	27	
% Mediums	8.8	0.6	8.6	-	-	1.7	9.5	5.6	1.9	0.0	-	4.2	2.7	4.2	2.0	-	-	3.7	
Articulated Trucks	1	1	0	0	-	2	0	0	0	0	-	0	0	0	0	0	0	0	0

Location Info		Count Data Info	
Location ID	016 4546_EB	Start Date	4/17/2018
Type	LINK	End Date	4/18/2018
Class	4	Start Time	1:00 PM
Located On	Emerson St	End Time	1:00 PM
Between	Green Bay Rd AND Sheridan Rd	Direction	EB
Direction	EB	Notes	
Community	EVANSTON	Source	EMERSON ST
MPO_ID		File Name	D1Submittal5a-016.mdb
HPMS ID		Weather	
Agency	Illinois DOT	Study	
		Owner	idotco
		QC Status	Accepted
Interval: 60 mins			
Time	Hourly Count		
00:00 - 01:00	17		
01:00 - 02:00	13		
02:00 - 03:00	9		
03:00 - 04:00	6		
04:00 - 05:00	8		
05:00 - 06:00	14		
06:00 - 07:00	30		
07:00 - 08:00	65		
08:00 - 09:00	126		
09:00 - 10:00	92		
10:00 - 11:00	63		
11:00 - 12:00	56		
12:00 - 13:00	59		
13:00 - 14:00	75		
14:00 - 15:00	64		
15:00 - 16:00	61		
16:00 - 17:00	76		
17:00 - 18:00	87		
18:00 - 19:00	99		
19:00 - 20:00	74		
20:00 - 21:00	53		
21:00 - 22:00	52		
22:00 - 23:00	31		
23:00 - 24:00	21		
TOTAL	1251		

Location Info		Count Data Info	
Location ID	016 4546_WB	Start Date	4/17/2018
Type	LINK	End Date	4/18/2018
Class	4	Start Time	1:00 PM
Located On	Emerson St	End Time	1:00 PM
Between	Green Bay Rd AND Sheridan Rd	Direction	WB
Direction	WB	Notes	
Community	EVANSTON	Source	EMERSON ST
MPO_ID		File Name	D1Submittal5a-016.mdb
HPMS ID		Weather	
Agency	Illinois DOT	Study	
		Owner	idotco
		QC Status	Accepted
Interval: 60 mins			
Time	Hourly Count		
00:00 - 01:00	7		
01:00 - 02:00	5		
02:00 - 03:00	8		
03:00 - 04:00	0		
04:00 - 05:00	7		
05:00 - 06:00	6		
06:00 - 07:00	8		
07:00 - 08:00	24		
08:00 - 09:00	59		
09:00 - 10:00	29		
10:00 - 11:00	29		
11:00 - 12:00	79		
12:00 - 13:00	88		
13:00 - 14:00	51		
14:00 - 15:00	33		
15:00 - 16:00	54		
16:00 - 17:00	56		
17:00 - 18:00	84		
18:00 - 19:00	57		
19:00 - 20:00	47		
20:00 - 21:00	30		
21:00 - 22:00	28		
22:00 - 23:00	13		
23:00 - 24:00	13		
TOTAL	815		

Location Info		Count Data Info	
Location ID	016 6119	Start Date	4/17/2018
Type	LINK	End Date	4/18/2018
Class	5	Start Time	12:00 PM
Located On	SHERMAN AVE	End Time	12:00 PM
Between	Emerson St AND Dempster St	Direction	2-WAY
Direction	2-WAY	Notes	
Community	EVANSTON	Source	SHERMAN AVE
MPO_ID		File Name	D1Submittal5a-016.mdb
HPMS ID		Weather	
Agency	Illinois DOT	Study	
		Owner	idotco
		QC Status	Accepted
Interval: 60 mins			
Time	Hourly Count		
00:00 - 01:00	67		
01:00 - 02:00	33		
02:00 - 03:00	1		
03:00 - 04:00	3		
04:00 - 05:00	16		
05:00 - 06:00	61		
06:00 - 07:00	145		
07:00 - 08:00	239		
08:00 - 09:00	339		
09:00 - 10:00	311		
10:00 - 11:00	226		
11:00 - 12:00	225		
12:00 - 13:00	274		
13:00 - 14:00	282		
14:00 - 15:00	252		
15:00 - 16:00	315		
16:00 - 17:00	377		
17:00 - 18:00	470		
18:00 - 19:00	410		
19:00 - 20:00	266		
20:00 - 21:00	214		
21:00 - 22:00	184		
22:00 - 23:00	149		
23:00 - 24:00	123		
TOTAL	4982		

APPENDIX C
ITE Trip Generation Pages

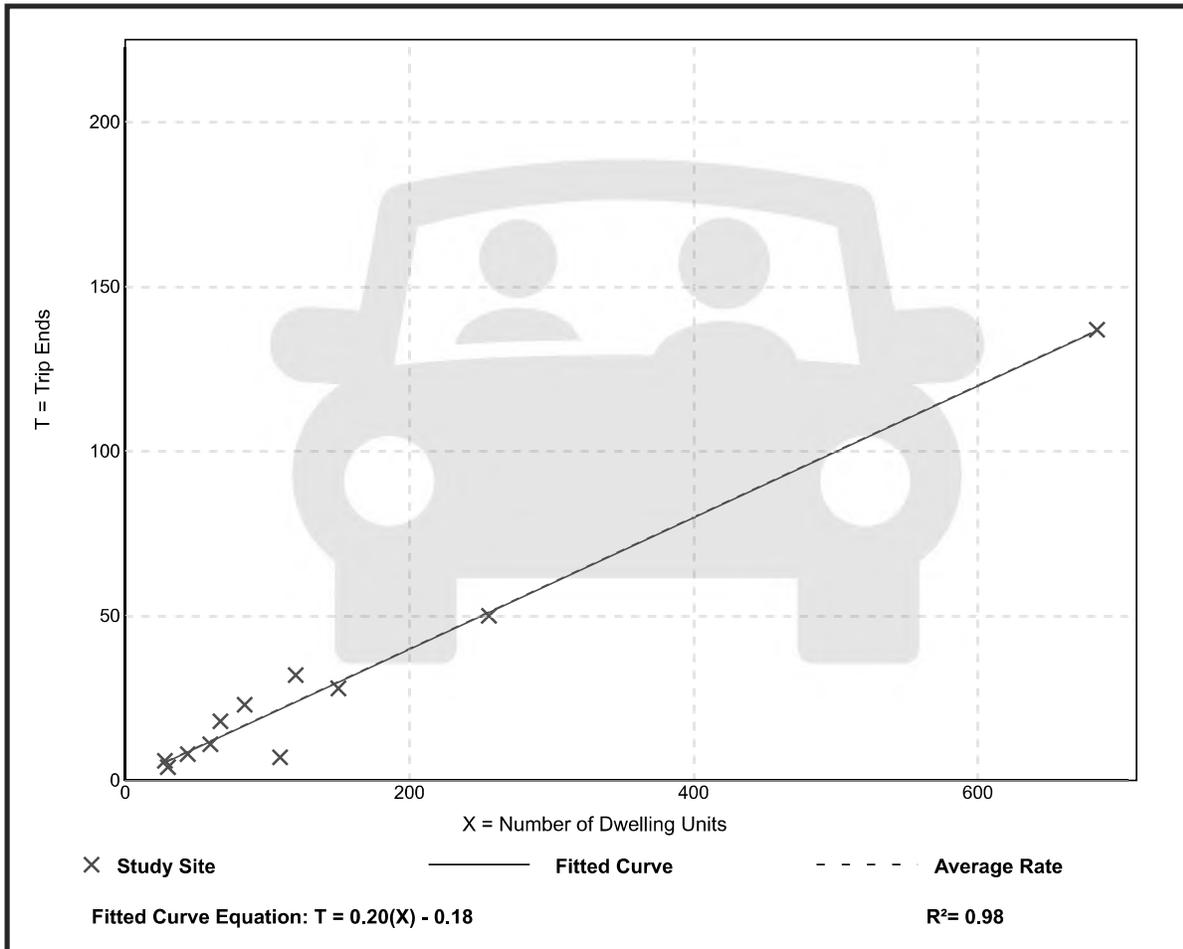
Senior Adult Housing - Attached (252)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 11
 Avg. Num. of Dwelling Units: 148
 Directional Distribution: 35% entering, 65% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.20	0.06 - 0.27	0.05

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

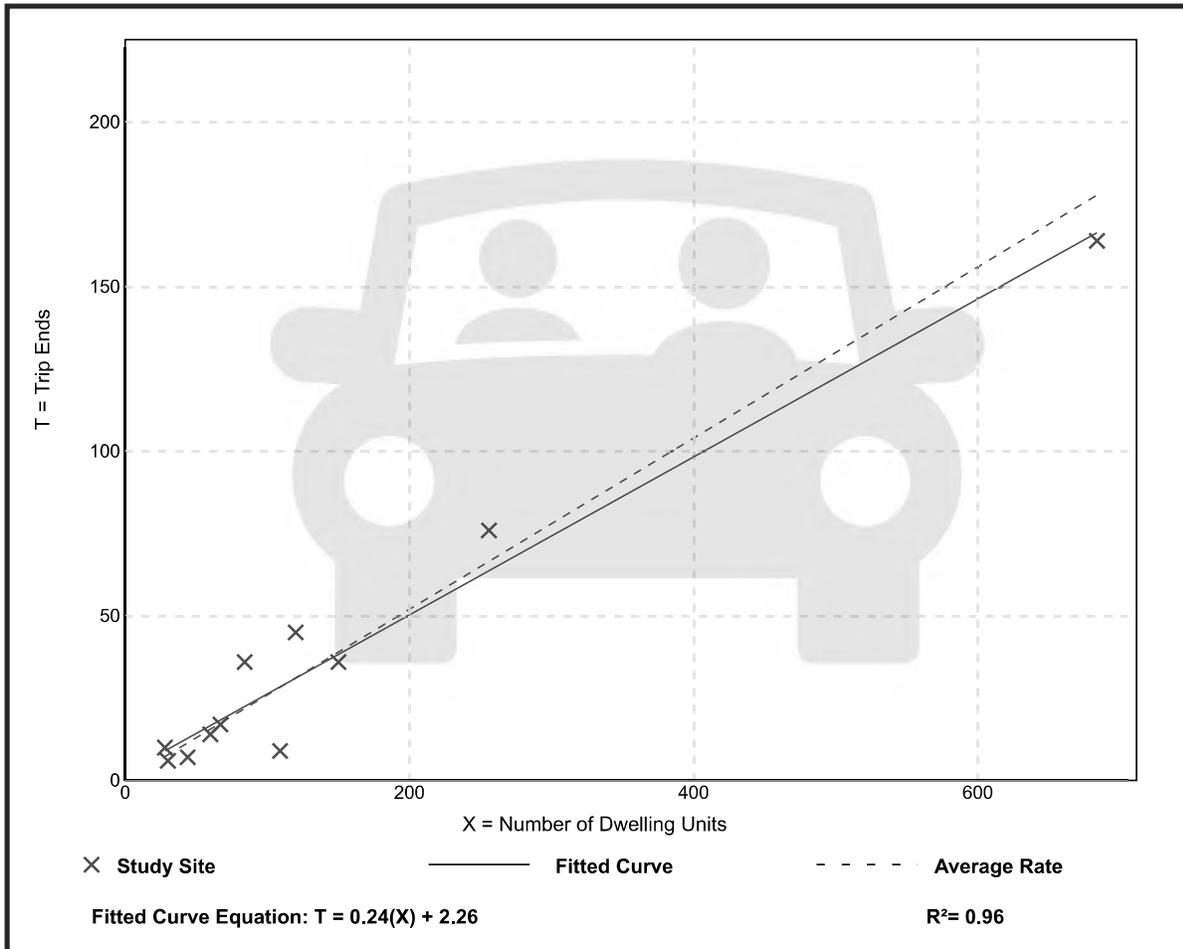
Senior Adult Housing - Attached (252)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 11
 Avg. Num. of Dwelling Units: 148
 Directional Distribution: 55% entering, 45% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.26	0.08 - 0.43	0.08

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Senior Adult Housing - Attached (252)

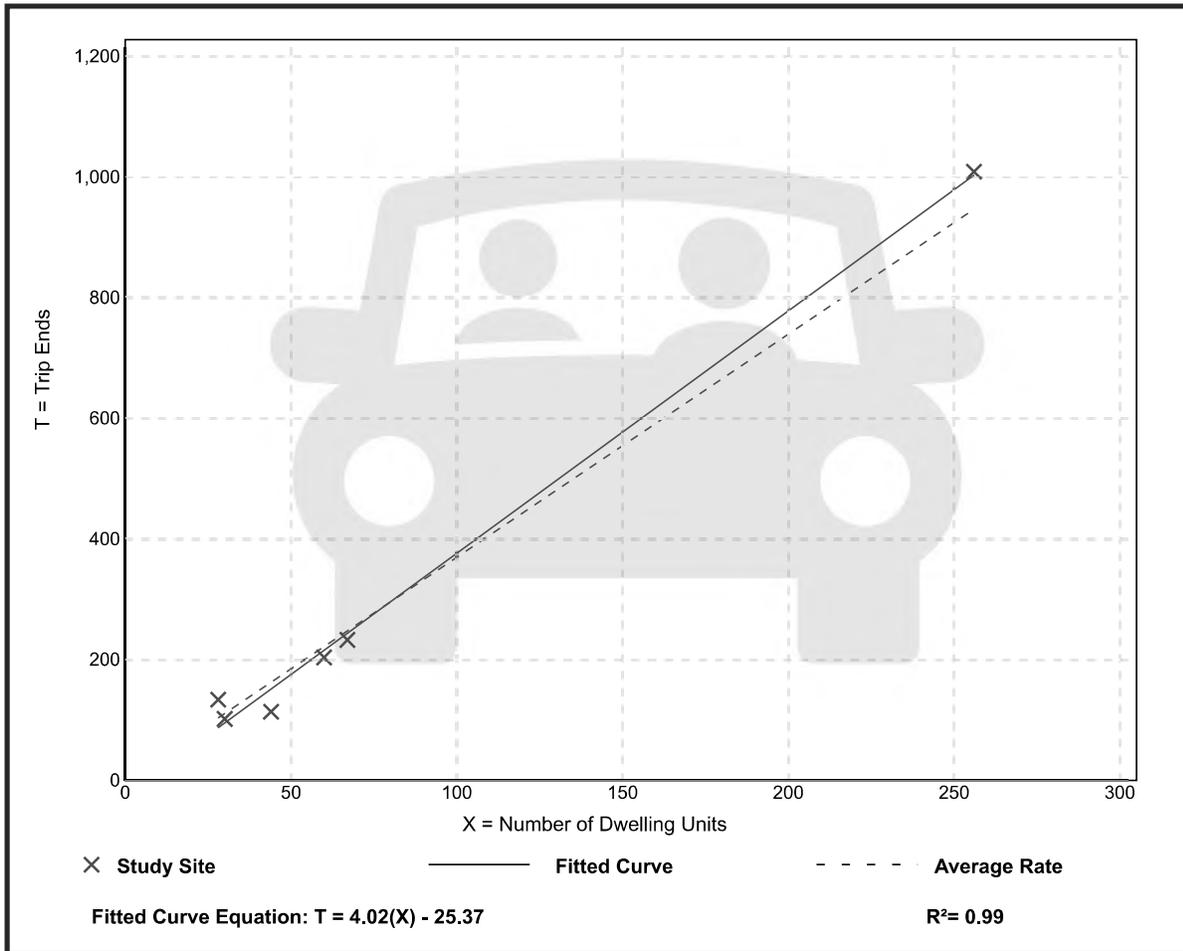
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 6
Avg. Num. of Dwelling Units: 81
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
3.70	2.59 - 4.79	0.53

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Land Use: 252

Senior Adult Housing—Attached

Description

Senior adult housing consists of attached independent living developments, including retirement communities, age-restricted housing, and active adult communities. These developments may include limited social or recreational services. However, they generally lack centralized dining and onsite medical facilities. Residents in these communities live independently, are typically active (requiring little to no medical supervision) and may or may not be retired. Senior adult housing—detached (Land Use 251), congregate care facility (Land Use 253), assisted living (Land Use 254), and continuing care retirement community (Land Use 255) are related uses.

Additional Data

Time-of-day distribution data for this land use are presented in Appendix A. For the one general urban/suburban site with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 11:45 a.m. and 12:45 p.m. and 12:00 and 1:00 p.m., respectively.

The sites were surveyed in the 1980s, the 1990s, and the 2000s in Alberta (CAN), California, Illinois, New Hampshire, New Jersey, New York, and Pennsylvania.

Source Numbers

272, 501, 576, 602, 703, 734, 741, 902, 970

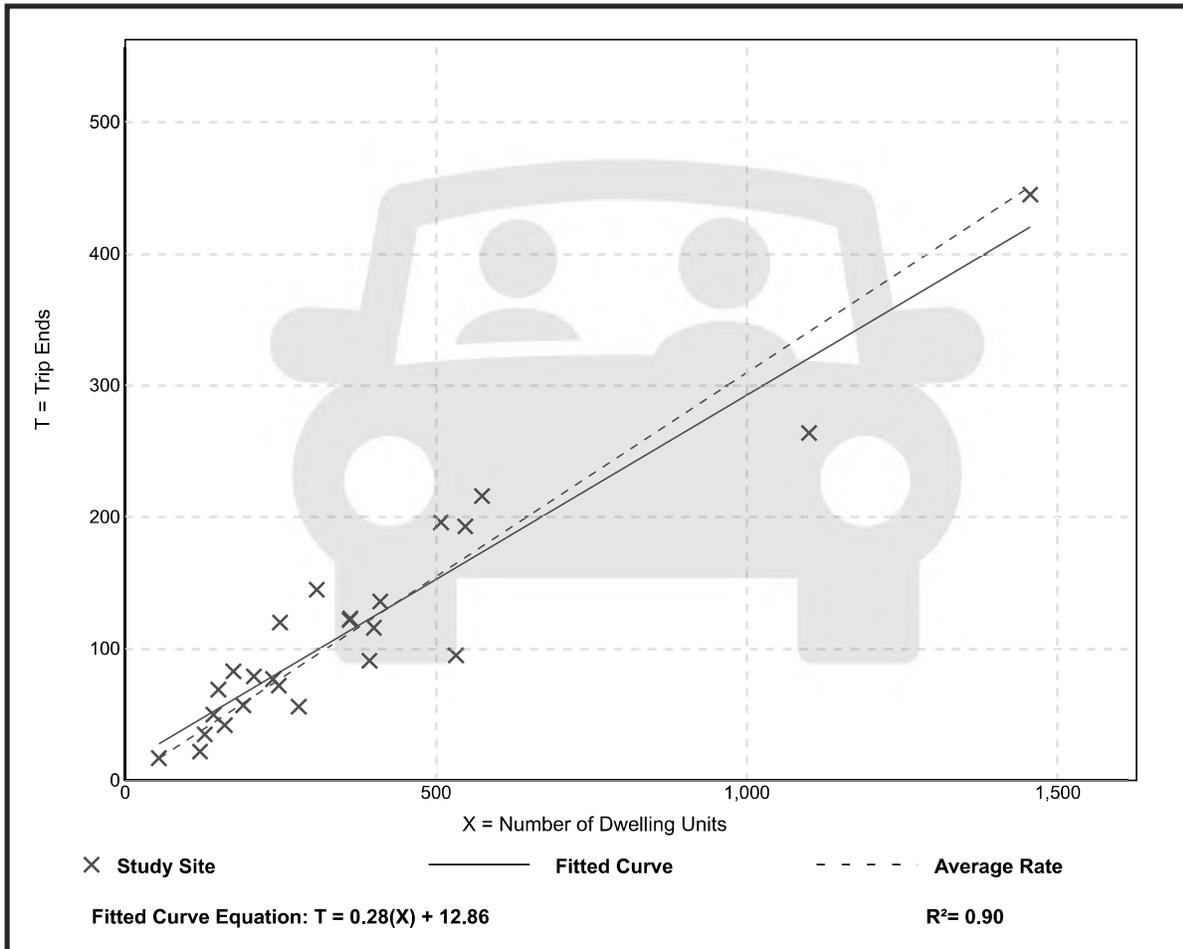
Multifamily Housing (High-Rise) (222)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 25
 Avg. Num. of Dwelling Units: 372
 Directional Distribution: 24% entering, 76% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.31	0.18 - 0.48	0.08

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Multifamily Housing (High-Rise) (222)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

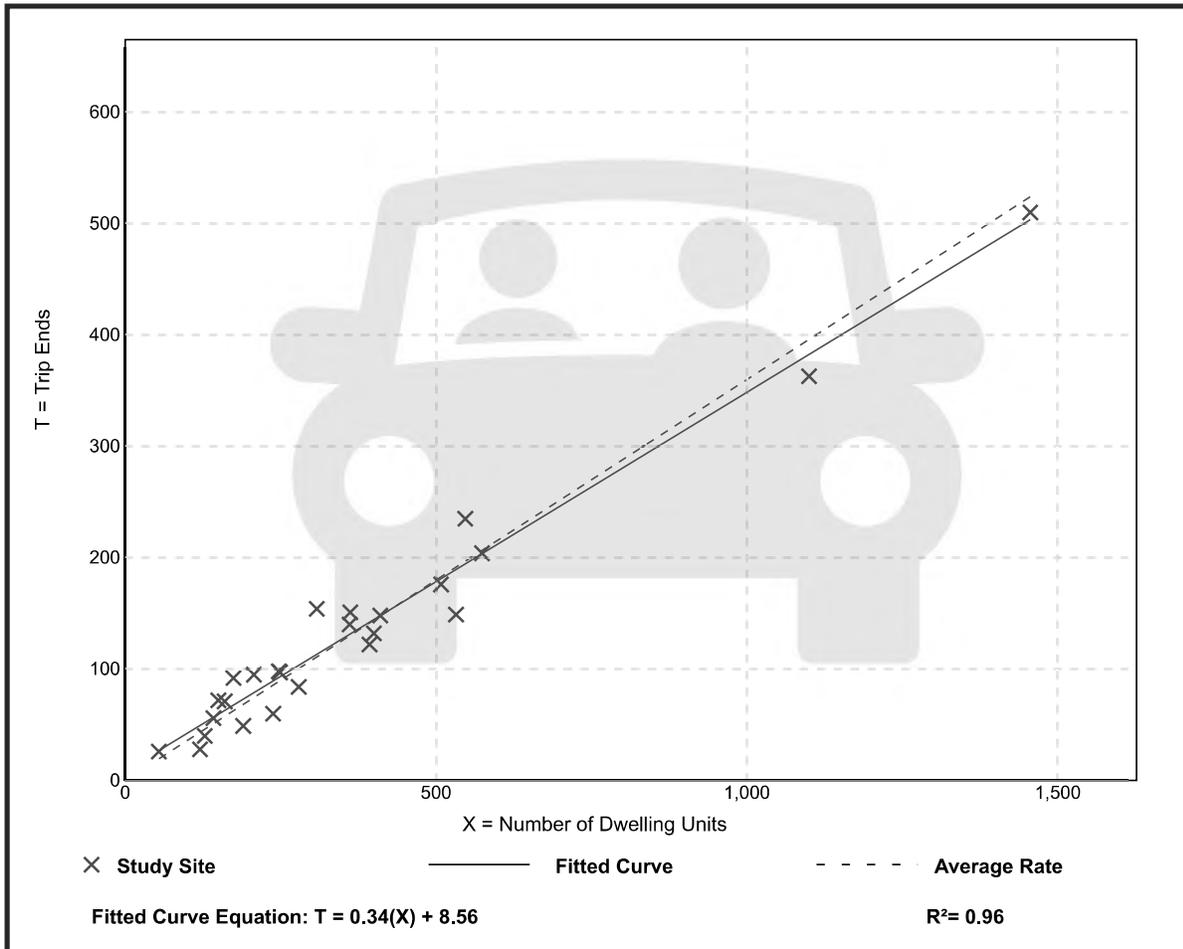
Setting/Location: General Urban/Suburban

Number of Studies: 25
 Avg. Num. of Dwelling Units: 372
 Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.23 - 0.53	0.06

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Multifamily Housing (High-Rise) (222)

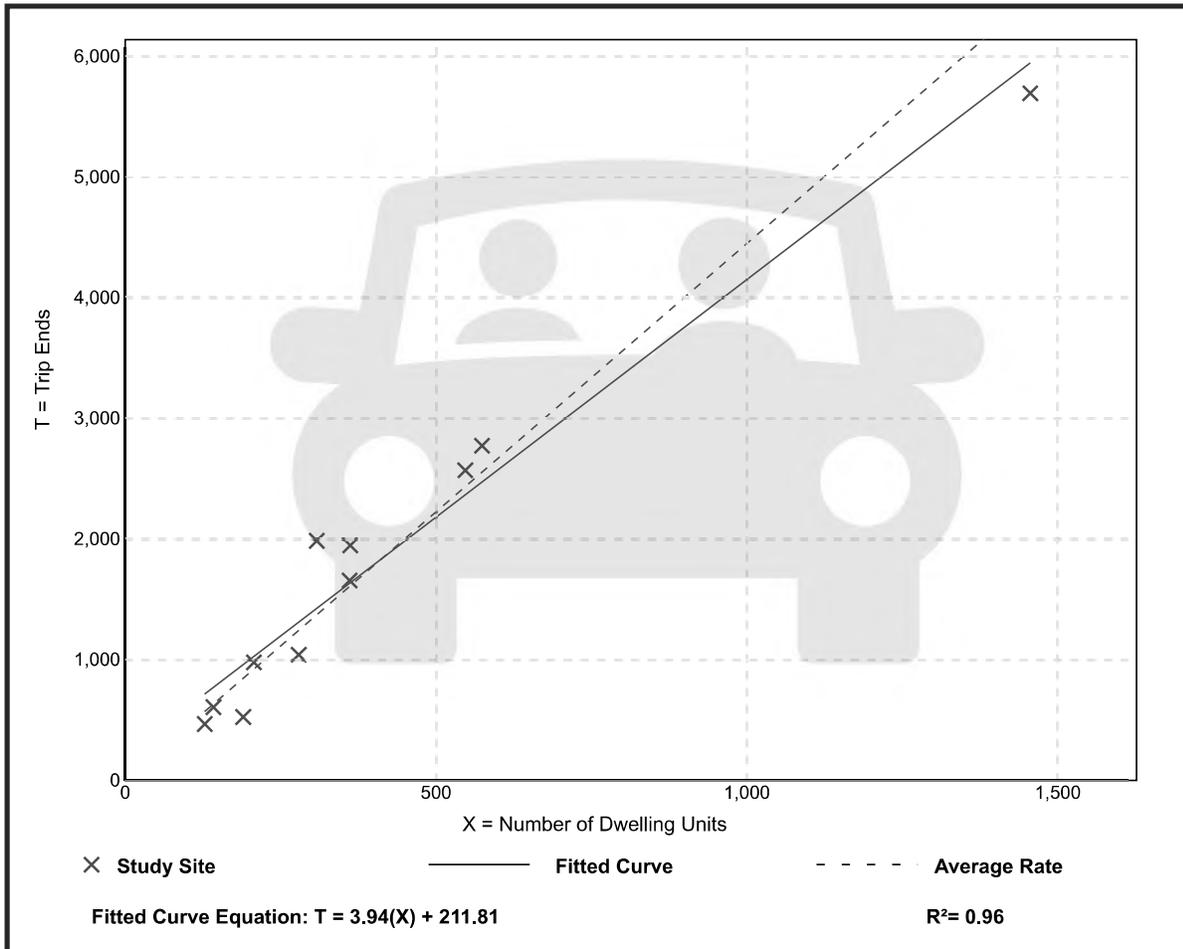
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 11
Avg. Num. of Dwelling Units: 414
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.45	2.77 - 6.45	0.83

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Land Use: 222

Multifamily Housing (High-Rise)

Description

High-rise multifamily housing includes apartments, townhouses, and condominiums that have more than 10 levels (floors). They are likely to have one or more elevators. Multifamily housing (low-rise) (Land Use 220), multifamily housing (mid-rise) (Land Use 221), off-campus student apartment (Land Use 225), and high-rise residential with 1st-floor commercial (Land Use 232) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the high-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the 12 sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 1.57 residents per occupied dwelling unit.

For the 26 sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 98.4 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight dense multi-use sites for which 24-hour time-of-day person trip data were collected, the overall highest vehicle volumes during the AM and PM on a weekday were between 7:30 and 8:30 a.m. and 5:30 and 6:30 p.m., respectively. The Saturday and Sunday peak hours for person trips were between 5:00 and 6:00 p.m. and 4:45 and 5:45 p.m., respectively.

For the six center city core sites for which 24-hour time-of-day person trip data were collected, the overall highest vehicle volumes during the AM and PM on a weekday were between 8:00 and 9:00 a.m. and 6:00 and 7:00 p.m., respectively. The Saturday and Sunday peak hours for person trips were between 11:30 a.m. and 12:30 p.m. and 11:00 a.m. and 12:00 p.m., respectively.

For the 12 sites for which data were provided for both occupied dwelling units and residents, there was an average of 1.57 residents per occupied dwelling unit.

For the 26 sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 98.4 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the three center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 2.52 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 2.70 during Weekday, AM Peak Hour of Generator
- 1.88 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.22 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the six dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 2.81 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 2.49 during Weekday, AM Peak Hour of Generator
- 2.17 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.85 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 2000s, and the 2010s in California, District of Columbia, Maryland, New Jersey, New York, Ontario (CAN), Oregon, Pennsylvania, Virginia, and Washington.

Source Numbers

105, 168, 169, 187, 305, 321, 356, 818, 862, 901, 910, 949, 963, 964, 966, 967

APPENDIX D
CMAP Correspondence



Chicago Metropolitan Agency for Planning

233 South Wacker Drive
Suite 800
Chicago, Illinois 60606

312 454 0400
www.cmap.illinois.gov

July 10, 2019

William C. Grieve, P.E., PTOE
Senior Transportation Engineer
Gewalt Hamilton Associates
625 Forest Edge Drive
Vernon Hills, IL 60061

Subject: Emerson Street @ Sherman Avenue
IDOT

Dear Mr. Grieve:

In response to a request made on your behalf and dated July 9, 2019, we have developed year 2050 average daily traffic (ADT) projections for the subject location.

ROAD SEGMENT	Current Volumes	Year 2050 ADT
Emerson St east of Sherman Ave	1,650	2,200
Sherman Ave south of Emerson St	3,300	4,400

Traffic projections are developed using existing ADT data provided in the request letter and the results from the March 2019 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2050 socioeconomic projections and assumes the implementation of the ON TO 2050 Comprehensive Regional Plan for the Northeastern Illinois area. The provision of this data in support of your request does not constitute a CMAP endorsement of the proposed development or any subsequent developments.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

Jose Rodriguez, PTP, AICP
Senior Planner, Research & Analysis

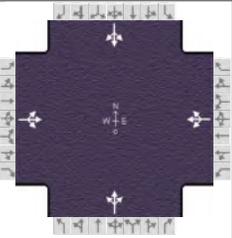
cc: Quigley (IDOT)
S:\AdminGroups\ResearchAnalysis\2019_ForecastsTraffic\Evanston\ck-93-19\ck-93-19.docx

APPENDIX E
Capacity Analysis Worksheets

HCS7 Signalized Intersection Input Data

Appendix E

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	Existing AM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	36	55	42	12	10	1	25	112	17	13	186	21

Signal Information																		
Cycle, s	50.0	Reference Phase	2															
Offset, s	0	Reference Point	End															
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0	1		2		3		4	
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	5		6		7		8	
				Red	1.0	1.0	0.0	0.0	0.0	0.0								

Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	36	55	42	12	10	1	25	112	17	13	186	21
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		R	4		None			R	2		None	
Heavy Vehicles (P _{HV}), %		2			2			2			2	
Ped / Bike / RTOR, /h	20	7	0	40	3	0	22	5	0	5	12	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	2	0	0	2
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft		12.0			12.0			12.0			12.0	
Turn Bay Length, ft		0			0			0			0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	25	25	25	25	25	25	25	25	25	25	25	25

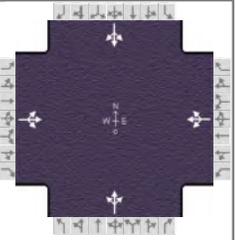
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s		23.0		23.0		27.0		27.0
Yellow Change Interval (Y), s		4.0		4.0		4.0		4.0
Red Clearance Interval (R _c), s		1.0		1.0		1.0		1.0
Minimum Green (G _{min}), s		18		18		22		22
Start-Up Lost Time (lt), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s		2.0		2.0		2.0		2.0
Recall Mode		Max		Max		Max		Max
Dual Entry		Yes		Yes		Yes		Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25									
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No									
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50										

HCS7 Signalized Intersection Results Summary

Appendix E

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	Existing AM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	36	55	42	12	10	1	25	112	17	13	186	21

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

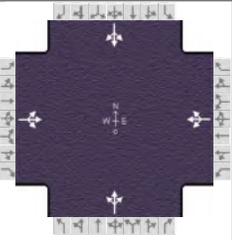
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g s), s						5.2		6.8
Green Extension Time (g e), s		0.0		0.0		0.9		0.9
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	162			28			188			268		
Adjusted Saturation Flow Rate (s), veh/h/ln	1618			1507			1718			1810		
Queue Service Time (g s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g c), s	3.3			0.5			3.2			4.8		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	674			652			840			873		
Volume-to-Capacity Ratio (X)	0.241			0.043			0.224			0.307		
Back of Queue (Q), ft/ln (95 th percentile)	58.1			9.1			55.5			83.4		
Back of Queue (Q), veh/ln (95 th percentile)	2.3			0.4			2.2			3.3		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d 1), s/veh	11.3			10.4			8.7			9.2		
Incremental Delay (d 2), s/veh	0.8			0.1			0.6			0.9		
Initial Queue Delay (d 3), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.1			10.5			9.4			10.1		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.1	B		10.5	B		9.4	A		10.1	B	
Intersection Delay, s/veh / LOS	10.4						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.76	A	0.53	A	0.80	A	0.93	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	Existing AM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	36	55	42	12	10	1	25	112	17	13	186	21

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0		
				Red	1.0	1.0	0.0	0.0	0.0	0.0		

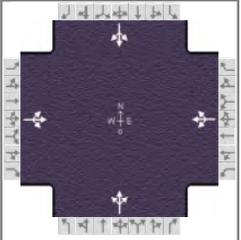
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.992	1.000	1.000	0.992
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.928	0.865		0.813	0.806		0.942	0.919		0.988	0.968	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.865		0.000	0.806		0.000	0.919		0.000	0.968
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.946			0.979			0.996			0.981		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.946			0.923			0.951			0.964
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	438	669	511	786	655	66	279	1250	190	107	1530	173
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1346		1266		1141		1225
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1651		1463		1687		1808
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		17.5		14.7		17.2		18.8
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		5.1		1.8		8.8		13.8
Queue Service Time Before Blockage (g_{ts}), s		1.8		0.5		2.7		4.1
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v	0.972	0.000		0.972	0.000		0.972	0.000		0.972	0.000	
Pedestrian F_s / F_{delay}	0.000	0.093		0.000	0.093		0.000	0.083		0.000	0.083	
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	720.00	10.28		720.00	10.26		880.00	7.86		880.00	7.89	
Bicycle F_w / F_v	-3.64	0.27		-3.64	0.05		-3.64	0.31		-3.64	0.44	

HCS7 Signalized Intersection Results Graphical Summary

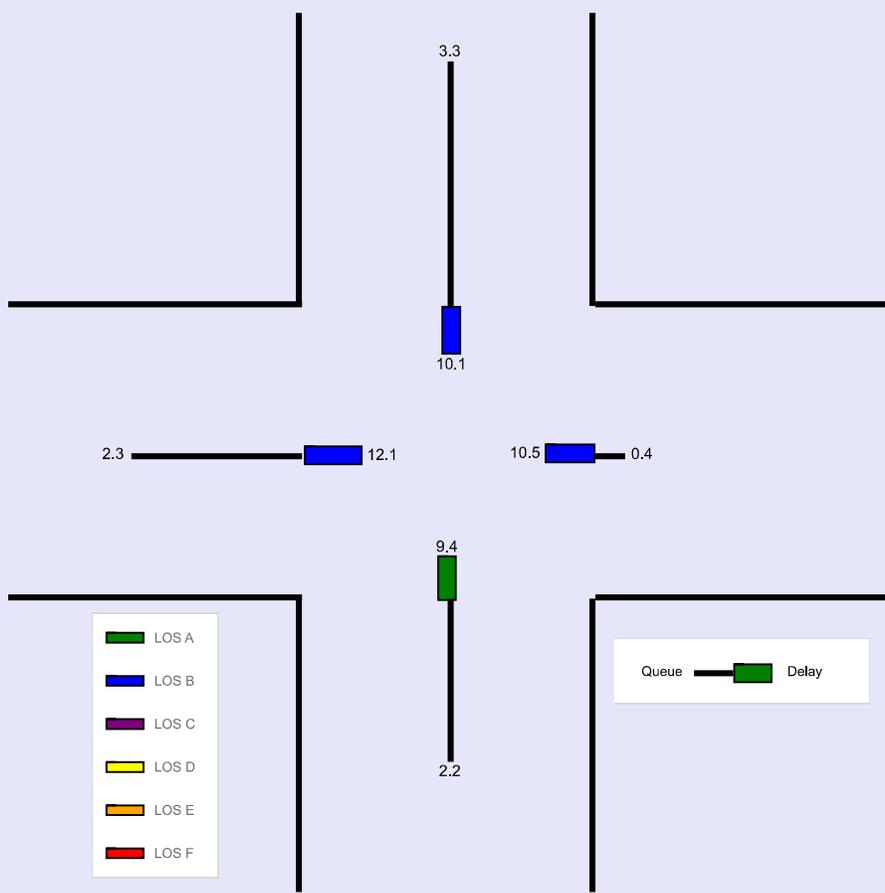
General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	Existing AM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	36	55	42	12	10	1	25	112	17	13	186	21

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0		
				Red	1.0	1.0	0.0	0.0	0.0	0.0		

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/l (95 th percentile)		58.1			9.1			55.5			83.4	
Back of Queue (Q), veh/l (95 th percentile)		2.3			0.4			2.2			3.3	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.1			10.5			9.4			10.1	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.1		B	10.5		B	9.4		A	10.1		B
Intersection Delay, s/veh / LOS	10.4						B					



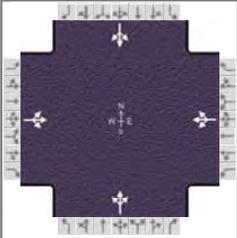
--- **Messages** ---

No errors or warnings exist.

--- **Comments** ---

HCS7 Signalized Intersection Input Data

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 7:45
Intersection	Emerson Street	File Name	2027 NB AM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	59	45	13	11	1	27	120	18	14	200	23

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0		
				Red	1.0	1.0	0.0	0.0	0.0	0.0		

Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	59	45	13	11	1	27	120	18	14	200	23
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		R	4		None			R	2		None	
Heavy Vehicles (P _{HV}), %		2			2			2			2	
Ped / Bike / RTOR, /h	28	7	0	40	3	0	22	5	0	7	14	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	2	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft		12.0			12.0			12.0			12.0	
Turn Bay Length, ft		0			0			0			0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	25	25	25	25	25	25	25	25	25	25	25	25

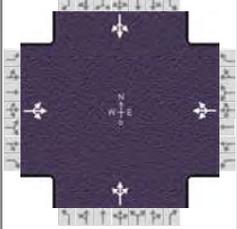
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s		23.0		23.0		27.0		27.0
Yellow Change Interval (Y), s		4.0		4.0		4.0		4.0
Red Clearance Interval (R _c), s		1.0		1.0		1.0		1.0
Minimum Green (G _{min}), s		18		18		22		22
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s		2.0		2.0		2.0		2.0
Recall Mode		Max		Max		Max		Max
Dual Entry		Yes		Yes		Yes		Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25									
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No									
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50										

HCS7 Signalized Intersection Results Summary

Appendix E

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	2027 NB AM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	59	45	13	11	1	27	120	18	14	200	23

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

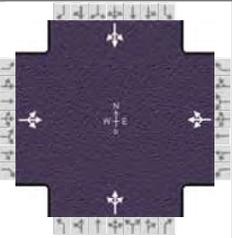
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						5.5		7.3
Green Extension Time (g _e), s		0.0		0.0		1.0		0.9
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	174			30			201			289		
Adjusted Saturation Flow Rate (s), veh/h/ln	1611			1496			1713			1808		
Queue Service Time (g _s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g _c), s	3.6			0.5			3.5			5.3		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	671			648			838			872		
Volume-to-Capacity Ratio (X)	0.260			0.047			0.240			0.332		
Back of Queue (Q), ft/ln (95 th percentile)	63.1			9.9			59.9			91.3		
Back of Queue (Q), veh/ln (95 th percentile)	2.5			0.4			2.4			3.6		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d ₁), s/veh	11.4			10.4			8.8			9.3		
Incremental Delay (d ₂), s/veh	0.9			0.1			0.7			1.0		
Initial Queue Delay (d ₃), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.3			10.5			9.5			10.3		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.3	B		10.5	B		9.5	A		10.3	B	
Intersection Delay, s/veh / LOS	10.6						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.78	A	0.54	A	0.82	A	0.96	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 7:45
Intersection	Emerson Street	File Name	2027 NB AM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	59	45	13	11	1	27	120	18	14	200	23

Signal Information														
Cycle, s	50.0	Reference Phase	2	Green	18.0	22.0	0.0	0.0	0.0	0.0				
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On											

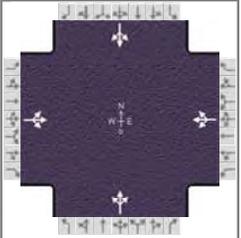
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.992	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.927	0.861		0.806	0.800		0.939	0.916		0.988	0.966	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.861		0.000	0.800		0.000	0.916		0.000	0.966
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.946			0.971			0.995			0.982		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.935			0.923			0.951			0.961
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	439	665	507	778	658	60	280	1246	187	107	1525	175
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1345		1246		1119		1214
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1649		1442		1677		1807
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		17.5		14.4		16.7		18.5
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		5.0		1.8		8.8		13.5
Queue Service Time Before Blockage (g_{ts}), s		2.0		0.5		2.9		4.4
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000
Pedestrian F_s / F_{delay}	0.000	0.093	0.000	0.093	0.000	0.083	0.000	0.083
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	720.00	10.28	720.00	10.26	880.00	7.86	880.00	7.90
Bicycle F_w / F_v	-3.64	0.29	-3.64	0.05	-3.64	0.33	-3.64	0.48

HCS7 Signalized Intersection Results Graphical Summary

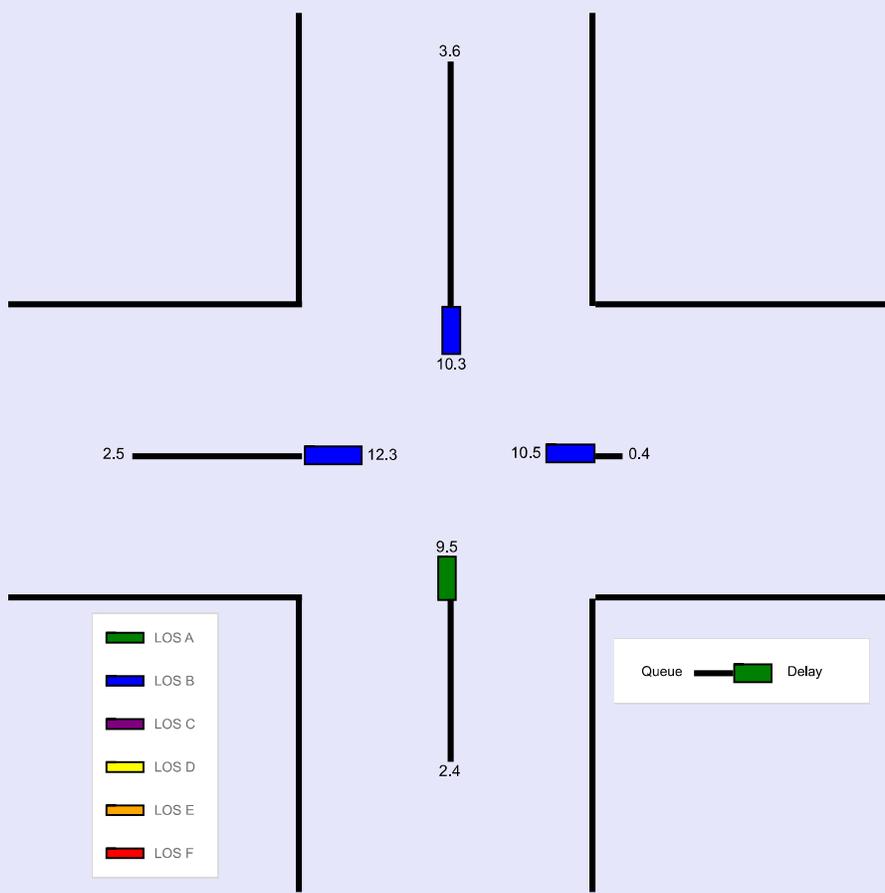
General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	2027 NB AM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	39	59	45	13	11	1	27	120	18	14	200	23

Signal Information				Timing Diagram									
Cycle, s	50.0	Reference Phase	2	[Timing Diagram: Shows signal phases for EB, WB, NB, SB with Green, Yellow, and Red durations. Includes diagrams for each approach movement.]									
Offset, s	0	Reference Point	End	Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)		63.1			9.9			59.9			91.3	
Back of Queue (Q), veh/ln (95 th percentile)		2.5			0.4			2.4			3.6	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.3			10.5			9.5			10.3	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.3		B	10.5		B	9.5		A	10.3		B
Intersection Delay, s/veh / LOS	10.6						B					



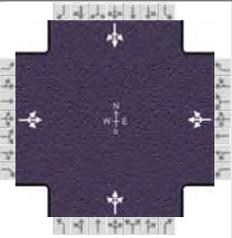
--- Messages ---

No errors or warnings exist.

--- Comments ---

HCS7 Signalized Intersection Input Data

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1 > 7:45
Intersection	Emerson Street	File Name	2027 TOTAL AM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	64	59	13	12	1	32	120	18	14	200	24

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0		
				Red	1.0	1.0	0.0	0.0	0.0	0.0		

Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	64	59	13	12	1	32	120	18	14	200	24
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		R	4		None			R	2		None	
Heavy Vehicles (P _{HV}), %		2			2			2			2	
Ped / Bike / RTOR, /h	28	7	0	40	3	0	22	5	0	7	14	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	2	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft		12.0			12.0			12.0			12.0	
Turn Bay Length, ft		0			0			0			0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	25	25	25	25	25	25	25	25	25	25	25	25

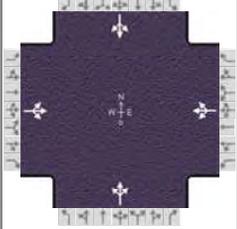
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s		23.0		23.0		27.0		27.0
Yellow Change Interval (Y), s		4.0		4.0		4.0		4.0
Red Clearance Interval (R _c), s		1.0		1.0		1.0		1.0
Minimum Green (G _{min}), s		18		18		22		22
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s		2.0		2.0		2.0		2.0
Recall Mode		Max		Max		Max		Max
Dual Entry		Yes		Yes		Yes		Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25									
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No									
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50										

HCS7 Signalized Intersection Results Summary

Appendix E

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1> 7:45		
Intersection	Emerson Street	File Name	2027 TOTAL AM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	39	64	59	13	12	1	32	120	18	14	200	24

Signal Information														
Cycle, s	50.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
				Red	1.0	1.0	0.0	0.0	0.0	0.0				

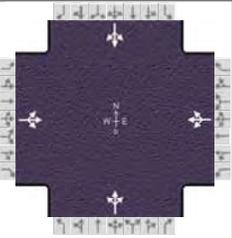
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						5.6		7.3
Green Extension Time (g _e), s		0.0		0.0		1.0		1.0
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	198			32			207			290		
Adjusted Saturation Flow Rate (s), veh/h/ln	1611			1495			1690			1806		
Queue Service Time (g _s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g _c), s	4.2			0.6			3.6			5.3		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	669			646			829			871		
Volume-to-Capacity Ratio (X)	0.295			0.049			0.250			0.333		
Back of Queue (Q), ft/ln (95 th percentile)	73			10.3			62.1			91.7		
Back of Queue (Q), veh/ln (95 th percentile)	2.9			0.4			2.4			3.6		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d ₁), s/veh	11.6			10.4			8.8			9.3		
Incremental Delay (d ₂), s/veh	1.1			0.1			0.7			1.0		
Initial Queue Delay (d ₃), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.7			10.6			9.6			10.3		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.7		B	10.6		B	9.6		A	10.3		B
Intersection Delay, s/veh / LOS	10.8						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.81	A	0.54	A	0.83	A	0.97	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1 > 7:45
Intersection	Emerson Street	File Name	2027 TOTAL AM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	39	64	59	13	12	1	32	120	18	14	200	24

Signal Information															
Cycle, s	50.0	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0					
				Red	1.0	1.0	0.0	0.0	0.0	0.0					

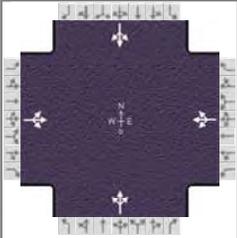
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.992	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.938	0.861		0.805	0.799		0.925	0.903		0.988	0.966	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.861		0.000	0.799		0.000	0.903		0.000	0.966
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.946			0.972			0.995			0.982		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.935			0.923			0.951			0.961
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	388	636	587	747	690	57	318	1193	179	106	1518	182
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1344		1222		1118		1214
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1672		1439		1651		1807
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		17.4		13.8		16.7		18.4
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		5.8		2.0		7.8		13.5
Queue Service Time Before Blockage (g_{ts}), s		2.1		0.5		3.0		4.4
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000	0.000	
Pedestrian F_s / F_{delay}	0.000	0.093	0.000	0.093	0.000	0.083	0.000	0.083	0.000	0.083	0.083	
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	720.00	10.28	720.00	10.26	880.00	7.86	880.00	7.90	880.00	7.90	7.90	
Bicycle F_w / F_v	-3.64	0.33	-3.64	0.05	-3.64	0.34	-3.64	0.48	-3.64	0.48	0.48	

HCS7 Signalized Intersection Results Graphical Summary

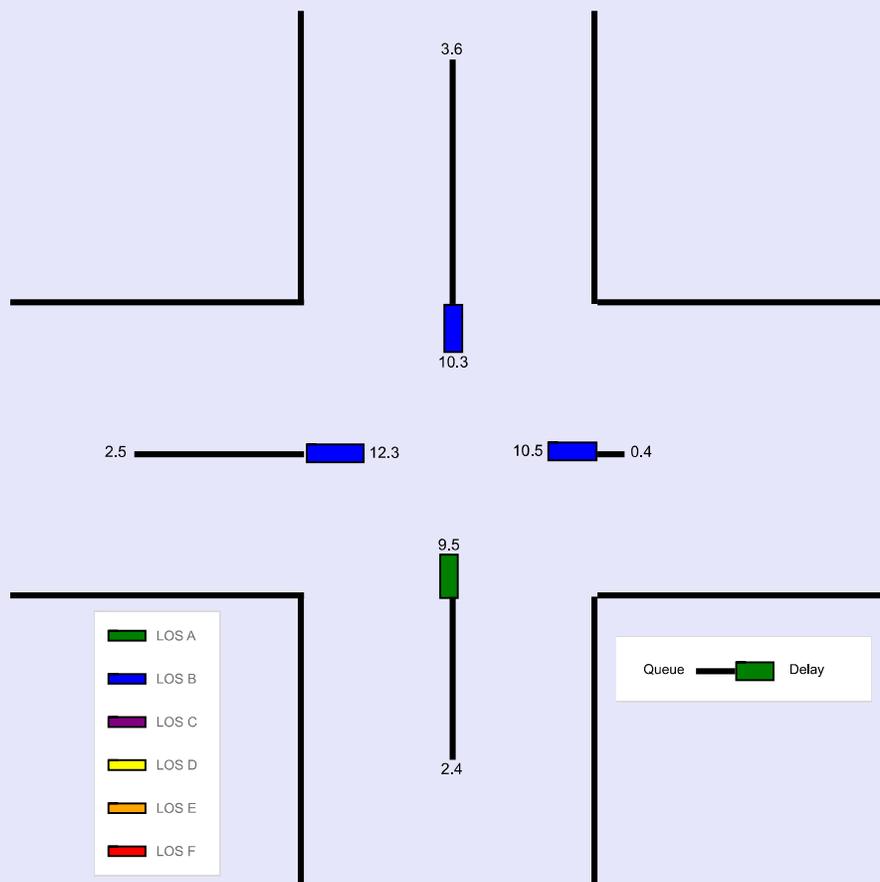
General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	AM Peak	PHF	0.82		
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1 > 7:45		
Intersection	Emerson Street	File Name	2027 TOTAL AM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	39	64	59	13	12	1	32	120	18	14	200	24

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0		
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0		
				Red	1.0	1.0	0.0	0.0	0.0	0.0		

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)		73			10.3			62.1			91.7	
Back of Queue (Q), veh/ln (95 th percentile)		2.9			0.4			2.4			3.6	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.7			10.6			9.6			10.3	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.7		B	10.6		B	9.6		A	10.3		B
Intersection Delay, s/veh / LOS	10.8						B					

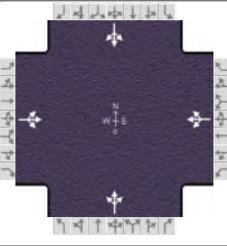
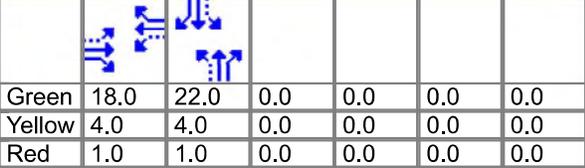


--- **Messages** ---

No errors or warnings exist.

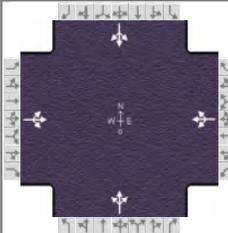
--- **Comments** ---

HCS7 Signalized Intersection Input Data

General Information						Intersection Information																			
Agency	GHA					Duration, h	0.250																		
Analyst	AM		Analysis Date	7/16/2019		Area Type	Other																		
Jurisdiction	IDOT		Time Period	PM Peak		PHF	0.91																		
Urban Street	Sherman Avenue		Analysis Year	2019		Analysis Period	1 > 4:30																		
Intersection	Emerson Street		File Name	Existing PM Peak.xus																					
Project Description	5582.900																								
Demand Information						EB			WB			NB			SB										
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R								
Demand (v), veh/h						43	46	40	38	32	7	28	147	11	7	241	28								
Signal Information												1		2		3		4							
Cycle, s	50.0	Reference Phase	2		Green							18.0	22.0	0.0	0.0	0.0	0.0	5		6		7		8	
Offset, s	0	Reference Point	End		Yellow							4.0	4.0	0.0	0.0	0.0	0.0	5		6		7		8	
Uncoordinated	No	Simult. Gap E/W	On		Red							1.0	1.0	0.0	0.0	0.0	0.0	5		6		7		8	
Force Mode	Fixed	Simult. Gap N/S	On															5		6		7		8	
Traffic Information						EB			WB			NB			SB										
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R								
Demand (v), veh/h						43	46	40	38	32	7	28	147	11	7	241	28								
Initial Queue (Q _b), veh/h						0	0	0	0	0	0	0	0	0	0	0	0								
Base Saturation Flow Rate (s ₀), veh/h						1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900								
Parking (N _m), man/h							R	4		None			R	2		None									
Heavy Vehicles (P _{HV}), %							2			2			2			2									
Ped / Bike / RTOR, /h						78	2	0	55	9	0	26	14	0	26	13	0								
Buses (N _b), buses/h						0	0	0	0	0	0	0	0	2	0	0	0								
Arrival Type (AT)						3	3	3	3	3	3	3	3	3	3	3	3								
Upstream Filtering (I)						1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00								
Lane Width (W), ft							12.0			12.0			12.0			12.0									
Turn Bay Length, ft							0			0			0			0									
Grade (Pg), %							0			0			0			0									
Speed Limit, mi/h						25	25	25	25	25	25	25	25	25	25	25	25								
Phase Information						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT												
Maximum Green (G _{max}) or Phase Split, s							23.0		23.0		27.0		27.0												
Yellow Change Interval (Y), s							4.0		4.0		4.0		4.0												
Red Clearance Interval (R _c), s							1.0		1.0		1.0		1.0												
Minimum Green (G _{min}), s							18		18		22		22												
Start-Up Lost Time (lt), s						2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0												
Extension of Effective Green (e), s						2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0												
Passage (PT), s							2.0		2.0		2.0		2.0												
Recall Mode							Max		Max		Max		Max												
Dual Entry							Yes		Yes		Yes		Yes												
Walk (Walk), s							0.0		0.0		0.0		0.0												
Pedestrian Clearance Time (PC), s							0.0		0.0		0.0		0.0												
Multimodal Information						EB			WB			NB			SB										
85th % Speed / Rest in Walk / Corner Radius						0	No	25	0	No	25	0	No	25	0	No	25								
Walkway / Crosswalk Width / Length, ft						9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0								
Street Width / Island / Curb						0	0	No	0	0	No	0	0	No	0	0	No								
Width Outside / Bike Lane / Shoulder, ft						12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0								
Pedestrian Signal / Occupied Parking						No	0.50	No	No	0.50	No	No	0.50	No	No	0.50	No								

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 4:30		
Intersection	Emerson Street	File Name	Existing PM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	43	46	40	38	32	7	28	147	11	7	241	28

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
		Green	18.0	22.0	0.0	0.0	0.0	0.0				
		Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
		Red	1.0	1.0	0.0	0.0	0.0	0.0				

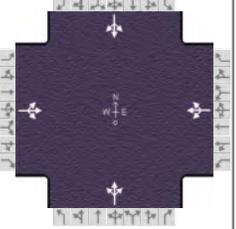
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						5.5		7.6
Green Extension Time (g _e), s		0.0		0.0		1.0		1.0
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	142			85			204			303		
Adjusted Saturation Flow Rate (s), veh/h/ln	1521			1468			1736			1816		
Queue Service Time (g _s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g _c), s	2.9			1.6			3.5			5.6		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	644			636			847			873		
Volume-to-Capacity Ratio (X)	0.220			0.133			0.241			0.347		
Back of Queue (Q), ft/ln (95 th percentile)	50.4			28.7			60.7			96.9		
Back of Queue (Q), veh/ln (95 th percentile)	2.0			1.1			2.4			3.8		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d ₁), s/veh	11.2			10.7			8.8			9.4		
Incremental Delay (d ₂), s/veh	0.8			0.4			0.7			1.1		
Initial Queue Delay (d ₃), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.0			11.2			9.5			10.5		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.0	B		11.2	B		9.5	A		10.5	B	
Intersection Delay, s/veh / LOS	10.6						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.72	A	0.63	A	0.82	A	0.99	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 4:30		
Intersection	Emerson Street	File Name	Existing PM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	43	46	40	38	32	7	28	147	11	7	241	28

Signal Information												Diagram			
Cycle, s	50.0	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0					
				Red	1.0	1.0	0.0	0.0	0.0	0.0					

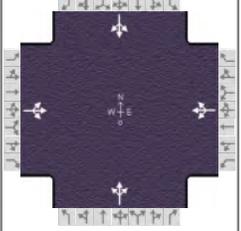
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.992	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.893	0.813		0.802	0.785		0.942	0.928		0.996	0.971	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.813		0.000	0.785		0.000	0.928		0.000	0.971
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.931			0.913			0.983			0.979		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.872			0.897			0.940			0.940
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	507	543	472	724	610	133	261	1372	103	46	1586	184
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1290		1207		1082		1204
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1567		1377		1672		1842
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		16.4		15.1		16.4		18.5
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		3.9		2.0		9.4		15.8
Queue Service Time Before Blockage (g_{ts}), s		1.6		1.3		3.2		4.7
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v	0.972	0.000		0.972	0.000		0.972	0.000		0.972	0.000	
Pedestrian F_s / F_{delay}	0.000	0.093		0.000	0.093		0.000	0.083		0.000	0.083	
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	720.00	10.25		720.00	10.29		880.00	7.90		880.00	7.89	
Bicycle F_w / F_v	-3.64	0.23		-3.64	0.14		-3.64	0.34		-3.64	0.50	

HCS7 Signalized Intersection Results Graphical Summary

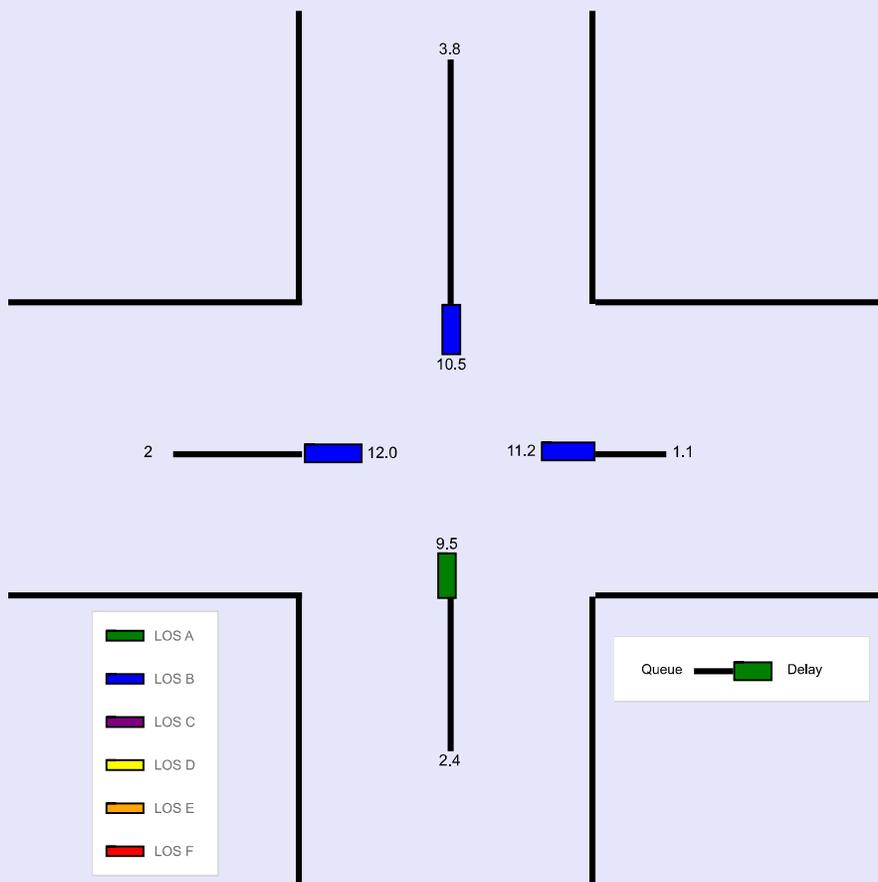
General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	AM	Analysis Date	7/16/2019	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2019	Analysis Period	1 > 4:30		
Intersection	Emerson Street	File Name	Existing PM Peak.xus				
Project Description	5582.900						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	43	46	40	38	32	7	28	147	11	7	241	28

Signal Information				EB				WB				NB				SB			
Cycle, s	50.0	Reference Phase	2	Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Force Mode	Fixed	Simult. Gap N/S	On																

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)		50.4			28.7			60.7			96.9	
Back of Queue (Q), veh/ln (95 th percentile)		2.0			1.1			2.4			3.8	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.0			11.2			9.5			10.5	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.0		B	11.2		B	9.5		A	10.5		B
Intersection Delay, s/veh / LOS	10.6						B					



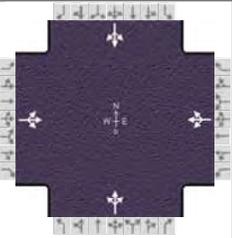
--- Messages ---

No errors or warnings exist.

--- Comments ---

HCS7 Signalized Intersection Input Data

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 4:30		
Intersection	Emerson Street	File Name	2027 NB PM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	46	49	43	41	34	8	30	158	12	8	259	30

Signal Information														
Cycle, s	50.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
				Red	1.0	1.0	0.0	0.0	0.0	0.0				

Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	46	49	43	41	34	8	30	158	12	8	259	30
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		R	4		None			R	2		None	
Heavy Vehicles (P _{HV}), %		2			2			2			2	
Ped / Bike / RTOR, /h	87	3	0	55	9	0	26	15	0	28	14	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	4	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft		12.0			12.0			12.0			12.0	
Turn Bay Length, ft		0			0			0			0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	25	25	25	25	25	25	25	25	25	25	25	25

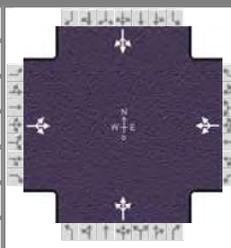
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
	Maximum Green (G _{max}) or Phase Split, s		23.0		23.0		27.0	
Yellow Change Interval (Y), s		4.0		4.0		4.0		4.0
Red Clearance Interval (R _c), s		1.0		1.0		1.0		1.0
Minimum Green (G _{min}), s		18		18		22		22
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s		2.0		2.0		2.0		2.0
Recall Mode		Max		Max		Max		Max
Dual Entry		Yes		Yes		Yes		Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25									
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No									
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No		0.50									

HCS7 Signalized Intersection Results Summary

Appendix E

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1> 4:30
Intersection	Emerson Street	File Name	2027 NB PM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	49	43	41	34	8	30	158	12	8	259	30

Signal Information												
Cycle, s	50.0	Reference Phase	2									
Offset, s	0	Reference Point	End									
Uncoordinated	No	Simult. Gap E/W	On									
Force Mode	Fixed	Simult. Gap N/S	On									
		Green	18.0	22.0	0.0	0.0	0.0	0.0				
		Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
		Red	1.0	1.0	0.0	0.0	0.0	0.0				

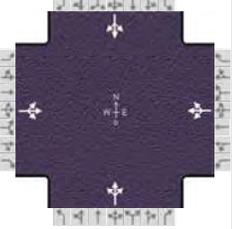
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						5.8		8.1
Green Extension Time (g _e), s		0.0		0.0		1.1		1.0
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	152			91			220			326		
Adjusted Saturation Flow Rate (s), veh/h/ln	1512			1453			1732			1814		
Queue Service Time (g _s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g _c), s	3.2			1.7			3.8			6.1		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	640			631			845			872		
Volume-to-Capacity Ratio (X)	0.237			0.145			0.260			0.374		
Back of Queue (Q), ft/ln (95 th percentile)	54.5			31.1			66.1			106.5		
Back of Queue (Q), veh/ln (95 th percentile)	2.1			1.2			2.6			4.2		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d ₁), s/veh	11.3			10.8			8.9			9.6		
Incremental Delay (d ₂), s/veh	0.9			0.5			0.7			1.2		
Initial Queue Delay (d ₃), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.1			11.3			9.6			10.8		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.1	B		11.3	B		9.6	A		10.8	B	
Intersection Delay, s/veh / LOS	10.8						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.74	A	0.64	A	0.85	A	1.03	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 4:30
Intersection	Emerson Street	File Name	2027 NB PM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	49	43	41	34	8	30	158	12	8	259	30

Signal Information															
Cycle, s	50.0	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0					
				Red	1.0	1.0	0.0	0.0	0.0	0.0					

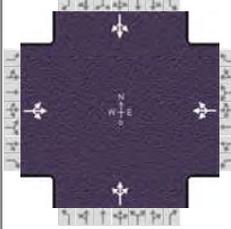
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.984	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.893	0.808		0.795	0.777		0.939	0.926		0.995	0.970	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.808		0.000	0.777		0.000	0.926		0.000	0.970
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.932			0.904			0.982			0.979		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.859			0.897			0.939			0.937
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	504	537	471	718	595	140	260	1368	104	49	1582	183
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1287		1188		1060		1190
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1565		1356		1664		1839
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		16.3		14.8		15.9		18.2
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		3.9		2.0		9.4		15.5
Queue Service Time Before Blockage (g_{ts}), s		1.7		1.4		3.4		5.1
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB			WB			NB			SB		
Pedestrian F_w / F_v	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000	0.000	
Pedestrian F_s / F_{delay}	0.000	0.093	0.000	0.093	0.000	0.083	0.000	0.083	0.000	0.083	0.083	
Pedestrian M_{corner} / M_{cw}												
Bicycle c_b / d_b	720.00	10.26	720.00	10.29	880.00	7.90	880.00	7.90	880.00	7.90	7.90	
Bicycle F_w / F_v	-3.64	0.25	-3.64	0.15	-3.64	0.36	-3.64	0.36	-3.64	0.54	0.54	

HCS7 Signalized Intersection Results Graphical Summary

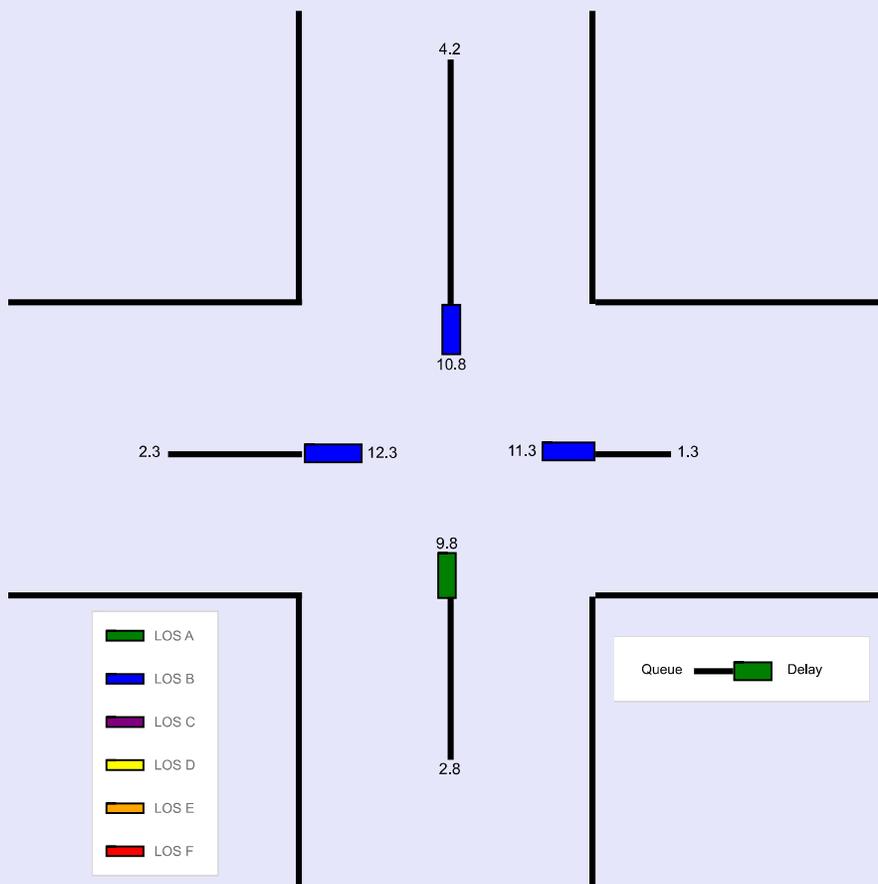
General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2027 NB	Analysis Period	1 > 4:30		
Intersection	Emerson Street	File Name	2027 NB PM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	46	49	43	41	34	8	30	158	12	8	259	30

Signal Information				EB				WB				NB				SB			
Cycle, s	50.0	Reference Phase	2	Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Force Mode	Fixed	Simult. Gap N/S	On																

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)		54.5			31.1			66.1			106.5	
Back of Queue (Q), veh/ln (95 th percentile)		2.1			1.2			2.6			4.2	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.1			11.3			9.6			10.8	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.1		B	11.3		B	9.6		A	10.8		B
Intersection Delay, s/veh / LOS	10.8						B					



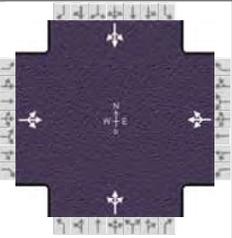
--- Messages ---

No errors or warnings exist.

--- Comments ---

HCS7 Signalized Intersection Input Data

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1> 4:30		
Intersection	Emerson Street	File Name	2027 TOT PM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	52	51	41	37	8	43	158	12	8	259	32

Signal Information				Signal Timing (s)								Signal Phases			
Cycle, s	50.0	Reference Phase	2	Green	18.0	22.0	0.0	0.0	0.0	0.0	1	2	3	4	
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	5	6	7	8	
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	0.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On												

Traffic Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	52	51	41	37	8	43	158	12	8	259	32
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		R	4		None			R	2		None	
Heavy Vehicles (P _{HV}), %		2			2			2			2	
Ped / Bike / RTOR, /h	87	3	0	55	9	0	26	15	0	28	14	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	4	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft		12.0			12.0			12.0			12.0	
Turn Bay Length, ft		0			0			0			0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	25	25	25	25	25	25	25	25	25	25	25	25

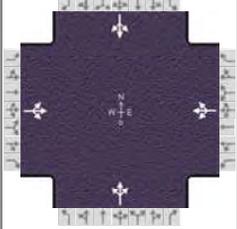
Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s		23.0		23.0		27.0		27.0
Yellow Change Interval (Y), s		4.0		4.0		4.0		4.0
Red Clearance Interval (R _c), s		1.0		1.0		1.0		1.0
Minimum Green (G _{min}), s		18		18		22		22
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s		2.0		2.0		2.0		2.0
Recall Mode		Max		Max		Max		Max
Dual Entry		Yes		Yes		Yes		Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25									
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No									
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50										

HCS7 Signalized Intersection Results Summary

Appendix E

General Information				Intersection Information			
Agency	GHA			Duration, h	0.250		
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other		
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91		
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1> 4:30		
Intersection	Emerson Street	File Name	2027 TOT PM Peak.xus				
Project Description	HACC SHERMAN UPDATE						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	46	52	51	41	37	8	43	158	12	8	259	32

Signal Information														
Cycle, s	50.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On	Green	18.0	22.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
				Red	1.0	1.0	0.0	0.0	0.0	0.0				

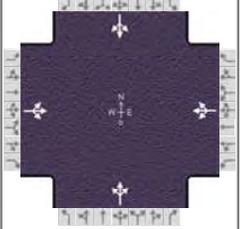
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		8.0		8.0		8.0		8.0
Phase Duration, s		23.0		23.0		27.0		27.0
Change Period, (Y+R c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g s), s						6.1		8.2
Green Extension Time (g e), s		0.0		0.0		1.1		1.1
Phase Call Probability						1.00		1.00
Max Out Probability						0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	164			95			234			329		
Adjusted Saturation Flow Rate (s), veh/h/ln	1512			1461			1675			1812		
Queue Service Time (g s), s	0.0			0.0			0.0			0.0		
Cycle Queue Clearance Time (g c), s	3.5			1.8			4.1			6.2		
Green Ratio (g/C)	0.36			0.36			0.44			0.44		
Capacity (c), veh/h	638			632			823			871		
Volume-to-Capacity Ratio (X)	0.256			0.149			0.284			0.377		
Back of Queue (Q), ft/ln (95 th percentile)	59.4			32.3			71.5			107.3		
Back of Queue (Q), veh/ln (95 th percentile)	2.3			1.3			2.8			4.2		
Queue Storage Ratio (RQ) (95 th percentile)	0.00			0.00			0.00			0.00		
Uniform Delay (d 1), s/veh	11.4			10.8			9.0			9.6		
Incremental Delay (d 2), s/veh	1.0			0.5			0.9			1.2		
Initial Queue Delay (d 3), s/veh	0.0			0.0			0.0			0.0		
Control Delay (d), s/veh	12.3			11.3			9.8			10.8		
Level of Service (LOS)	B			B			A			B		
Approach Delay, s/veh / LOS	12.3		B	11.3		B	9.8		A	10.8		B
Intersection Delay, s/veh / LOS	10.9						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.67	B	1.67	B	1.65	B	1.65	B
Bicycle LOS Score / LOS	0.76	A	0.64	A	0.87	A	1.03	A

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1> 4:30
Intersection	Emerson Street	File Name	2027 TOT PM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	52	51	41	37	8	43	158	12	8	259	32

Signal Information															
Cycle, s	50.0	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	No	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
		Green		18.0	22.0	0.0	0.0	0.0	0.0						
		Yellow		4.0	4.0	0.0	0.0	0.0	0.0						
		Red		1.0	1.0	0.0	0.0	0.0	0.0						

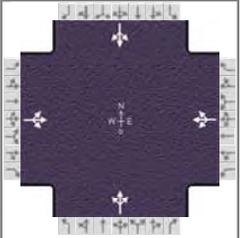
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000	1.000	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	0.880	1.000	1.000	1.000	1.000	1.000	0.890	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	0.984	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.902	0.808		0.798	0.781		0.907	0.895		0.995	0.969	
Right-Turn Adjustment Factor (f_{RT})		0.000	0.808		0.000	0.781		0.000	0.895		0.000	0.969
Left-Turn Pedestrian Adjustment Factor (f_{LPb})	0.932			0.907			0.982			0.979		
Right-Turn Ped-Bike Adjustment Factor (f_{RPb})			0.859			0.897			0.939			0.937
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	467	528	517	696	628	136	338	1242	94	48	1569	194
Proportion of Vehicles Arriving on Green (P)	0.36	0.36	0.36	0.36	0.36	0.36	0.44	0.44	0.44	0.44	0.44	0.44
Incremental Delay Factor (k)		0.50			0.50			0.50			0.50	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)		5.0		5.0		5.0		5.0
Green Ratio (g/C)		0.36		0.36		0.44		0.44
Permitted Saturation Flow Rate (s_p), veh/h/ln		1284		1179		1058		1191
Shared Saturation Flow Rate (s_{sh}), veh/h/ln		1583		1364		1602		1839
Permitted Effective Green Time (g_p), s		18.0		18.0		22.0		22.0
Permitted Service Time (g_u), s		16.2		14.5		15.8		17.9
Permitted Queue Service Time (g_{ps}), s		0.0		0.0		0.0		0.0
Time to First Blockage (g_l), s		4.3		2.2		7.2		15.5
Queue Service Time Before Blockage (g_{ts}), s		1.8		1.5		3.7		5.1
Protected Right Saturation Flow (s_R), veh/h/ln								
Protected Right Effective Green Time (g_R), s								

Multimodal	EB		WB		NB		SB	
Pedestrian F_w / F_v	0.972	0.000	0.972	0.000	0.972	0.000	0.972	0.000
Pedestrian F_s / F_{delay}	0.000	0.093	0.000	0.093	0.000	0.083	0.000	0.083
Pedestrian M_{corner} / M_{cw}								
Bicycle c_b / d_b	720.00	10.26	720.00	10.29	880.00	7.90	880.00	7.90
Bicycle F_w / F_v	-3.64	0.27	-3.64	0.16	-3.64	0.39	-3.64	0.54

HCS7 Signalized Intersection Results Graphical Summary

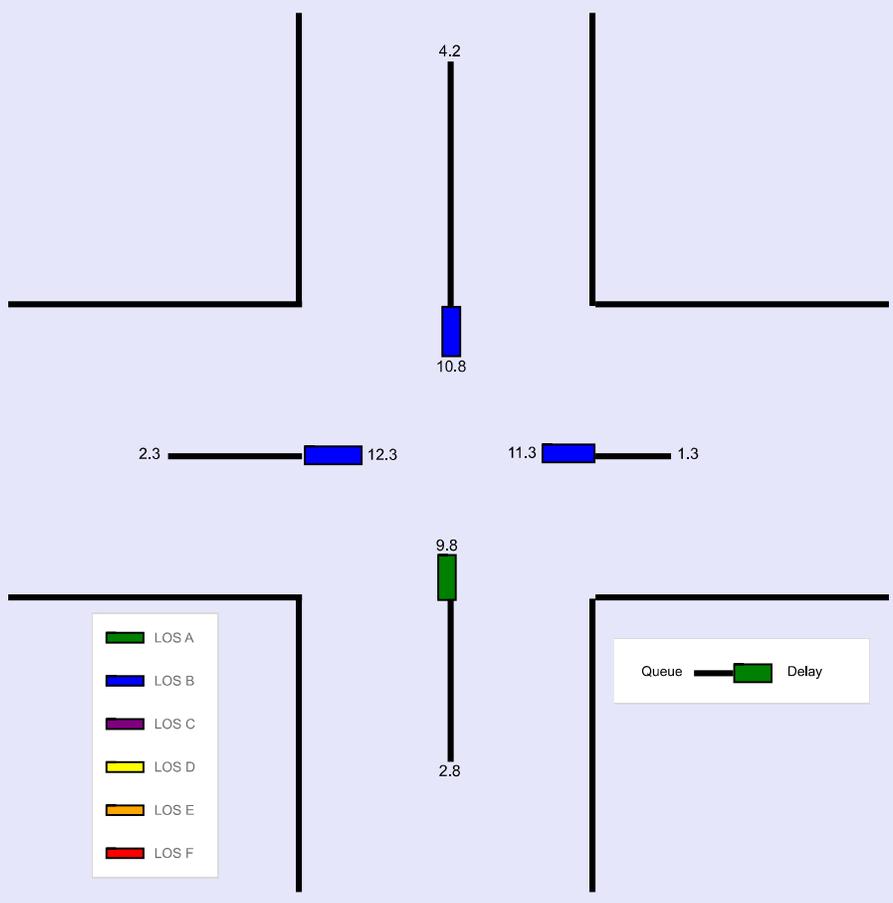
General Information				Intersection Information	
Agency	GHA			Duration, h	0.250
Analyst	DB	Analysis Date	Sep 28, 2021	Area Type	Other
Jurisdiction	IDOT	Time Period	PM Peak	PHF	0.91
Urban Street	Sherman Avenue	Analysis Year	2027 TOTAL	Analysis Period	1 > 4:30
Intersection	Emerson Street	File Name	2027 TOT PM Peak.xus		
Project Description	HACC SHERMAN UPDATE				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	46	52	51	41	37	8	43	158	12	8	259	32

Signal Information				EB				WB				NB				SB			
Cycle, s	50.0	Reference Phase	2	Green	18.0	22.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Uncoordinated	No	Simult. Gap E/W	On	Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Force Mode	Fixed	Simult. Gap N/S	On																

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Back of Queue (Q), ft/ln (95 th percentile)		59.4			32.3			71.5			107.3	
Back of Queue (Q), veh/ln (95 th percentile)		2.3			1.3			2.8			4.2	
Queue Storage Ratio (RQ) (95 th percentile)		0.00			0.00			0.00			0.00	
Control Delay (d), s/veh		12.3			11.3			9.8			10.8	
Level of Service (LOS)		B			B			A			B	
Approach Delay, s/veh / LOS	12.3		B	11.3		B	9.8		A	10.8		B
Intersection Delay, s/veh / LOS	10.9						B					



--- Messages ---

No errors or warnings exist.

--- Comments ---

RE: Link Parking Evanston

Rachel Goldstein <RGoldstein@ca-ventures.com>

Fri 9/24/2021 7:08 AM

To: Daniel Brinkman <dbrinkman@gha-engineers.com>

Dan,

210 units are currently leased.

Best,

Rachel Goldstein
Regional Manager
Property Management

130 E. Randolph Street
Suite 2100
Chicago, IL 60601
Direct: +1 312 561 6328
Email: RGoldstein@ca-ventures.com

www.ca-ventures.com



From: Daniel Brinkman <dbrinkman@gha-engineers.com>

Sent: Friday, September 24, 2021 7:07 AM

To: Rachel Goldstein <RGoldstein@ca-ventures.com>

Subject: Re: Link Parking Evanston

[ALERT * External Email * ALERT]

Rachel,

Thanks so much for the quick response. One last question. How many units are currently leased?

Dan

From: Rachel Goldstein <RGoldstein@ca-ventures.com>

Sent: Friday, September 24, 2021 6:58 AM

To: Daniel Brinkman <dbrinkman@gha-engineers.com>; Logan Hollensteiner <lhollensteiner@ca-ventures.com>

Cc: NGuzman@thehacc.org <NGuzman@thehacc.org>

Subject: RE: Link Parking Evanston

Daniel,

Please find the requested information below.

- Number of Units - [211](#)

- Unit Mix (2 bed,1 bed, studio) –
71 - Studios, 39 - 1 bedrooms, 92 – 2 bedroom, 39 – 3 bedroom
- # of units market rate 211 units are market rate
- # of affordable units (if applicable). -0
- # of parking spaces provided 161
- # of parking spaces leased/rented/ etc. – 61

Best,

Rachel Goldstein
Regional Manager
Property Management

130 E. Randolph Street
Suite 2100
Chicago, IL 60601
Direct: +1 312 561 6328
Email: RGoldstein@ca-ventures.com

www.ca-ventures.com



From: Daniel Brinkman <dbrinkman@gha-engineers.com>
Sent: Thursday, September 23, 2021 8:45 AM
To: Logan Hollensteiner <lhollensteiner@ca-ventures.com>; Rachel Goldstein <RGoldstein@ca-ventures.com>
Cc: NGuzman@thehacc.org
Subject: Link Parking Evanston

[ALERT ** External Email ** ALERT]

Rachel and Logan,

Nancy Guzman with HACC provided me your contact information. As part of the study for the expansion of the 1900 Sherman building, we are being asked to review parking demand for buildings in the area.

I'm hoping you can provide the following information regarding the Link building in Evanston:

Number of Units
Unit Mix (2 bed,1 bed, studio)
of units market rate
of affordable units (if applicable)
of parking spaces provided
of parking spaces leased/rented/ etc.

We are trying to establish what the actual demand for parking is for buildings like Link vs what the City code may require.

Your attention to this inquiry is appreciated. Should you have any questions at all please don't hesitate to reach out to me directly.

Thanks in advance

Dan

Dan Brinkman, P.E.,PTOE
Assistant Director of Transportation Services



An Employee Owned Company

625 Forest Edge Drive | Vernon Hills, Illinois 60061

Office: [\(847\) 478-9700](tel:8474789700) | Fax: [\(847\) 478-9701](tel:8474789701)

Direct: [\(847\) 821-6222](tel:8478216222) | Email: dbrinkman@gha-engineers.com

www.gha-engineers.com

P Please consider the environment before printing this email.

This message may contain information that is confidential, legally privileged, or subject to other restrictions on use or redistribution. This message is intended only for the use of the addressee(s) named above. If you are not the intended recipient of this message, or if you have received this message in error, you are hereby notified that any review, dissemination, distribution, or duplication of this message or the information contained within it is strictly prohibited. If you are not the intended recipient, please so notify the sender by reply email and delete all copies of the original message.

This message may contain information that is confidential, legally privileged, or subject to other restrictions on use or redistribution. This message is intended only for the use of the addressee(s) named above. If you are not the intended recipient of this message, or if you have received this message in error, you are hereby notified that any review, dissemination, distribution, or duplication of this message or the information contained within it is strictly prohibited. If you are not the intended recipient, please so notify the sender by reply email and delete all copies of the original message.

RECORD OF TELEPHONE CONVERSATION

PROJECT: KLBC Expansion

GHA PROJECT #: 5582.900 DATE: 9/20/21

TIME: 11

Name of Caller: DPB

Representing: _____

Phone Number: _____

Name of Person Called: Dhanesh Dahi

Representing: EZ Expansion

Phone Number: 312-867-7181

Discussion: _____

UNITS 353 90% occupied +/-

178 USED Some DAY PARKING

175 AVAILABLE

353 TOTAL SPACES

ply = 1:1

Recommendation and/or action to be taken: _____

Copies / Distribution _____



MEETING MINUTES

PLAN COMMISSION

Wednesday, October 13, 2021

7:00 P.M.

Evanston Civic Center, 2100 Ridge Avenue, James C. Lytle Council Chambers

Members Present: George Halik, John Hewko, Brian Johnson, Matt Rodgers, Kristine Westerberg

Members Absent: Jeanne Lindwall,

Staff Present: Meagan Jones, Neighborhood and Land Use Planner
Alexandria Ruggie, Assistant City Attorney

Presiding Member: Matt Rodgers

1. CALL TO ORDER / DECLARATION OF QUORUM

Commissioners Rodgers called the meeting to order at 7:00 P.M. Ms. Jones called the roll and a quorum was established. Commissioner Rodgers clarified that Chair Lindwall is recusing herself from this evening's agenda item and he will be acting as Chair for this meeting.

2. APPROVAL OF MEETING MINUTES: August 11, 2021.

Commissioner Halik suggested minor edits. Commissioner Westerberg made a motion to approve the meeting minutes of August 11, 2021 as amended. Seconded by Commissioner Halik. A roll call vote was taken and the minutes were approved, 5-0.

3. NEW BUSINESS

**A. Major Adjustment to a Planned Development –
1900 Sherman Avenue**

21PLND-0076

The Housing Authority of Cook County submits for a Major Adjustment to a Planned Development at 1900 Sherman Ave, previously approved by ordinance 109-O-20, to construct a 16-story residential building in the C1a Commercial Mixed-Use Zoning District. The applicant proposes changes to the following site development allowances: 1) a decrease in the number of dwelling units from 168 to 152, 2) a decrease in the number of below grade parking spaces from 37 to 25, and 3) a decrease in zoning height from 172 ft. 8 in. to 168 ft. 4 in.

APPROVED

Ms. Jones provided a brief overview of the proposed adjustment to the planned development detailing project characteristics, proposed changes to approved site development allowances and staff's recommendation.

Mr. William James of Camiros, representing the Housing Committee of Cook County, asked for clarification on the parking recommendation. Ms. Jones responded that recommendation to increase the number of off-site parking spaces was meant to offset the loss of on-site parking. The Commission can agree with that recommendation or suggest a different number of spaces or parking solution.

Mr. Monocchio of the Housing Authority of Cook County provided an introduction of the project and development team and shared that the Board's Vice-President, Polly Kuehl was present at the meeting.

Mr. James then explained the need for the three proposed adjustments and emphasized that none of the approved public benefits are being changed. He detailed that structural issues unearthed due to the existing Perlman building's foundation lead to the need to separate the existing and new building by 10 feet in order to ensure structural stability and reduce the parking levels by one floor. He added that the Covic-19 pandemic also changed what people want with regards to units: larger personal space and less shared space.

Mr. Greg Klosowski, Pappageorge Hames, explained the reduction in units, stating the reconfiguration of the building lead to 1 unit reduction per floor and an increase in the square footage of the units. Mr. James stated that this is a needed change and decreases impacts with regards to traffic and parking demand. The approved on-site affordable units will be maintained.

Mr. James and Mr. Klosowski then explained the height reduction for the building. Mr. Klosowski stated with the further design of the building the space was able to be tightened up and lead to a slight reduction in the height.

Mr. Klosowski then explained the need to reduce the parking. There was an initial assumption that the Perlman building was built on caissons, however it was not and was built on a thick slab of concrete. This changes the spread of the weight load and lead to the need to reconfigure the garage space and pull it away from the Perlman building. Mr. James added that there is a reduction in units by 16 and the parking by 12 spaces. He mentioned that staff noted the underutilization of parking of nearby buildings. The Link at 811 Emerson only has 61 of its spaces being leased and there is capacity in nearby buildings. HACC is in discussion with The Link and has an agreement with E2 at 1890 Maple. HACC has promised the Perlman residents that parking will be provided to them.

The hearing was then open to questions from the Commission.

Commissioner Halik stated that there have been a number of projects that have mentioned that recent projects have more parking than they need. He then stated it would be god to keep track of the parking utilization. Ms. Jones stated that this is noted. The recent TOD

APPROVED

parking study showed the underutilization and there are recent projects that have requested a reduction in their required parking. This is something that staff will look into in the future.

Commissioner Westerberg asked for clarification on the number of units and number of parking spaces being provided. Mr. James clarified that 152 units will be provided with a total of 39 on-site parking spaces. He added that with the population being seniors there will likely be less demand than at a building such as The Link.

Commissioner Johnson asked for clarification on the parking lease. Mr. James responded that currently HACC has an agreement with E2 to lease 50 parking spaces and is talking with The Link to lease additional spaces (up to 80 spaces) as there are unutilized parking spaces that are available to be leased.

Commissioner Halik asked if there is a cost for residents to lease those parking spaces. Mr. James responded that existing Perlman residents would not have to pay for parking. Residents of the new building would have to pay for parking spaces. He sensed that those who are in the affordable units would not need the parking. Commissioner Halik added that understands that the need for parking is decreasing but that an argument sometimes used is that people do not want to pay for parking and demand is low due to those residents not wanting to pay then end up parking on the street. Mr. James clarified that part of the approval of the development was that residents not be eligible for on-street parking permits

Commissioner Westerberg inquired if data had been gathered on how many seniors actually may need cars. Mr. James responded that the traffic study provided analyzed anticipated demand; however, he believes the need is overstated, showing a higher utilization than what is currently seen at The Link. Due to more senior tenants, there is less parking demand.

Commissioner Westerberg if there is a plan that could be shared for addressing caregivers needing a space to park and ensuring they do not park on the street. Mr. James responded that the development team has looked into the demand. Mr. Monocchio added that as it relates to caregivers, HACC will pay for that parking in The Link building.

Chair Rodgers asked for information on the parking report summary requested by Plan Commission. Ms. Jones provided a brief summary stating that generally, the summary showed that there is less demand for parking spaces especially if the development is near public transit options.

The hearing was then open to public question and testimony. Chair Rodgers stated that the Plan Commission's purview is the adjustments being requested by the applicant and questions and comments should be related to those items.

Mr. Bruce Enenbach requested a continuance due to some nearby residents not receiving public notice for the meeting, he had concerns and questions on the possible needed remediation of the soil, the building structural issues, and the need for the C1a zoning that

APPROVED

was granted since the buildings are now separated and there is no commercial space provided. He added that there should be an ability to participate in the meeting remotely

Chair Rodgers asked for clarification on the noticing requirements and actions. Ms. Jones stated that, per the code, a legal notice was published in the Evanston Review, notices were sent to residents within 1,000 feet of the property and signs with public hearing information were posted on the property. Ms. Ruggie confirmed that this met noticing requirements. Chair Rodgers then stated that items related to soil remediation and structural issues are outside of the purview of the Commission. The C1a rezoning was previously approved by Council and is not something that is under consideration. The meeting format is something that is determined by the City and can be discussed.

Ms. Cecile McHugh also requested a continuance in order to rebut the claim that the development meets Inclusionary Housing Ordinance and calculations for the site development allowances as well as raise concerns regarding soil testing information. The Link site and parking lot behind it had to remediate soil prior to construction and that should be addressed.

Chair Rodgers stated that the Inclusionary Housing is not what being looked at this evening. They are not proposed to be changed. Parking is under review and can be discussed.

Ms. Clare Waistell requested a continuance to rebut the adequacy and accessibility of parking and mentioned concern over the inability of renters to request a continuance. Chair Rodgers asked if this was indeed the case. Ms. Ruggie stated she will confirm if that is the case.

Chair Rodgers explained that a number of the items being mentioned are items that City Council has previously approved. The Commission is reviewing the items where there is a proposed major adjustment but if City Council wishes to refer items back to the Commission it is able to do so.

Commissioner Westerberg stated she is inclined to grant the continuance explaining that the Commission was split on the vote for the approved development and proceeded to Planning and Development Committee without a recommendation. She expressed some concern regarding the C1a zoning issue raised. Commissioner Halik agreed.

Commissioner Johnson asked for clarification on granting the continuance. Chair Rodgers responded that Plan Commission rules are a bit different than Zoning Board of Appeals and state that continuances are for the purpose of rebutting testimony. He then asked Legal staff if testimony could still be taken from the public once a continuance is presented. Ms. Ruggie and Ms. Jones provided clarification on procedures stating that testimony could still be taken at the meeting following the submission of a written continuance. Rules are not clear if the continuance has to be immediate. Ms. Jones stated that the Rules do state that testimony can be taken if those individuals know they will be unable to attend the continued hearing.

Mr. Enenbach stated that Mr. Monocchio has stated that “no one is building this type of building” and this shows that this is not a financially viable building and not what the County is paid to do. The County provides needed services but speculative real-estate is not their job. He then asked the Commission to reject the proposal.

Mr. Enenbach then emphasized the structural issues stating that moving the building seems to be inefficient and there should be shoring done of the existing Perlman building. This was done to the condominium building that stands next to the southern Mather Building in Evanston. He added he is not satisfied with the structural review and stated it should be done by an independent structural engineer.

Ms. McHugh stated that the new building will be on the southernmost section of the lot. She calculated the FAR relative to its section of the lot and the Perlman building relative to its portion of the lot. She does not think that if both buildings were new that they would be approved and this should be taken as an opportunity to stop the development in exchange for something that is closer to the design of the building.

Chair Rodgers then provided clarification on when

Mr. Steve Morrison clarification on if the proportion of units and unit size has changed. Mr. Klosowski responded that the unit sizes are larger with the exception of the units that were adjacent to the existing building. Mr. Morrison then asked if there was information on where data on tenant preference changes due to the Covid pandemic. Mr. James responded that this information was brought forward by the applicant and their partner, Related Midwest which has expertise in private market housing.

Ms. Jones then read comments from Ms. Kiera Kelly into the record. Expressing concerns of the C1a zoning, emphasizing that half of the Commission was not in favor of the original development when it was presented. She also mentioned that a precedent is being set with regards to building size and lack of parking. Additional concerns included traffic already being untenable and rental pricing concerns. Ms. Jones stated that these comments will be included in a comment addendum she will include with the next meeting packet.

Additional discussion occurred regarding what will be discussed at the next meeting and the ability to grant a continuance for this agenda item.

Commissioner Westerberg made a motion to continue this agenda item to the October 27, 2021 Plan Commission meeting. Seconded by Commissioner Johnson. A roll call vote was taken and the motion passed, 4-1.

Ayes: Hewko, Johnson, Rodgers, Westerberg

Nays: Halik

Councilmember Kelly asked if the next meeting could be held in a hybrid manner. Mr. Rodgers responded that City staff determines how the meeting would be held. Ms. Ruggie stated that this could be done if the Commission chooses for the meeting to be held in that manner.

Commissioner Westerberg made a motion to hold the meeting in a hybrid manner. Seconded by Commissioner Hewko. A roll call vote was taken and the motion was approved 4-1.

Ayes: Halik, Hewko, Rodgers, Westerberg

Nays: Johnson

Councilmember Kelly asked for clarification on the Plan Commission's purview since there were items brought up during the meeting that were stated as not being under the Commission's purview. Those items should be considered. Chair Rodgers stated that once an application is received, staff outlines those items that are to be reviewed. Ms. Jones responded that the zoning code does outline what the Commission's responsibilities are and overtime Legal staff has stated that there are items that are not under the Commission purview, such as public benefits. Ms. Ruggie added that the Commission is restricted to the applicant's application and not dictated by staff. The purview is pursuant to the application that is provided, especially since this project has already been approved.

Halik stated that he does not believe the structural integrity of a building and soil has been under the Commission's purview and are building permit issues. Commissioner Westerberg responded that zoning is under the Commission's purview and if there are development changes that triggered the zoning change that should be looked at.

Mr. James stated he believes many of the topics mentioned have been off-topic and should be limited to the requested adjustments. He clarified that comments will be limited to the parking.

Mr. Enebach stated that the building is not structurally the same and should start over.

5. PUBLIC COMMENT

No public Comment

6. ADJOURNMENT

Commissioner Johnson moved to adjourn the meeting. Commissioner Halik seconded. A voice vote was taken and the motion was approved. Meeting was adjourned at 8:22 PM.

Respectfully Submitted,
Meagan Jones, Neighborhood and Land Use Planner, Community Development
Department

Comments on planned development at 1900 Sherman Avenue

1 message

Judith Segal <judywsegal@gmail.com>
To: mmjones@cityofevanston.org

Sat, Oct 9, 2021 at 5:24 PM

Attention: Meagan Jones
Neighborhood and Land Use Planner

As per your recent e-mail, I am writing to offer comments on the planned development at 1900 Sherman Avenue.

One of the changes being proposed is a reduction in the number of underground parking spaces. The 25 spaces allotted for parking will not be sufficient for the number of expected residents in the building. Having the residents park elsewhere will create congestion in the neighborhood.

I understand that the number of parking spaces has been reduced because of safety issues. I am concerned about having such a large building erected close to my current residence that may pose safety issues.

I am also concerned that none of the affordable units in this building will have two bedrooms. I understand that this is not consistent with the inclusionary housing ordinance for the City of Evanston. Even though the affordable units are expected to be occupied by seniors rather than families, a second bedroom may be needed in one's unit if a senior requires help from a caregiver.

I am also concerned about the fact that initial approval for the building required a relaxing of local zoning regulations. I purchased property in the City of Evanston five years ago believing that these zoning regulations would protect me. Much to my surprise, every time a new high rise is proposed that violates existing zoning regulations, the zoning regulations are relaxed to accommodate the developer's request. I would like the City of Evanston to have dependable zoning regulations that are enforced and serve as a protection to residents.

Sherman Avenue going north from the proposed development is presently an area of two story houses and low rise apartment buildings. A sixteen story building at 1900 Sherman would change the nature of this neighborhood. As a resident living in this area, I would like the neighborhood to retain its current nature.

Judith Segal

Proposed adjustment to 1900 Sherman

1 message

Mickey <mjp3483@comcast.net>
To: mmjones@cityofevanston.org

Tue, Oct 12, 2021 at 11:11 PM

I question the wisdom of allowing a building of such height be approved in an area that abuts a residential neighborhood. In addition parking is already very problematic in this area and a building with multiple units will only make it more difficult. Please reconsider these factors in your decisions. Thank you. Mal and Mickey Poland. 800 Elgin Road 801.

Sent from my iPhone

Please deny today's submission (was denied ability to comment remotely)

Kiera Kelly <kiera.kelly@k2-pr.com>

Wed, Oct 13, 2021 at 7:26 PM

To: Meagan Jones <mmjones@cityofevanston.org>, Johanna Leonard <jnyden@cityofevanston.org>

Cc: Clare <clareangelakelly@gmail.com>

Meagan and Johanna,

Please forward the following to the Plan Commissioners. I am sorry that the public is being denied the ability to participate remotely tonight under the circumstances of a pandemic and to the exclusion of seniors and others (like me) who are not able to attend.

Hello,

I was stunned not to be able to attend the meeting virtually to share my public comment, being that we are still in a pandemic and because it is much more exclusionary. In any case, here are my comments. I ask you also to remember that in the last Plan Commission, half of the members denied this application because of the two-part zoning changes and the impact and precedent to the fragile neighborhood (on the edge of downtown and in Northwestern's backyard.)

Comments:

“The primary reason for the request to change the zoning is that a Planned Development in a Residential District requires a 12-foot separation between buildings, which cannot be altered via a site development allowance.”

(Taken from the developer's presentation to Plan Commission on 9/9/2020.)

We are here today because the applicant no longer needs the 12-foot separation between buildings, which was the primary rationale for having requested two absurd zoning classification changes:

1. A “map amendment” to change this parcel from RESIDENTIAL R6 to C1a COMMERCIAL, but once that is approved...
2. Ask for a “special use” so this COMMERCIAL property can be flipped back to a “RESIDENTIAL” use.

Since this separation is no longer needed, neither is the grounds for the “zoning gymnastics” (the developer's words) required for the building, which undermines our zoning code and led to a building that was nearly twice as tall -- 16 stories -- as the original zoning allows in this RESIDENTIAL AREA. The submission today should be denied.

Otherwise, we are beginning a precedent where developers simply select the type of zoning they need in order to get the building they feel like they want and then inform the city accordingly (city staff, city commissions, City Council) accordingly. As we know, our zoning is supposed to guide developers -- not have developers guide our planning. Not only does it give us a building that is nearly twice as tall as it should be, but the city creates a situation where another developer can use this case as a precedent. Flash forward to the next developer's power point presentation slide given to another Plan Commission in a few years. (See: the LINK building that was promised as a one-off, now used as precedent.)

Today's requested changes are an important lesson for us: developers' “rationales” for outrageous asks beyond our zoning is the absolute only way to make a building work — is false. Here we were told that the spacing and the exact number of units were firm and there was absolutely no wiggle room. We hear similar claims all the time. Yet the very few times the city denied a proposal for exceeding zoning, such as The LINK right next door, the developer miraculously comes back with a plan . Another example -- the 34 story office building tower on Davis that the developer said was absolutely the only way it was economically viable, yet came back later with a proposal that was

nearly half as high — and met our zoning. Or Northlight Theatre saying the only way they could come back to Evanston was the 37th St. tower on Sherman. They are now back with a three-story building.

The rationale has disappeared for the Emerson's egregious zoning changes, so please re-double your duty to uphold the normal zoning classifications and the zoning we have. More shifting false claims are happening time and time again because developers it works in this City and a zoning code is completely adjustable. We also know they do not care about our neighborhoods, they don't care about gentrification or people being priced out of town as what is the cycle with these luxury buildings, but we hope you do.

Please remember that at the last plan commission meeting half of the members recognized that this building being nearly two times too tall for a residential neighborhood was it too much to ask, especially forever changing the precedent in this fragile area outside of downtown and stressed by the developers creating northwestern housing. Also, the public benefits have been greatly reduced as this is a 70% luxury building with only the required number of affordable housing. The \$1666 rent for a 1 bedroom is hardly providing any "missing middle." Hundreds of currently available units in Evanston offer rents much lower.

Finally, the traffic on Emerson is untenable as is -- people waiting 2-3 times through a traffic light when traveling west. Any analysis showing otherwise is yet another falsehood.

Please deny this change today as it undermines the integrity of our zoning code and as the Plan Commissioners said at the last vote, it asks too much of the existing neighborhood,

Thank you.
Kiera Kelly

Plan Commission Public Comment Sign Up Form

noreply@formstack.com <noreply@formstack.com>
 Reply-To: noreply@formstack.com
 To: mmjones@cityofevanston.org

Mon, Oct 25, 2021 at 9:25 PM



Formstack Submission For: [Plan Commission Public Comment Sign Up Form](#)

Submitted at 10/25/21 9:25 PM

Name: Judith Segal

Address of Residence: [800 Elgin Road, Unit 1603, Evanston, IL 60201](#)

Phone: (224) 307-2448

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here:

I am opposed to the proposed changes to the plan for 1900 Sherman. I am concerned about the lack of parking at the site. Requiring that most of the residents of this building park elsewhere in the neighborhood will create congestion. I am also concerned that the number of parking spaces was reduced because of safety issues. I wonder if there are continuing safety concerns associated with this project.

In general, I do not like the idea of having a building of this size erected in this neighborhood. It will change the character of the neighborhood, which presently consists entirely of two story homes and low rise apartment buildings. I think the City of Evanston should use this land to build a facility that consists entirely of low income housing units and that is similar in overall size to the public housing building next to it which will share the same lot. This would represent a more effective use of the land for low income housing.

Agenda Item (or comment on item not on the agenda): 1900 Sherman

Position on Agenda Opposed

Item:

[Quoted text hidden]

Plan Commission Public Comment Sign Up Form

noreply@formstack.com <noreply@formstack.com>
Reply-To: noreply@formstack.com
To: mmjones@cityofevanston.org

Wed, Oct 27, 2021 at 6:48 PM

**Formstack Submission For: [Plan Commission Public Comment Sign Up Form](#)**

Submitted at 10/27/21 6:48 PM

Name: Toni Rey**Address of Residence:** 1020 Grove St**Phone:** (847) 847-7644**How would you like to make your public comment?:** Written (see below)**Provide Written Comment Here:** I support changes to the Emerson building, and are proud that Evanston is planning a senior multi income building with many affordable units. I'd like to live there in a full fee unit and I qualify agewise.**Agenda Item (or comment on item not on the agenda):** 1900 Sherman**Position on Agenda Item:**

[Quoted text hidden]



MEETING MINUTES

PLAN COMMISSION

Wednesday, October 27, 2021

7:00 P.M.

Evanston Civic Center, 2100 Ridge Avenue, James C. Lytle Council Chambers

Members Present: George Halik, John Hewko, Brian Johnson, Matt Rodgers, Kristine Westerberg

Members Absent: Jeanne Lindwall,

Staff Present: Meagan Jones, Neighborhood and Land Use Planner
Brian George, Assistant City Attorney

Presiding Member: Matt Rodgers

1. CALL TO ORDER / DECLARATION OF QUORUM

Commissioners Rodgers called the meeting to order at 7:00 P.M. Ms. Jones called the roll and a quorum was established. Commissioner Rodgers clarified that Chair Lindwall is recusing herself from this evening's agenda item and he will be acting as Chair for this meeting.

2. APPROVAL OF MEETING MINUTES: October 13, 2021.

Commissioner Westerberg made a motion to approve the meeting minutes of October 13, 2021. Seconded by Commissioner Halik. A roll call vote was taken and the minutes were approved, 5-0.

3. OLD BUSINESS

**A. Major Adjustment to a Planned Development –
1900 Sherman Avenue**

21PLND-0076

The Housing Authority of Cook County submits for a Major Adjustment to a Planned Development at 1900 Sherman Ave, previously approved by ordinance 109-O-20, to construct a 16-story residential building in the C1a Commercial Mixed-Use Zoning District. The applicant proposes changes to the following site development allowances: 1) a decrease in the number of dwelling units from 168 to 152, 2) a decrease in the number of below grade parking spaces from 37 to 25, and 3) a decrease in zoning height from 172 ft. 8 in. to 168 ft. 4 in.

DRAFT- NOT APPROVED

Cecile McHugh stated that the proposed parking is not adequate. Staff determined that 88 spaces needed. The City recommends leasing additional spaces. HACCC's analysis uses weighted analysis for E2 and The Link. Students at the Link do not need cars. The net new parking spaces are 2% of what is required. She added that The Link and Focus are in talks to develop the parking lot north of the Link. The E2 development is too far away from the site and too much street and alley crossing would be needed.

Clare Waistell stated she lives within 500 ft. of the site and she rebuts that the parking is adequate. The Zoning Analysis and the Applicant show that there is a need for more parking. She added that key fobs are needed to access parking at The Link and questioned access. She then questioned the accessible spaces and where staff will park, adding that there are 25 spaces for 152 new units.

Bruce Enenbach stated he has concerns regarding what is under consideration. He expressed that the developer is deciding what is under consideration and "the tail is wagging the dog" and nothing of something was discussed. He then stated that the changes proposed should lead to the zoning reverting back to R6 and that an independent structural analysis needs to be conducted.

Kiera Kelly explained that additional parking was found to be needed in the parking study. She then stated that the comparable buildings nearby, The Link and E2, are largely student living and questioned how those buildings were used versus the new building on Howard Street and the Avidor. She stated that it is not accurate to use the Perlman building to predict parking and there is a big assumption of bikes being used by seniors. She expressed that residents in the area would have to live with the consequences of approval of the development and urged the Commission to deny the proposal

Bill James of Camiros stated that there are a number of comments being made by people who are not experts. The applicant used a consultant, Gewalt Hamilton Associates, to conduct its study.

Dan Brinkman of Gewalt Hamilton Associates stated that there were preliminary discussions done with E2 and The Link to determine their need for parking. The study stated that 101 spaces would be needed which is greater than the 88 required by the City. He added that the site itself is under-parked.

Mr. James stated that the parking utilization is comparable and useful. Mr. Brinkman stated that the market rate is probably not but is accurate in terms of usage. It is helpful to have spaces nearby.

Mr. James stated that the request for continuance was granted for the purposes of discussing parking. Relevant information is what requested deviations from the Code are. The underground parking needed to be redesigned. Utilization in nearby buildings has been shown. There has been a deletion of 16 units which compensates for the loss in parking spaces. Mr. James added that the original approval provided a letter with a commitment from E2 to lease 50 parking spaces. The Link currently has up to 70 parking

spaces to lease. The applicant is compensating for the loss of on-site parking spaces with leased spaces. He added that there will not be a perfectly comparable building for this project.

Commissioner Westerberg inquired if the applicant had a chance to check the ratio of parking spaces to residents in other nearby senior spaces. Mr. Brinkman responded that The Link and E2 were the only properties consulted.

Commissioner Hewko asked if residents would be prohibited from obtaining on street parking permits as is done for other developments. Staff clarified that this was one of the conditions of the original approval and would carry over with the Major Adjustments.

Sue Loellbach asked that the Commission support the proposed changes. Both staff and other organizations have vetted the changes. She asked the hearing be brought to a close and the project move forward.

Ms. Kelly stated that caregivers are foreseen and it was mentioned that the applicant stated they would pay for the caregivers and asked if the agreement to cover the costs of caregivers to park would be put in writing as it could get expensive. Mr. James stated that HACC made a statement that spaces would be leased at The Link building. Mr. Brinkman added that there was no specific data on caregivers using spaces but that 23 visitor spaces were included as 101 spaces referenced in the parking and traffic study. Richard Minnocchio, Executive Director stated that this information is not known but he did make the commitment that caregivers would not pay for parking if their agency did not cover that cost. Mr. James stated that this could be a condition that the Commission makes.

Ms. Kelly asked if undergrad dorm parking needs are similar to what is seen in a typical luxury building. Mr. Brinkman responded that there is not a lot of information on dormitory type parking, nor is there much information on parking demands for affordable units. Generally if a building is more than 5 or 6 stories the parking need is typically about half the number of units. National standards were used and there is not a ton of information on this specific use.

Ms. McHugh stated that there are errors in math and that it is not in question if The Link is a student dorm. There is a different utilization for students that live a few blocks away from their university from a TOD building. E2 matches the City's requirements for parking and the zoning analysis stated 88 parking spaces are required in addition to existing parking spaces.

Mr. James stated that parking demand varies from building to building. The Housing Authority will make sure that Perlman residents will have parking onsite or in the adjacent building. Residents of the new building will purchase parking either in the underground parking garage or nearby building and it is to the developers advantage to make sure this parking is secured. Ample off-site parking is not a question as there are buildings nearby that have overbuilt parking and have more than they need. He added that the loss of 12

DRAFT- NOT APPROVED

spaces along with the reduction of units by 16 does not change the original premise for consideration of parking or project approval in the applicant's opinion.

The public hearing was closed and the Commission began deliberation.

Commissioner Westerberg stated that parking was a problem at the initial review and percentages can be difficult to ascertain. She then inquired how solid the agreements to lease the parking spaces are and how to ensure residents will not park in the surrounding neighborhood. She added that seniors will not want to walk 3 blocks. Parking is adjacent but is it still accessible.

Commissioner Halik stated that there are 90 spaces available at the Link so there would be no need to go to E2 for parking.

Commissioner Rodgers stated that, per staff, there is possibility of development on the lot adjacent to The Link development so parking is not necessarily firm. He expressed concern of a new development coming in and saying they have no parking.

Commissioner Hewko stated that the developer could lower the building height but that would have economic impacts. With regards to parking, if residents are prohibited from obtaining on-street parking permits, that alleviates some of the concern. Some residents will do this, some will not but that is not a basis to deny the project.

Commissioner Rodgers stated that he is not concerned about the height or number of units which are lower than what was originally approved. The main concern is parking. Without residents being able to get on-street parking permits they risk getting tickets. Parking is also available elsewhere.

The Commission then reviewed the standards.

Standards for Special Use were found to be met with some additional discussion occurring surrounding parking impact being offset by prohibiting on-street parking permits to building residents and requiring 12 additional parking spaces.

Standard for Planned Development was found to be met with a reduction in impact relating to height and number of dwelling units.

Standards for development in the C1a District were found to be maintained noting that an extension is being requested to the amount of time provided to obtain building permits.

Commissioner Halik made a motion to recommend approval of the major adjustments as stated by staff with additional conditions that 1) 12 additional parking spaces are leased to offset those lost in the underground facility and 2) parking for caregivers is paid for, if requested, by the Housing Authority of Cook County. A roll call vote was taken and the motion passed, 4-1.

Ayes: Halik, Hewko, Johnson, Rodgers
Nays: Westerberg

5. PUBLIC COMMENT

No public Comment

6. ADJOURNMENT

Commissioner Halik moved to adjourn the meeting. Commissioner Westerberg seconded. A voice vote was taken and the motion was approved. Meeting was adjourned at 8:17 PM.

Respectfully Submitted,
Meagan Jones, Neighborhood and Land Use Planner, Community Development
Department